
2021/0884– Full Planning Application

Applicant: Mr & Mrs John Farnsworth

Development: Demolition of existing bungalow and erection of new two storey detached dwellinghouse with attached double garage and associated external works

Address: 2 Rob Roy Lane, Barnsley, S70 6NR

Description

The site is a 925sqm parcel of land that is developed with a residential bungalow and surrounding private curtilage. The site backs onto the surrounding countryside and looks out towards Wentworth Castle on the opposite side of the valley. The property is accessed via a long private drive (Rob Royd Lane) that traverses north east from Keresforth Hall Road and which acts as the site's eastern boundary. The topography of the site falls in gradient to the west with a boundary shared with the residential curtilage 51 Keresforth Hall Road to the north (within the blue line boundary of the applicant) and Green Belt boundaries to the west and south. In respect of scale, all of the surrounding dwelling's are two-storey in nature and very large with equally large curtilages.



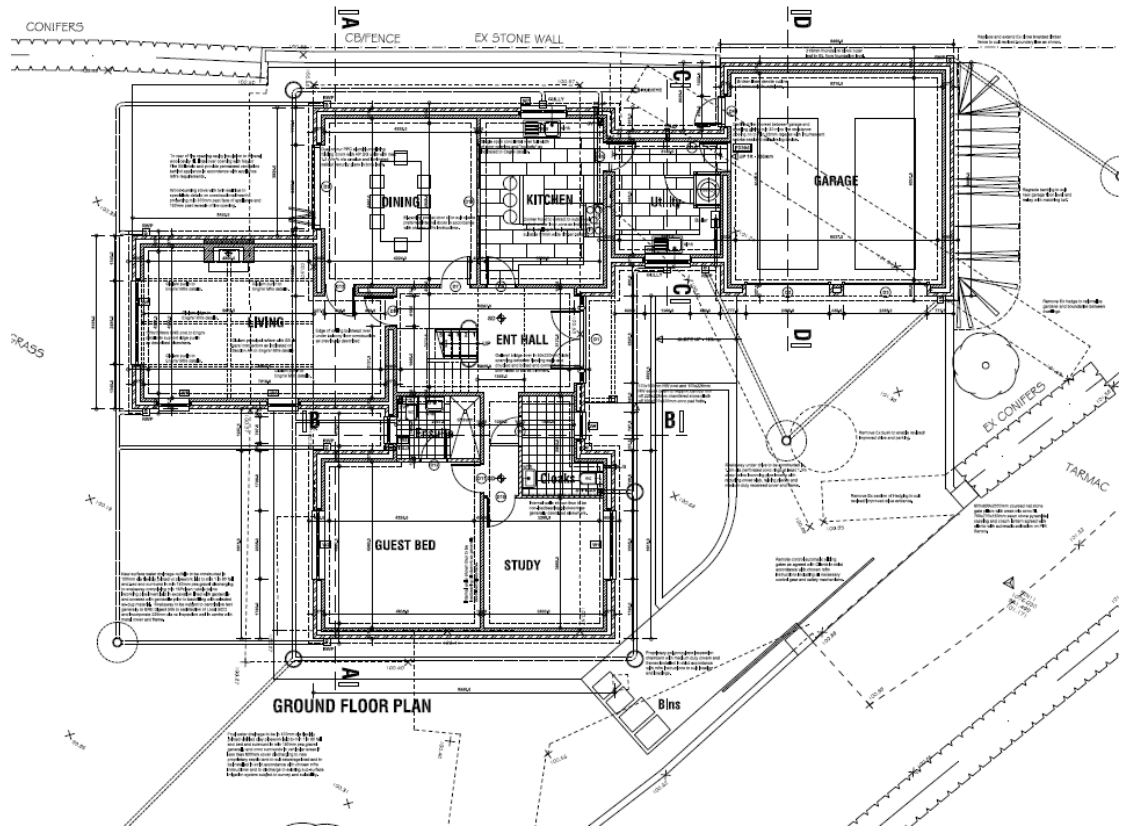
Proposed Development

The applicant seeks to erect a two-storey detached dwellinghouse on the site with a pitched roof and perpendicular garage arrangement. The proposed development will resurface the access in tarmac and form a large four bedroom dwellinghouse. The internal layout at the ground floor consists of an entrance hall, cloakroom/WC, a study, a guest bedroom, living room, kitchen/diner, utility room and double garage. A further three bedrooms are located at the first floor with a main bathroom and a walk-in wardrobe and en-suite provisioned for the master bedroom with a recessed roof terrace located on the southern slope of the western range. The external appearance of the dwellinghouse is such that it forms an L-shape footprint with four two-storey gables facing east and west connected by a central ridge. The utility room and garage form an eastern range that back onto the northern boundary with a single storey range on the western elevation of the main dwelling housing the living room. The eastern principle elevation features common-style casement windows and a wide feature door set within a lean-to porch whereas the western elevation is significantly different with vertically oriented full-height glazing. The northern elevation features an obscure glazed window serving the en-suite master bedroom. Materials are to be facing brick, natural roof slates, artificial stone heads and cills, coloured pvc windows and doors, aluminium bi-fold doors. 2no. parking spaces are provisioned within the garage and the overall layout, barring the western and eastern ranges, follows the footprint of the existing dwellinghouse.

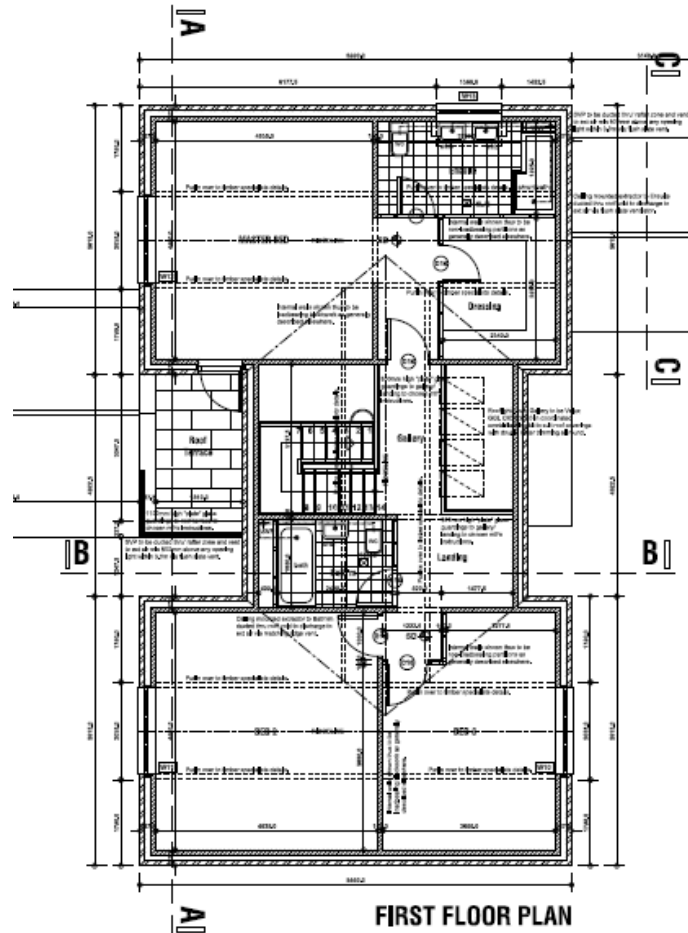
Site Layout



Ground Floor Plans



First Floor Plans



D1 – High Quality Design and Place Making

T4 – New Development and Transport Safety

Supplementary Planning Documents

Design of Housing Development (DHD)
Parking

Other Considerations

South Yorkshire Residential Design Guide

NPPF

The National Planning Policy Framework sets out the Government's planning policies for England and how these are expected to be applied. At the heart is a presumption in favour of sustainable development. Development proposals that accord with the development plan should be approved unless material considerations indicate otherwise. Where the development plan is absent, silent or relevant policies are out-of-date, permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework as a whole; or where specific policies in the Framework indicate development should be restricted or unless material considerations indicate otherwise.

In respect of this application, the policies above are considered to reflect the NPPF, which relates to high quality design and good standard of amenity for all existing and future occupants of land and buildings. They also reflect the advice in paragraph 130 (general design considerations) and paragraph 134, which state that 'permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions'.

Consultations

Responses have been received from the following consultees:

Public Rights of Way – 1no. informative recommended

Pollution Control – No objections subject to 1no. condition

Air Quality – No objections subject to 1no. condition

SYMAS – No objections

Highways Drainage – No objections subject to compliance with the building regulations.

Highways DC – No objections subject to 1no. condition

Forestry – No objections subject to 1no. condition

Representations

The application was advertised by way of neighbour notification letters, a site notice and a press notice. No comments have subsequently been received.

Assessment

The proposal will be reviewed under the following assessment topics:

- Principle of Development
- Visual Amenity + Residential Amenity
- Highway Safety
- Other Considerations

Principle of Development

The proposed development is designated as Urban Fabric in the Local Plan Policies Map. Urban Fabric is an allocation that acknowledges that land has previously been developed and is capable of being redeveloped on the basis that the scale, type, appearance, layout and landscaping of development accord with the predominant use of the area. As such development in Urban Fabric is generally acceptable where these planning factors satisfy the requirements of the visual and residential amenity section below.

When viewed purely in terms of the proposed location and use of the land, the proposal is acceptable in that it is a form of C3 development in a residential area within Urban Barnsley. The development therefore accords with the spatial requirements of LP Policy LG2 – Location of Growth.

Visual + Residential Amenity

In respect of the visual amenity of the proposed dwelling, there will be limited visibility within the wider streetscene and from Keresforth Hall Road that would impact the appearance of the local area. That being said, the impact on the private drive will likely be positive as it will replace a dilapidated bungalow of low architectural merit. Indeed the indicative finishing materials and detailing, such as high level vertically oriented glazing, are of a high quality and attractive format.

The layout and orientation of the proposal relative to the surrounding built environment is also visually acceptable in that it is to be set broadly within the footprint of the existing dwellinghouse albeit with eastern and western extensions that increase the footprint and which sit comfortably within the confines of the site area. The scale of the proposed dwelling will also increase to match the predominant two-storey dwellinghouses adjacent at 51 and 53 Keresforth Hall Road, but will remain less visible within the streetscene relative to those properties.

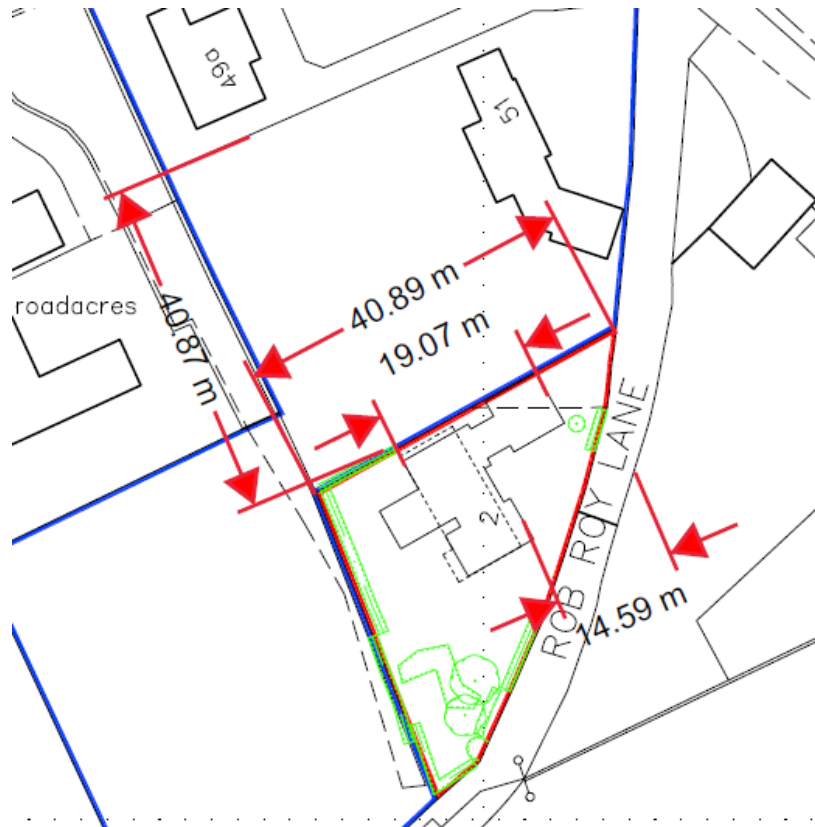
As regards the impact of the proposed dwelling on surrounding residents and their dwellinghouses, the impact upon the amenity of No.53 Keresforth Hall Road is within acceptable parameters as the creation of the east facing first floor window on the southern gable opposite No.53's rear curtilage will be approximately 14.6m from the rear boundary. Contextually, the rear curtilage of No.53 is significantly up-hill of the proposed dwelling and subject to a tall hedge across its western edge; both of which will significantly reduce opportunities for overlooking and potential privacy loss from the new habitable window to bedroom 3 in combination with the adequate 14.6m separation distance which is 46% in excess of the minimum 10m distance required as set out within the SPD Design of Housing Development.



The en-suite bathroom window facing No.51's curtilage is proposed as obscure glazed and shall be conditioned to ensure that this remains the case. The main impact in amenity terms is the height of the proposed dwelling relative to the southern boundary of No.51 Keresforth Hall Road. The side elevation of the dwelling shall have a width of approximately 19.10m across the

shared boundary with an eaves and total height of the dwellinghouse's northern gable reaching 4.05m and 7.30m respectively while the eaves and total height of the garage shall be 1.80m and 6.20m respectively. The total length of the boundary shared with No.51 is approximately 40.90m long and the width of No.51's rear curtilage is also approximately 40.90m wide with a total area of 1552sqm.

Plot Dimensions



The eaves heights for the proposed dwelling's gable are low for a two-storey property at around 4m as is the 2m eaves height of the garage. The ridge heights are more akin to typical two-storey development, however the roof slope of the dwellinghouse and its garage are directed away from the shared boundary to reduce their impact despite their southern location relative to the curtilage of 51 Keresforth Hall Road to the north. Given the size of 51's curtilage and that mutual site levels dictate that the curtilage of that neighbouring dwelling is set mostly at a higher point than that of the proposed dwelling, the impact on overbearance and overshadowing of No.51's rear curtilage is within acceptable parameters. In any case, the owner of No.51, who is also the applicant for this application, has confirmed agreement to the amenity loss that may be incurred through the proposed development. Overall the residential amenity situation is determined to be acceptable due to the size of the neighbouring curtilage at No.51 and it being set at a higher level.

The height of the garage is such that it could be capable of accommodating rooms in the roofspace at a future date. It is noted that both 51 and 53 Keresforth Hall Road have two-storey attached garages and therefore a precedent is set within the local area. Likewise the pitch of the proposed garage roof matches that of the main dwelling and therefore aesthetically conforms whereas a lower pitch would not tie-in with the appearance of the main roof and, in any case, no rooms in the roofspace of the attached garage have been proposed. To ensure that any future proposal for use of the roofspace conforms with the LPA's guidance, permitted development rights shall be removed as a part of this decision.

The internal and external amenity space standards also far exceed what is required as a minimum and therefore the proposed development is acceptable in relation to Local Plan Policies GD1 and D1 in respect of residential and visual amenity.

Highway Safety

The 2no. parking spaces in front of the dwellinghouse meet the quantum of parking spaces required for a dwelling of this size as set out in the SPD Parking and the dimensions of those spaces exceeds the minimum as set out in the SYRDG and are in fact in line with the Lifetime Home standard. The proposed integral garage is not long enough to be considered a garage as stipulated in the SYRDG, however this is not an issue as sufficient off-street parking is available in front of the property. The forecourt depth in front of the garage is sufficient to enable vehicles to manoeuvre and leave the site in a forward gear. The

The topography of the site should also not cause any issues in respect of levels and the width of the private drive exceeds the minimum of 3.1m across its length.

The distance to the dwelling from Keresforth Hall Road is in excess of 30m and though there is a requirement for a turning head capable of manoeuvre for a fire appliance, there is no increase in the quantum of dwellinghouses being served by this existing access and therefore there is no requirement to improve capacity to make this feasible.

The case officer noted on his site visit that the access was in poor repair, therefore a condition will be required for evidence of the improvements to the surfacing of the access prior to the dwelling's occupation.

On the basis of the above, the proposal is found to be in line with Local Plan Policy T4.

Other Considerations

The boundary of the site will be altered between 2 Rob Roy Lane and 51 Keresforth Hall Road so that it becomes straight, as it is currently dog-legged. The erection of the attached garage will incur the loss of the leylandii hedge and will impact upon a small ornamental tree/bush. Comments have been received by the Forestry Officer recommending protection plans submitted by condition, therefore the scheme is considered to be acceptable in respect of the impacts on the surrounding foliage.

Conclusion

On the basis of the above assessment, the proposal is found to be acceptable subject to conditions.

Recommendations:

Grant subject to conditions: