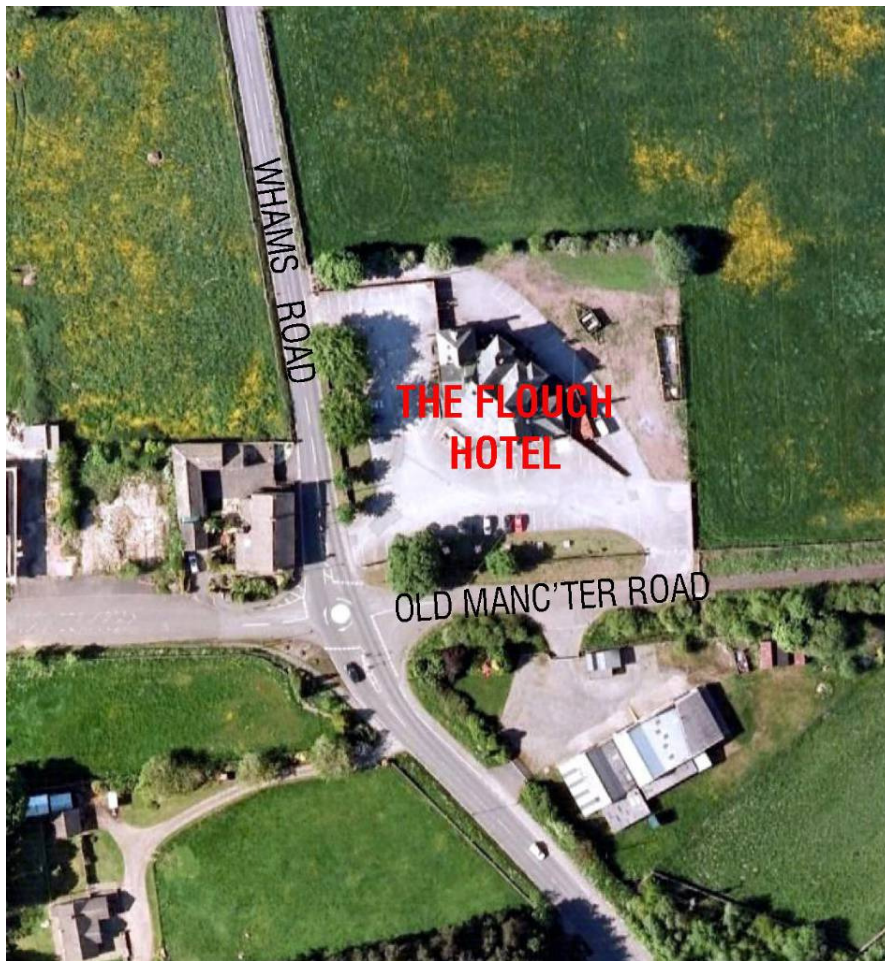


**Residential Development
The Flouch Hotel
Whams Road
Hazelhead
Sheffield
S36 4HH**

Ref: 11.17
Date: September 2017
Version: 02

DESIGN AND ACCESS STATEMENT



 **mboothdesign**

architectural design and building consultants

Fairfield House
Berneslai Close
Barnsley S70 2FL

T: 01226 286256
M: 07881898300
E: mboothdesign@btconnect.com

1.0 Introduction

- 1.1 This statement has been prepared in support of an application for Outline Planning Permission for the erection of 5No Dwellings on land adjacent to The Old Flouch Hotel.
- 1.2 The site has been the subject to recent planning permissions for the demolition of the Public House/Hotel and the erection of 5 dwellings.
 - 2011/0223 Full Planning Permission granted on 6 May 2011
 - 2012/0265 Full Planning Permission granted 30 May 2012
 - 2017/0989 Application to vary design and layout in respect of planning consent 2012/0265.
- 1.3 The current proposals for Outline Planning Permission with details of Access, Siting and Scale, are for approval for development in the form of a further 5No dwellings with garages located on the tarmac surfaced car park land to the west of the original hotel. The access used to serve the development and approved by the consent 2012/0265 will be retained and improved to adoptable standards to serve the development. The proposals are illustrated on drawing ref: P10 Rev A.
- 1.4 The proposal takes place on a previously developed site within the Green Belt and Green Belt issues are considered further in a supporting Planning Statement.
- 1.5 Construction works have started based upon the 2012/0265 and 2017/0989 planning consents at the same time as dismantling of the Old Flouch Hotel is underway.
- 1.6 These proposals have been carefully designed to make the best use of the site and mirror the previously approved development to complete a 'courtyard' arrangement of buildings which is appropriate to the semi-rural location. Consideration has been given to retaining peripheral trees and to providing additional landscaping in the form of open space that will retain a sense of openness, enhance the visual appearance of the site and avoid any loss of amenity to local residents opposite the site to the west of Whamms Road.

2.0 Location

- 2.1 The site is located at the junction of Whams Road and Old Manchester Road, Hazelhead Sheffield S30 4HH.

It is 170m north of the A628 Manchester Road which connects Barnsley to Manchester 9 miles west of Barnsley. A Site Location Plan is provided on drawing ref P10 Rev A.

3.0 Assessment

- 3.1 The site has a total area of 0.57Ha, and the current proposals (enclosed by a red line on the plans) have an area of 0.33Ha. Access is from Old Manchester Road, the details of which are similar to those previously approved but have now been improved to serve a total development of 10No dwellings.
- 3.2 The Barnsley Metropolitan Borough Council Unitary Development Plan designates the site as Green Belt. The previous use is Hotel Public House and Restaurant. There is an extant planning permission for change of use to residential, ref: 2012/0265 which establishes the principal of residential use at the site.
- 3.3 The site has a natural gradient of approximately 3 metres from the highest point at the south west down towards the north eastern corner. This natural gradient has been filled to the south and west to provide level car parks and the existing building used as a retaining structure to separate the higher levels from those at the rear (north and east).
- 3.4 There are a number of existing residential properties to the west of the site fronting Whams Road and a light industrial workshop fronting Old Manchester Road to the south. To the north and east of the site is open agricultural land. The site boundaries are formed by a mix of stone walls and timber fencing, in conjunction with limited hedges to the outer boundaries and a number of mature and semi mature trees.
- 3.5 A Topographical Survey has been prepared and a copy is provided as part of the application documents which includes details of the existing site layout, boundaries and levels.
- 3.6 A Photographic Survey has also been carried out and a copy is attached at Appendix B.
- 3.7 A Tree Survey of the existing trees and hedges on site has been commissioned and is included as part of the application documents. None of the existing trees will need to be removed to permit development although one tree has been identified as potentially hazardous and is proposed for removal.
- 3.8 A Site Investigation report s included as part of the application documents. The report identifies some areas of fill but no significant quantities of biodegradable materials were recorded. The report does however recommend that a 600mm thickness of clean capping including 150mm a minimum of 150mm of clean topsoil is used in garden areas where made ground is present. Strip foundation for new dwellings should be suitable taken through any fill to undisturbed ground.

- 3.9 Foul drainage from the existing Hotel discharges to a septic tank within land at the rear (east). It is then piped across the adjacent field to a watercourse. The discharge from the building which includes commercial kitchens, restaurants, bars and 10 ensuite bedrooms would have been considerable. The proposals will include a modern packaged sewage treatment plant to treat foul drainage from the site and utilise the existing outfall.
- 3.10 Surface water soakaway tests have not been carried out but the existing buildings and car park drain to soakaway. Proposed areas of hard surface will be reduced with the removal of the existing building and tarmac car park producing a net reduction in positive drainage. There is no anecdotal evidence to suggest that the existing surface water soakaway systems are inadequate and therefore soakaways should be suitable.
- 3.11 The site is not within an area identified by the Environment Agency as being at risk of flooding nor is it likely to lead to flooding elsewhere.
- 3.12 The existing Hotel is a large three storey building with additional attic space. To the east of the building within the car park, the height to eaves is 7.8m and 10.6m to ridge. At the rear the height to eaves is 9.9m and 12.7m to ridge. The Hotel has a gross internal floor area of 1230m²; this excludes storage space within the attic which provides a further 145m² giving a total floor area of 1375m².



The original Hotel building dominates the local landscape with the rear, three storey elevation being particularly prominent from the Manchester Road to the east as shown above.

- 3.13 Protected species surveys were carried out as part of the previous applications including emergence surveys in advance of dismantling works. The current proposals do not require the removal of any buildings and a protected species survey is not required.
- 3.14 Social and Economic Context.
The proposals have the opportunity to attract families to the area that are likely to use local schools at Thurlstone and Penistone as well as the wide range of other retail and leisure facilities available in the area. They will have a limited effect on the economic context however additional development will provide employment for construction workers and trades.
- 3.15 Local bus services link the site to Thurlstone Penistone and Langsett as well as Barnsley Holmfirth Huddersfield and Sheffield. Train services are available from Pensitone which also provide links to the wider area.

4.0 Evaluation

4.1 Use

The site is a previously used site within the Green Belt. The previous use was a Hotel Bar and Restaurant. It is within a small developed area comprising housing and light industrial use. The principal of residential use of the site has been established by the previous planning approvals.

The current proposals allow for additional development to improve the efficient use of the site by providing infill development of a previously developed site without impacting upon the openness of the Green Belt or the amenity of local residents.

4.2 Amount

The total area of the overall site is 0.57Ha and the area of the current application site which includes the access is 0.33Ha.

The proposals are for 5No dwellings with garages to increase the total on the site to 10No dwellings. There is a good mix of small and larger dwellings and a relatively low density which is appropriate to the location.

The original building on the site is a three storey hotel, bar and restaurant constructed over three storeys and with attic storage space providing a total floor area of 1375m² as outlined in para 3.12 above.

It includes extensive tarmac surfaced car parking to the frontage which also extends to the rear of the hotel.

The gross internal floor area of the buildings currently approved, Units 1 to 5 including the garages is shown below:

Unit 1	193.4 m ²
Unit 2	218.2 m ²
Unit 3	148.4 m ²
Unit 4	148.4 m ²
Unit 5	166.6 m ²
Total	875 m ²

The gross internal floor area of the additional five dwellings proposed is:

Unit 6	148.4 m ²
Unit 7	148.4 m ²
Unit 8	86.6 m ²
Unit 9	86.6 m ²
Unit 10	209 m ²
Total	679 m ²

The total gross internal floor area of all proposed dwellings on the site including garages is 1554 m²; compared to the original Hotel which results in an increase of 179m² or 11.5%.

The cumulative development is below the threshold of 15 dwellings which would qualify for an affordable housing requirement.

4.3 Layout

The additional dwellings, Plots 6-10 have been designed to mirror the arrangement of Plots 1-5 and complete a courtyard style development of dwellings of differing types and sizes that will create a sense of place which is appropriate to the semi-rural location.

Minimum distances between opposite dwellings are appropriate to the courtyard setting and all dwellings have large private gardens and amenity space. Rear gardens to Whamms Road and Old Manchester Road are protected by hedge planting to allow the existing low level boundary walls to be retained.

An informal area of Open Space has been allocated at the south west corner of the site which will allow a mature tree to be retained and additional landscape planting to be provided. It will also avoid any impact upon the existing dwellings on the opposite side of Whamms Road to the west and maintain an attractive and open outlook into the site from the junction of Whamms Road and Old Manchester Road.

Access is taken from Old Manchester Road, a cul-de-sac, from the junction with Whamms Road, and which leads to a central turning and servicing area within the centre of the development.

The proposed access is similar to that previously approved and makes provision for local footway improvements and manoeuvring of refuse and service vehicles within site.

The access road will be a shared surface designed to adoptable standards with waste and recycling bin collections being made from the edge of the access road in front of each plot.

4.4 Scale

Although there is a slight increase in the floor area of proposed buildings on the site, the height, mass and scale of the buildings is much less than the original three storey Hotel, with the majority of the buildings being two storey and a number of garages single storey.

The floor levels of the new dwellings have been reduced to suit the natural ground levels of the site therefore also reducing the scale and mass of the buildings and allowing them to sit more comfortably into the landscape. Refer to Section AA on drawing ref: P10 Rev A.

The reduction in to domestic proportions as is illustrated on the site section combined with the reduction in ground levels within the car park area helps the proposals to sit more comfortably into the landscape. This results in a development of less scale and mass than the existing hotel building which is more appropriate to the location and will make a positive contribution to the appearance and openness of the green belt.

4.5 Landscaping

Landscaping is a reserved matter however the proposed layout plan ref: P10 Rev A shows how existing mature trees will be retained and how additional planting and allocation of open space has the potential to can improve the visual amenity and soft landscaping of the site. Final details of soft and hard landscaping are to be agreed at reserved matters stage.

4.6 Appearance

Appearance is a reserved matter however we would propose that dwellings would be of good quality design with traditional wide frontage, narrow plan arrangement with steeply pitched roofs utilising a mix of two storey and single storey buildings appropriate to the local vernacular. Materials would include the use of natural stone walling and good quality artificial stone and slate for roofing materials in conjunction with casement style windows set into deep reveals. Boundaries would be a mix of natural stone walling, fencing and hedges.

5.0 Access

- 5.1 Access proposals are similar to those previously agreed and include improvements to the footpath links immediately adjoining the site to accommodate pedestrians.
- 5.2 Vehicular access is provided from the Manchester Road cul-de-sac and manoeuvring space for service vehicles is available within the site. Bin collections will be from the edge of the kerb within the turning area.
- 5.3 The majority of the site can be accessed by level approach to dwellings but given the cross falls that exist it may be necessary to design Units 6 7 8 and 9 with ambulant stepped approach.

6.0 Summary

- 6.1 The proposals are for the development of a previously developed site within the Green Belt. They include the removal of a large, dominant, uneconomic and underused building with large areas of tarmac car park and its replacement with a small scale development of traditionally designed dwellings around a courtyard setting that will make a positive contribution to the appearance and openness of the Green Belt and the special character of the countryside.
- 6.2 They provide an economical use of previously developed land with a good mix of dwelling sizes to improve the local housing and help the council to meet its target for housing.
- 6.3 The supporting Planning Statement outlines how the proposals comply with national planning policy for the redevelopment of a previously developed site including a degree of infill within the Green Belt.

As such, we look forward to receiving the councils support in respect of these proposals.

A handwritten signature in black ink, appearing to read 'M Booth', followed by a horizontal line.

Signed for MBooth Design Ltd