

Application reference number	2025/0583
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Application Type	Full
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Proposal Description:	Erection of detached two-storey dwelling with associated access and garage.
Location:	Land adjacent St John the Evangelist Church, Carlton Road, Carlton, Barnsley.

Applicant	Mr R Gates
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Number of Third-Party Reps	None	Parish:	N/A
		Ward:	Monk Bretton

Summary

The proposal seeks full planning permission for the erection of 1 no detached dwelling. The proposal is acceptable in policy terms being designated as Urban Fabric within the Local Plan and is therefore considered to be an acceptable and sustainable form of development in line with paragraphs 7 and 8 of the National Planning Policy Framework (NPPF, 2024). The report demonstrates that any harm generated by the proposal is outweighed by other material planning considerations. The development would not cause an unacceptable level of harm to neighbouring properties, the highway network, trees, or the wider character of the conservation area or listed buildings subject to conditions.

Recommendation: **GRANT Planning Permission**

Site Description

The application relates to a currently vacant site which is rectangular in shape and laid to grass. The site measures circa 0.11 hectares and is located to the west of St John the Evangelist Church and to the east of The Gables, both Grade II listed buildings, and to the north of a new residential development accessed from Spring Lane. The site is also located within the Carlton Conservation Area.

The site is level and set at a slightly elevated position to the classified B6132 Carlton Road and is bounded by a low-level stone wall to the north, close boarded timber fencing to the south and mature trees/hedgerow to the west. There is no existing boundary treatment located along the eastern boundary between the site and the Church. There is no existing vehicular access to the site from Carlton Road.

The immediate area is predominantly residential in nature and consists of a mix of housing styles and types and construction materials.

Proposal

The applicant seeks permission for the erection of 1 no detached dwelling and associated detached, single storey garage, with access to the dwellings taken from Carlton Road.

The proposed dwelling is to be orientated on a north/south axis following a similar building line to the neighbouring church. The property is two storey and is to be arranged internally in a traditional manner with living accommodation at ground floor and bedrooms on the upper floor.

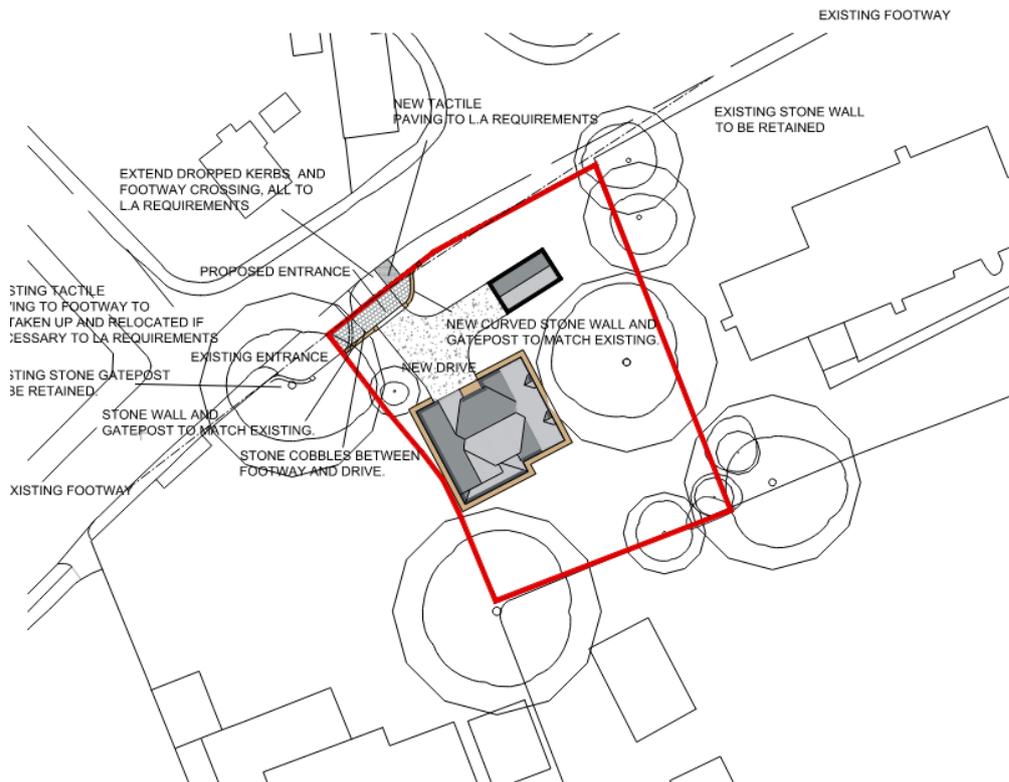


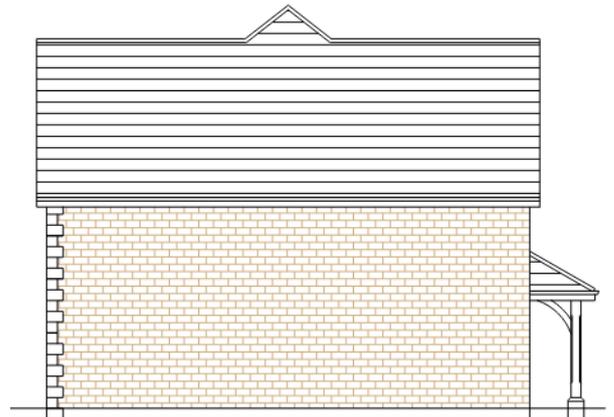
Figure 2 Proposed Site Plan



Figure 2 Proposed Front and Side Elevations

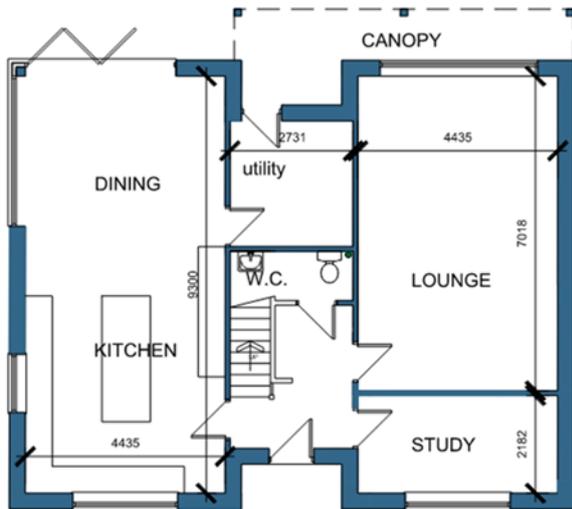


SOUTH ELEVATION



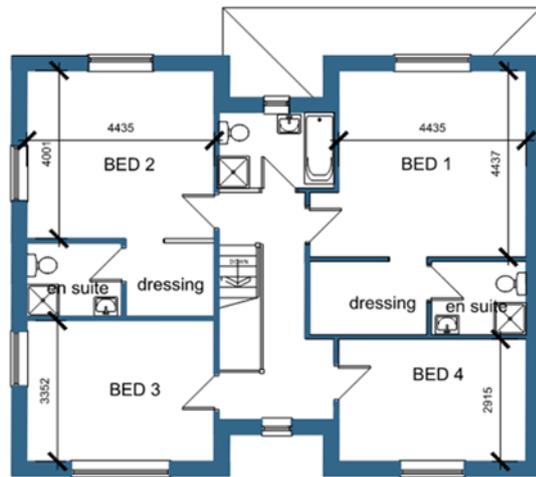
WEST ELEVATION

Figure 5 Proposed Rear and Side Elevations



GROUND FLOOR PLAN

Figure 5 Proposed Floor Plans



FIRST FLOOR PLAN



WEST ELEVATION



SOUTH ELEVATION

Figure 5 Proposed Garage Front and Side Elevations

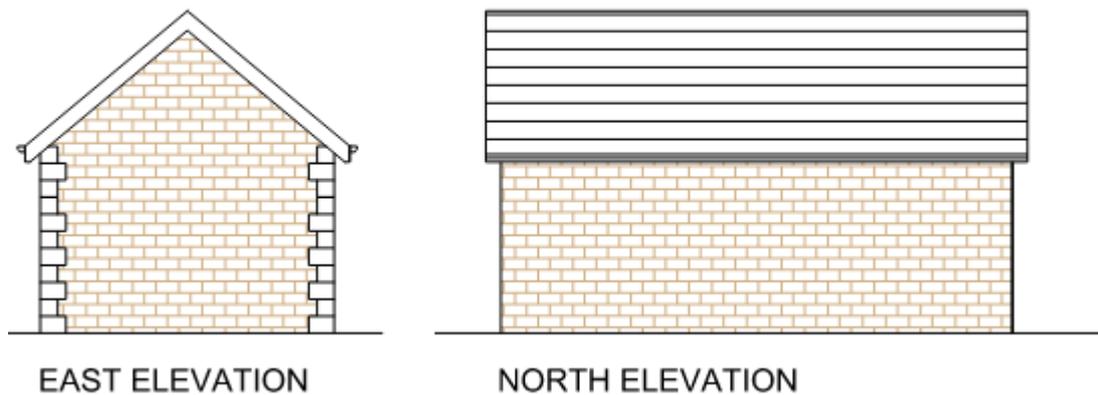


Figure 7 Proposed Garage Rear and Side Elevations



Figure 7 Proposed Front Boundary Detail

Policy Context

The Development Plan

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires proposals to be determined in accordance with the development plan unless material considerations indicate otherwise. The development plan for Barnsley consists of the Barnsley Local Plan (adopted January 2019).

The Local Plan was adopted in January 2019 and is accompanied by seven masterplan frameworks which apply to the largest site allocations (housing, employment, and mixed-use sites). In addition, the Council has adopted a series of Supplementary Planning Documents and Neighbourhood Plans which provide supporting guidance and specific local policies which are a material consideration in the decision-making process.

The Local Plan review was approved at the full Council meeting held on 24th November 2022. The review determined that the Local Plan remains fit for purpose and is adequately delivering its objectives. This means, no updates to the Local Plan, in whole or in part, are to be carried out ahead of a further review. The next review is due to take place in 2027, or earlier, if circumstances require it.

Local Plan

The site is located within Urban Barnsley and is identified as Urban Fabric and within the Carlton Conservation Area within the Local Plan and as such the following policies are considered to be relevant to this application:

- Policy H4 Residential Development on Small Non-allocated Sites
- Policy GD1 General Development
- Policy D1 High Quality Design and Place Making

Policy T4 New development and Transport Safety
Policy BIO1 Biodiversity and Geodiversity
Policy HE1 The Historic Environment
Policy HE3 Developments affect the Historic Buildings
Policy Poll1 Pollution Control and Protection
Policy SD1 Presumption in favour of Sustainable Development
Policy LG2 The Location of Growth
Policy H1 The Number of New Homes to be Built
Policy H2 The Distribution of new Homes
Policy RE1 Low Carbon and Renewable Energy

Adopted Supplementary Planning Documents

In line with the Town and Country Planning (Local Planning) (England) Regulations 2012, Barnsley has adopted twenty-eight Supplementary Planning Documents (SPDs) following the adoption of the Local Plan in January 2019. The most pertinent SPDs in this case is:

Design of Housing Development
Parking

Other

South Yorkshire Residential Design Guide (SYRDG)

National Planning Policy Framework (NPPF) (2024)

The National Planning Policy Framework sets out the Governments planning policies for England and how these are expected to be applied. At the heart is a presumption in favour of sustainable development. Development proposals that accord with the development plan should be approved unless material considerations indicate otherwise.

Where the development plan is absent, silent, or relevant policies are out-of-date, permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework as a whole; or where specific policies in the Framework indicate development should be restricted or unless material considerations indicate otherwise.

Chapter 5 Delivering a sufficient supply of homes

Paragraph 73 states that small and medium sized sites can make an important contribution to meeting the housing requirement of an area, are essential for Small and Medium Enterprise housebuilders to deliver new homes and are often built out relatively quickly. To promote the development of a good mix of sites, local planning authorities should:

- d) support the development of windfall sites through their policies and decisions – giving great weight to the benefits of using suitable sites within existing settlements for homes.

Chapter 9 Promoting Sustainable Transport.

Paragraph 110 states that significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions and improve air quality and public health. However, opportunities to maximise sustainable transport solutions will vary between urban and rural areas, and this should be taken into account in both plan-making and decision-making.

Paragraph 115 states that in assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:

- a) sustainable transport modes are prioritised taking account of the vision for the site, the type of development and its location;
- b) safe and suitable access to the site can be achieved for all users
- c) the design of streets, parking areas, other transport elements, and the content of associated standards reflects current national guidance, including the National Design Guide and the National Model Design Code; and
- d) any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree through a vision-led approach.

Paragraph 116 states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network, following mitigation, would be severe taking into account all reasonable future scenarios.

Chapter 12 Achieving well-designed places

Paragraph 135 states that planning policies and decisions should ensure that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) are visually attractive as a result of good architecture, layout, and appropriate and effective landscaping;
- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming, and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) create places that are safe, inclusive, and accessible and which promote health and well-being, with a high standard of amenity for existing and future users⁵¹; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

Paragraph 136 states that trees make an important contribution to the character and quality of urban environments and can also help mitigate and adapt to climate change. Planning policies and decisions should ensure that new streets are tree-lined, that opportunities are taken to incorporate trees elsewhere in developments, that appropriate measures are in place to secure the long-term maintenance of newly planted trees, and that existing trees are retained wherever possible.

Paragraph 139 states that development that is not well designed should be refused, especially where it fails to reflect local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes.

Chapter 15 Conserving and Enhancing the Natural Environment.

Paragraph 187 states planning decisions should contribute to and enhance the natural and local environment by; protecting sites of biodiversity value; minimising impacts on and

providing net gains for biodiversity, and incorporating features which support priority or threatened species such as swifts, bats and hedgehogs; and preventing new development from contributing to, being put at unacceptable risk from, or being adversely affected by, unacceptable levels of soil, air, water or noise pollution or land instability.

Paragraph 198 states that planning policies and decisions should also ensure that new development is appropriate for its location taking into account the likely effects (including cumulative effects) of pollution on health, living conditions and the natural environment, as well as the potential sensitivity of the site or the wider area to impacts that could arise from the development. In doing so they should:

- a) mitigate and reduce to a minimum potential adverse impact resulting from noise from new development – and avoid noise giving rise to significant adverse impact on health and the quality of life;
- b) identify and protect tranquil areas which have remained relatively undisturbed by noise and are prized for their recreational and amenity value for this reason; and
- c) limit the impact of light pollution from artificial light on local amenity, intrinsically dark landscapes, and nature conservation.

Paragraph 201 states that the focus of planning policies and decisions should be on whether proposed development is an acceptable use of land, rather than the control of processes or emissions (where these are subject to separate pollution control regimes). Planning decisions should assume that these regimes will operate effectively. Equally, where a planning decision has been made on a particular development, the planning issues should not be revisited through the permitting regimes operated by pollution control authorities.

Chapter 16 Conserving and enhancing the historic environment

Paragraph 202 states that heritage assets range from sites and buildings of local historic value to those of the highest significance, such as World Heritage Sites which are internationally recognised to be of Outstanding Universal Value. These assets are an irreplaceable resource and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations.

Paragraph 208 states that Local Planning Authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this into account when considering the impact of a proposal on a heritage asset, to avoid or minimise any conflict between the heritage asset's conservation and any aspect of the proposal.

Paragraph 210 states that in determining applications, local planning authorities should take account of:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- c) the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraph 212 states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Paragraph 213 states that any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification. Substantial harm to or loss of:

- a) grade II listed buildings, or grade II registered parks or gardens, should be exceptional;
- b) assets of the highest significance, notably scheduled monuments, protected wreck sites, registered battlefields, grade I and II* listed buildings, grade I and II* registered parks and gardens, and World Heritage Sites, should be wholly exceptional.

Paragraph 219 states that local planning authorities should look for opportunities for new development within Conservation Areas and World Heritage Sites, and within the setting of heritage assets, to enhance or better reveal their significance. Proposals that preserve those elements of the setting that make a positive contribution to the asset (or which better reveal its significance) should be treated favourably.

Relevant Consultations:

Biodiversity - No objections subject to conditions
Conservation Officer - No objections subject to conditions
Forestry Officer - verbal comments – no objections subject to conditions
Highways Drainage – No objections subject to conditions
Highways DC – No objections subject to conditions
Pollution control – No objections subject to conditions
Ward Councillors - No objections received
Yorkshire Water – No objections received

Representations

Neighbour notification letters were sent to 11 surrounding properties, and a site notice was posted adjacent to the site; no representations were received in relation to this application.

Assessment

The main issues for consideration are as follows

- The acceptability of residential development
- The impact on the listed building and conservation areas
- The impact on neighbouring residential properties
- The impact on the highway network and highways standards
- The impact on the ecology of the site

For the purposes of considering the balance in this application the following planning weight is referred to in this report using the following scale unless the NPPF establishes a specific weight.

- Substantial
- Considerable
- Significant
- Moderate
- Modest
- Limited
- Little or no

Principle of development

The site is located within a wider allocation of Urban Fabric. The site is also located in a residential area and so it is therefore considered that redevelopment for housing is a compatible use. The site is located within Urban Barnsley which is the main focus for housing growth.

Local Plan Policy H4 'Residential Development on Small Non-allocated Sites' states that proposals for residential development on sites below 0.4Ha will be allowed where the proposal complies with other relevant policies in the plan. These sites make a small but valuable contribution to the housing supply. Sites in towns and villages can offer good opportunities for providing houses where other people already live, near to shops and services. Developing these sites also reduces the need to provide new sites outside settlement boundaries. We will allow small scale residential development within towns and villages in line with other plan policies, including those protecting peoples living conditions, road safety, and design.

The site is also located within a Conservation Area and between two Grade II listed buildings, therefore development will be expected to respect historic precedents of scale, form, massing, architectural detail and the use of appropriate materials that contribute to the special interest of a building.

In addition to the above all new dwellings must ensure that living conditions and overall standards of residential amenity arise provided or maintained to an acceptable level both for new residents and those existing. In addition, development will only be granted where it would maintain visual amenity and not create traffic problems/reduce highway safety. An assessment of the proposals against those criteria is set out within the sections of the report below.

Residential Amenity

The SPD Design of New Housing Development states that 'in order to ensure adequate levels of privacy are provided/ maintained, to ensure residential development does not result in unacceptable levels of loss of outlook and in order to provide adequate amenity, development will be expected to comply with external spacing standards'.

It states that a minimum distance of 12m should be maintained between habitable room windows and blank elevations, 21m should be maintained between facing habitable room windows and that dwellings should be sited with a rear garden at least 10m in length where it borders a private rear garden. The siting and orientation of the dwelling is such that it would comply with the external spacing set out in the SPD and as such would not lead to an increase in overlooking, overshadowing or loss of outlook; this carries significant weight in favour of the application.

The plans submitted indicate that the property is proportioned with the overall floor area of the property exceeding the technical guidelines set out in the South Yorkshire Residential Design Guide (SYRDG) which would not detrimentally impact on the residential amenity of the future occupants of the proposed dwelling. The SYRDG also states that dwellings with 3 or more bedrooms should have a private amenity space exceeding 60m²; the proposed dwelling is designed with a private amenity space exceeding 60m²; this carries significant weight in favour of the application.

It is in this regard that the proposed development complies with SPD Designing New Housing Development, Local Plan Policy H4 Residential Development on Small Non-allocated Sites and Local Plan Policy GD1 General Development.

Scale, Design and Impact on the Character

This application seeks permission for the erection of a detached dwelling on a plot of land circa 20 metres to the west of the listed St John the Evangelist Church and 27m north of the Gables, both properties are Grade II listed. The site is also located within Carlton Conservation Area, albeit at the northern end of it, with historic buildings (many converted and altered) along Church Street and in close proximity to the site. The application is supported by a heritage statement which makes the case for the development having an overall neutral impact on the adjacent designated assets.

The plot appears never to have been developed and formerly formed part of the Wharnccliffe Estate lands. By 1893, the land appears to have been subsumed into the grounds / curtilage of the vicarage (The Gables) to the southwest. The site appears out of curtilage to the Gables and is physically screened by dense vegetation from it and as such the plot only makes a minor contribution to the setting of the Gables.

Due to the location of the plot, the situation is subtly different in relation to the setting of the church, as it is closer, and there is clear intervisibility from the church grounds looking west and vice versa and as such there is a minor contribution to the setting of the church. The heritage statement goes on to justify the proposal by suggesting the minor contribution the site makes, and the sympathetic nature / design of the proposed dwelling means there is on balance a justification for approval. The Conservation Officer has been consulted on the application, and they agree that the impact due to the change to the setting and any resulting harm is low and takes no issue with the proposal in principle. This carries significant weight in favour of the application.

The proposed dwelling will be seen in close context with the church and the Gables (to an extent) and as such, from a conservation area perspective and a group value standing, the development along this stretch of Carlton Road will be considered together. The design of the house has relatively steeply raked gables and mullioned windows which reflects elements of style from The Gables; the use of timber framing around the rear canopy, and the use of quoins are noted and welcomed.

The heritage statement notes the use of 'stock brick' to imitate the stone in buildings nearby. The Conservation Officer would suggest stone not brick represents the typical historic material and is present in the buildings that contribute the most to the conservation area and its group value. The bulk of the buildings in the immediate vicinity including the listed Church and The Gables are in stone and so coursed (split or pitched face) stone with ashlar dressings is the most appropriate material for the walls of the proposed dwelling. Therefore, a condition requiring details, along with a representative sample, of materials to be submitted for approval will be included.

Concerns have been raised in relation to the feather edged 1800 timber fencing along the eastern boundary of the site with St Johns Church. This appears extremely modern and utilitarian and is not especially sympathetic to the setting of the church. This could easily be softened with other materials and fence types that complement the setting more effectively or supplemented with native planting including shrubs and trees that enhance the screen between the plot and the church to the east. It is considered that an updated boundary treatment along the eastern boundary can be secured by a planning condition.

Supplementary Planning Document Design of Housing Development states that detached garages should ideally be located behind the frontage of their dwelling and should not be a feature of the street scene. It is acknowledged that the garage associated with the proposed dwelling would be located to the side and forward of the main elevation of the dwelling contrary to the guidance within the SPD. However, whilst there is space to the side of the

dwelling to accommodate a single garage, it would be located within the rooting area of the prominent mature Horse Chestnut tree located to the east of the proposed dwelling and siting a detached garage in this location could have significant impact on the trees long term viability. In addition, the garage is set back from the front boundary by approximately 5.5m, is to be a single garage measuring 6.5m by 3.5m, with the low eaves and roof sloping away from the boundary. The set back will also allow for landscaping planting to be introduced to provide screening of the garage. This carries significant weight in favour of the application and outweighs the potential impact on the street scene.

Based on the above assessment, it is therefore considered that the proposed dwelling would not be out of character with the area, the conservation area or impact upon the setting of the adjacent listed buildings. This carries significant weight in favour of the application and is considered acceptable and in compliance with Local Plan Policies HE1 The Historic Environment, HE3 Developments affecting Historic Buildings and D1 High Quality Design and Place Making.

Highway Considerations

The site is positioned off B6132 Carlton Road; this is considered to be a sustainable location with access to local amenities and good public transport links.

The proposal involves the creation of an access onto the classified road and plans outlining the visibility splays have been provided. It is acknowledged that the visibility splay to the west encroaches into the carriageway, however, it is in the channel of the lane of traffic moving away from the site and the splays are commensurate with the 85th-percentile speeds measured by way of an independent speed survey. This carries significant weight in favour of the application.

The development consists of a single 4 bedroomed dwelling and the adopted Supplementary Planning Document – Parking states that 3-bedroomed properties require the provision of 2no spaces per dwelling, which has been provided.

The layout plan shows the property with 1no driveway parking space and 1no detached garage space. The off-street parking spaces are located within the curtilage of the dwelling, and it is apparent that there is adequate space for vehicles to turn and be able to enter and exit the site in a forward gear. The site layout also provides ample space for refuse bin storage, and the access arrangement can serve as a refuse bin collection pad on refuse collection days. This carries significant weight in favour of the application.

Highways DC have been consulted on the application, and they raise no objection to the development or the introduction of the access onto B6132 Carlton Road, subject to conditions

As such the proposal is considered acceptable in terms of its impact on the highway network, in compliance with Local Plan Policy T4.

Impact on Geodiversity and Biodiversity

National requirements have seen the introduction of Biodiversity Net Gain (BNG). In England; BNG is mandatory under the Town and Country Planning Act 1990, whereby developers must deliver a biodiversity net gain of 10%, resulting in more or better-quality habitat than before a development. These regulations were introduced on 12th February 2024 and applied to any application submitted after this date.

Local Plan Policy BIO1 states that development will be expected to conserve and enhance the biodiversity and geodiversity features by protecting and improving habitats; maximising

biodiversity opportunities in and around new developments and encouraging provision of biodiversity enhancements. Policy BIO1 follows on to state that development will be expected to conserve and enhance the biodiversity and geological features by protecting ancient and veteran trees.

The Ecologist has reviewed the application in terms of the impact upon habitats. The application has been supported by a Preliminary Ecological Appraisal (PEA) and BNG Metric and report.

The submitted BNG metric includes baseline information as per the national validation requirements and indicates that there would be a loss of 1.24 habitat units and 0.24 hedgerow units and on this basis, the habitat trading rules are not met. The metric does not include post-development habitat information. However, this is not a reason for refusal, as there is no requirement for post development calculations to be provided at the application stage and would be dealt with through the discharge of the Biodiversity Net Gain condition and does not have to be rectified for the purposes of this application.

The Preliminary Ecological Appraisal has been amended to include protective measures for retained features during construction, and the plans and elevations drawing (ref: 24-157 01, Rev. B) has been amended to include the swift brick in an appropriate location and can be secured by way of a planning condition. This carries moderate weight in favour of the application.

The development has been sited to limit the impact on the substantial tree located to the east of the site and the trees/hedgerow located along the western boundary. This carries moderate weight in favour of the application.

The forestry officer has been consulted on the application and raises no objections to the development subject to the inclusion of conditions.

In light of the above, it is considered that the development is acceptable and in compliance with Local Plan Policy BIO1 Biodiversity and Geodiversity.

PLANNING BALANCE & CONCLUSION

In accordance with Paragraph 11 of the NPPF (2024), the proposal is considered in the context of the presumption in favour of sustainable development. The proposal is considered to be located within a sustainable location on a site suitable for residential development in the Local Plan and this weighs considerably in favour of the application. In addition, the site layout and design are reflective of the character of the area and safeguard neighbouring properties through appropriate separation distances, this weighs significantly in favour of the application. The amendments that have been undertaken have shown that a suitable access can be achieved that would not have a detrimental impact on highway safety and this weighs significantly in favour of the application.

All other material planning considerations have been fully explored by the appropriate consultees who have raised no objections to the proposal subject to conditions and holistically this weighs moderately in favour of the application.

The noise and disturbance associated with equipment used during the construction of the site can be mitigated and controlled by condition and the short-term noise and disturbance associated with implementing the planning permission is considered to carry limited weight against the proposal.

Having balanced all material planning considerations, the positive aspects of the proposal outlined above are not outweighed by any other materials planning considerations and as such the proposal is therefore, on balance, recommended for approval subject to the conditions listed below.

Recommendation

Grant planning permission subject to conditions

Justification

STATEMENT OF COMPLIANCE WITH ARTICLE 35 OF THE TOWN AND COUNTRY DEVELOPMENT MANAGEMENT PROCEDURE ORDER 2015

In dealing with the application, the Local Planning Authority has worked with the applicant to find solutions to the following issues that arose whilst dealing with the planning application.

To address the initial concerns raised by highways in terms of a suitable access to a classified road.

Due regard has been given to Article 8 and Protocol 1 of Article 1 of the European Convention for Human Rights Act 1998 when considering objections, the determination of the application and the resulting recommendation. It is considered that the recommendation will not interfere with the applicant's and/or any objector's right to respect for his private and family life, his home, and his correspondence.