

2021/1644

Mr Jim Wadsworth

15 Bentham Drive, Monk Bretton, Barnsley, S71 2PE

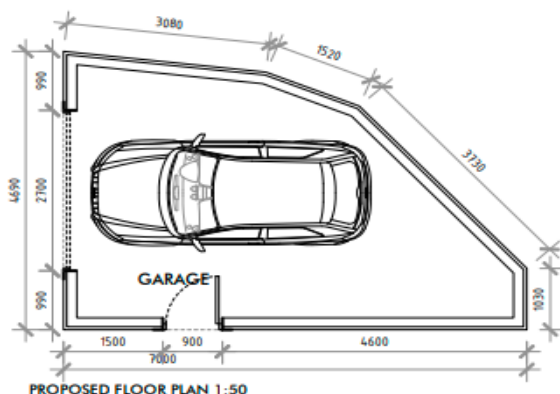
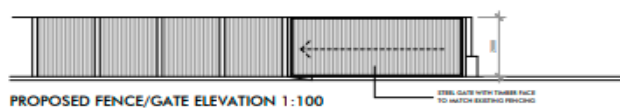
Demolition of existing outbuilding, erection of new detached garage, new entrance gates, widened dropped kerb, associated hardstanding/landscaping

Site Description

The dwelling is a two-storey semi-detached dwelling located in Monk Bretton. Bentham Drive has a consistent street scene featuring other dwellings of similar design and type. The dwelling has a driveway to the side/front (northwest) which leads to a garden area to the side/rear (southwest). To the north is an existing outbuilding (to be replaced). There is an existing boundary fence that is 2 meters in height which the applicant states to have been erected by the previous homeowner in 2016. This date would therefore make it immune to enforcement action as a fence of that size, adjacent the highway cannot be erected using permitted development rights.

Proposed Development

The applicant is seeking approval for the erection of a detached garage. The garage will be located to the north of the site. The garage has a length of 7 meters and a width of 4.69 meters. The garage will feature a flat roof with a total height of 2.90 meters and a eaves height of 2.62 meters. The materials used will be composite anthracite cladding. The proposal also includes a new entrance gate, a widened dropped kerb and associated hardstanding. The gate is to be 5.2 meters with a height of 2 meters level with the existing fencing and be a steel gate with a timber face.



REV	DATE	DESC
		
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CLIENT: MR M BAILEY		
PROJECT: 15 BENTHAM DRIVE BARNLEY S71 2PE		
DRAWING TITLE: EXISTING & PROPOSED PLANS AND ELEVATIONS		
PROJECT REF: 2021-167		
DRAWING NO: 200 REVISION:		
SCALE: AS SHOWN @ A3 DATE: NOV 2021		
 		

Policy Context

Planning decisions should be made in accordance with the current development plan policies unless material considerations indicate otherwise. The recently revised National Planning Policy Framework (NPPF) does not change the statutory status of the development plan as the starting point for decision making.

Local Plan Allocation – Urban Fabric

The Barnsley Local Plan (BLP) was adopted at the full Council meeting held 3rd January 2019. In addition, the Local Authority (LPA) has adopted a series of Supplementary Planning Documents (SPD), and Neighbourhood Plans which provide supporting guidance and specific local policies and are a material consideration in the decision-making process.

In reference to this application, the following policies are relevant:

Policy SD1: Presumption in favour of Sustainable Development – States that proposals for development will be approved where there will be no significant adverse effect on the living conditions and residential amenity of existing and future residents. Development will be expected to be compatible with neighbouring land and will not significantly prejudice the current or future use of neighbouring land. Policy GD1 below will be applied to all development.

Policy GD1: General Development – States that proposals for development will be approved where there will be no significant adverse effect on the living conditions and residential amenity of existing and future residents. Development will be expected to be compatible with neighbouring land and will not significantly prejudice the current or future use of neighbouring land. Policy GD1 below will be applied to all development.

Policy D1: High quality design and place making – Supersedes Core Strategy Policy CSP 29 and sets out the overarching design principles for the borough. Policy D1 States that development is expected to be of high-quality design and will be expected to respect, take advantage of and reinforce the distinctive, local character and features of Barnsley.

Policy T4: New Development and Highway Improvement – New development will be expected to be designed and built to provide all transport users within and surrounding the development with safe, secure and convenient access and movement.

Supplementary Planning Document: House extensions and Other Domestic Alterations

This document establishes the design principles that specifically apply to the consideration of planning applications for house extensions, roof alterations, outbuildings & other domestic alterations.

The general principles are that proposals should;

- Be of a scale and design which harmonises with the existing building;
- Not adversely affect the amenity of neighbouring properties;
- Maintain the character of the street scene; and
- Not interfere with highway safety

The above principles are to reflect the revised principles in the NPPF, which promote high quality design and a good standard of amenity for all existing and future occupants of land and buildings.

National Planning Policy Framework

The NPPF sets out the Government's planning policies and how these are expected to be applied. The core of this is a presumption in favour of sustainable development. Proposals that align with the Local Plan should be approved unless material considerations indicate otherwise. In respect of this application, relevant policies include:

Section 12: Achieving well designed places - The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. It is important to plan positively for the inclusion of high-quality design for all developments, including individual buildings, public and private spaces and wider area development schemes.

Within section 12, paragraph 134 states that *"development that is not well designed should be refused, especially where it fails to reflect local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes"*.

Consultations

Highways Development Control (DC) were consulted and raised no objections subject to conditions.

Representations

Neighbour notification letters were sent to surrounding properties, no comments were received.

Assessment

Principle of Development

The site falls within urban fabric which has no specific land allocation; however, the site and surrounding area is made up principally of housing. Alterations to residential properties are considered acceptable where they do not have a detrimental impact on the amenity of surrounding residents, visual amenity and highway safety.

Visual Amenity

The SPD states that *"detached garages should relate sympathetically to the main dwelling in style, proportions and external finishes. In most cases, it will not be appropriate for a garage to be sited between the house and the road"*. The materials used are suitable for the use. The proposed garage is to be located to the north of the dwelling between it and the highway however it relates well to the site and the original dwelling in term of design, detailing and construction materials. The garage would be set in line with existing dwelling and appears subservient to the host dwelling.

The ridge height is modest from the highest point of the ground level and maintains a balanced building in terms of the ratio of eaves to ridge heights. In terms of the siting, it is acceptable in this position due to the layout of the plot as the only location within the curtilage not in between the dwelling and the highway is the side/rear garden space. It is not considered that the garage would have a significantly detrimental impact on the character of the street scene.

The proposed gate although 2 meters tall will not have a detrimental impact upon the character of the street scene as any potential impact is negated by the impact of the existing fence to which the addition of a gate is minimal. Also, the adjoining boundary treatment with the neighbouring dwelling (17 Bentham Drive) is a hedge of a similar height. The proposed development is therefore not considered to be contrary to Local Plan Policy D1: High Quality Design and Place Making and is considered acceptable in terms of visual amenity.

Residential Amenity

The SPD states that *“detached garages should be single storey structures and the eaves height should not normally exceed 2.5m from ground level”*, the eaves height is proposed to be 0.12 meters above the recommended height however this increase is not significant enough to be detrimental. The proposed garage will not have a significant detrimental impact on any neighbouring property by way of overshadowing or being overbearing and it sits in the same footprint (albeit larger) as the existing outbuilding in between the dwelling and the highway and not another dwelling.

The garage is to be located approximately 10.5 meters away from the boundary with the neighbouring dwelling (17 Bentham Drive) and approximately 13 meters away from the boundaries with the dwellings on the opposite side of Bentham Drive. With the proposed total height being 2.9 meters, this is not deemed to have a detrimental effect. The garage will also be partially screened by the existing fencing that is 2 meters in height. The proposal is considered to be in compliance with Local Plan Policy GD1: General Development and is acceptable in terms of residential amenity.

Highway Safety

There will be no impact upon highway safety. The 2-meter-high boundary fence (although not a part of this application) along much of the property frontage onto Bentham Drive is not impactful as the boundary of the property is splayed as to allow for forward visibility around the bend of the unmarked junction. Therefore, the height of the fence will not have a detrimental impact. A widened access with a sliding gate at the same height as the fence is proposed, this is sufficient to provide adequate pedestrian intervisibility. Ample off-street parking will be retained. It is therefore considered that the proposals do not adversely impact upon the highway and are acceptable in terms of highway safety.

Recommendation

Approve with conditions