
2023/1071

Mr Ryan Bower

2 Norman Close, Worsbrough, Barnsley, S70 5QB

Conversion of existing garage into residential annexe, erection of single storey extension to rear of existing garage and dwelling and erection of porch to front of dwelling (Amended Description).

Site Description

The application relates to a plot located on the eastern side of Norman Close and in an area that is principally residential, comprising a mix of dwelling types, including two-storey detached properties and bungalows with pitched and gable roof types.

The property in question is a two-storey detached dwelling constructed of brick with a pitched roof that uses grey coloured roof tiles. An existing flat roof attached garage is located to the north of the dwelling and is fronted by a driveway and some soft landscaping bounded by a low brick wall. To the rear of the dwelling is a stepped garden set above the ground level of the dwelling that is bounded by timber panelled fencing.



Planning History

There is one previous planning application associated with the application site:

1. B/95/0337/WB – Erection of side attached garage. – Approved.

Proposed Development

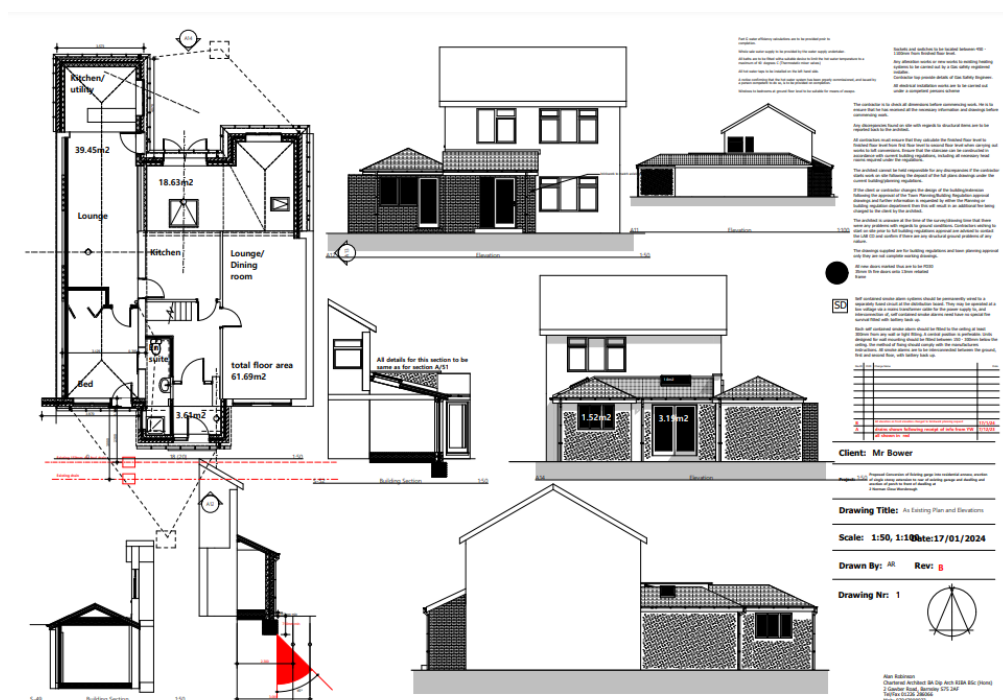
The applicant is seeking approval for the erection of a porch to the front of the dwelling, a single storey extension to the rear of the dwelling, a single storey extension to the rear of an existing attached garage and the conversion of the garage into a residential annexe.

The proposed porch would project from the front elevation of the dwelling by approximately 1.5 metres and have a width of approximately 3.7 metres. The porch would adopt a mono pitched roof with an approximate eaves and ridge height of 2.6 metres and 3.4 metres respectively.

The proposed extension to the rear of the dwelling would project from the rear elevation by approximately 4 metres and would have a total width of approximately 6.5 metres. The extension would adopt mono pitched and hipped roof types with an approximate eaves and ridge height of 2.6 metres and 3.4 metres respectively.

The proposed extension to the rear of the attached garage would project from the rear elevation by approximately 3 metres and have a width of approximately 3.6 metres. A new hipped roof would be erected atop the existing garage and proposed extension to the garage with an approximate eaves and ridge height of 2.6 metres and 3.5 metres respectively.

Brickwork would be retained on the front wall of the attached garage and would be used in the construction of the proposed porch. The proposed rear extensions to the garage and dwelling would be finished in a light-coloured render. Grey concrete roof tiles will be used throughout.



Policy Context

Planning decisions should be made in accordance with the current development plan policies unless material considerations indicate otherwise; the National Planning Policy Framework (NPPF) does not change the statutory status of the development plan as the starting point for decision making. The Local Plan was adopted in January 2019 and is now accompanied by seven masterplan frameworks which apply to the largest site allocations (housing, employment, and mixed-use sites). In addition, the Council has adopted a series of Supplementary Planning Documents and Neighbourhood Plans which provide supporting guidance and specific local policies which are a material consideration in the decision-making process.

The Local Plan review was approved at the full Council meeting held 24th November 2022. The review determined that the Local Plan remains fit for purpose and is adequately delivering its objectives. This means, no updates to the Local Plan, in whole or in part, are to be carried out ahead of a further review. The next review is due to take place in 2027, or earlier, if circumstances require it.

Local Plan Allocation – Urban Fabric

The application site is allocated as urban fabric within the adopted Local Plan which has no specific land allocation. Therefore, the following policies are relevant:

- ***Policy SD1: Presumption in favour of Sustainable Development.***
- ***Policy GD1: General Development.***
- ***Policy D1: High quality design and place making.***
- ***Policy T4: New Development and Transport Safety.***

Supplementary Planning Document: House Extensions and Other Domestic Alterations

This document establishes the design principles that specifically apply to the consideration of planning applications for house extensions, roof alterations, outbuildings & other domestic alterations; reflecting the principles of the NPPF, which promote high quality design and a good standard of amenity for all existing and future occupants of land and buildings.

Supplementary Planning Document: Parking

This document establishes parking standards in relation to development size and type.

National Planning Policy Framework

The NPPF sets out the Government's planning policies and how these are expected to be applied. The core of this is a presumption in favour of sustainable development. Proposals that align with the Local Plan should be approved unless material considerations indicate otherwise. In respect of this application, relevant policies include:

- ***Section 12: Achieving well-designed and beautiful places.***

Other Material Considerations

South Yorkshire Residential Design Guide 2011.

Consultations

Highways DC – No objection subject to condition.

Representations

Neighbour notification letters were sent to surrounding properties and no representations were received.

Assessment

Principle of Development

Extensions and alterations to a domestic property are acceptable in principle provided that they remain subsidiary to the host dwelling, are of a scale and design which is appropriate to the host property and are not detrimental to the amenity afforded to adjacent properties, including visual amenity and highway safety.

Residential Amenity

Proposals for extensions and alterations to a domestic property are considered acceptable provided that they would not adversely affect the amenity of neighbouring properties.

The proposed porch extension would be erected to the front of the host dwelling and would be located to the north of 1 Norman Close and to the south of 3 Norman Close. The porch is unlikely to result in significantly increased levels of overshadowing as it would adopt a modest projection and restrained roof height, it would be set in from adjacent side boundaries, and would be set down from 3 Norman Close where the greatest impact would have otherwise been likely to occur. The porch would feature minimal glazing limited to its front elevation that would serve an internal space that is unlikely to be in constant use throughout the day.

The proposed extension to the rear of the host dwelling would be erected adjacent to the southern boundary line. It is acknowledged that some overshadowing could occur, especially as the extension would be set higher due to level differences. However, the extension would be located to the north of 1 Norman Close and would be erected immediately adjacent to an existing detached garage within the curtilage of the neighbouring property, therefore lessening any potential impact. Additionally, the extension would adopt a relatively modest rearwards projection and restrained roof height, in accordance with the SPD. The extension would feature minimal glazing limited to its rear elevation that would face into the application site and away from surrounding properties. A sufficient separation distance would be maintained to the rear boundary, in accordance with the SPD, and the raised garden level could provide further mitigation.

The proposed extension to the rear of the attached garage would be erected adjacent to the northern boundary line. It is acknowledged that some overshadowing could occur, especially as a new hipped roof would be erected atop the garage and extension and it would be located to the south of an adjacent neighbouring property. However, the extension would be set down from 3 Norman Close due to level differences and an existing boundary fence could offer some mitigation, therefore lessening any potential impact. Additionally, the extension would adopt a relatively modest scale, including roof height. The extension would feature minimal glazing limited to its southern side elevation that would face into the application site and away from surrounding properties. As such, it is not considered that the proposal would result in significantly increased levels of overshadowing, overlooking or reduced outlook.

The proposed residential annexe would comprise three rooms in total, including one bedroom. The annexe would maintain a close physical relationship with the host dwelling as it would be facilitated by the conversion of an existing attached garage and an internal doorway between the annexe and

the host dwelling would be created. The annexe would not have its own separate access or garden, in accordance with the SPD. The use of the annexe as ancillary accommodation for immediate family members will be conditioned.

The proposal is therefore considered to comply with *Local Plan Policy GD1: General Development* and would be acceptable regarding residential amenity.

Visual Amenity

The proposed porch and front elevation of the attached garage would be visible from the public realm of Norman Close and would be prominent features within the street scene.

There are limited examples of existing porches within the street scene of Norman Close. However, the proposed porch to the front of the host dwelling would be modest in scale, sufficiently discreet, and would adopt a form and features that would be sympathetic to the host dwelling, including external materials. The proposed porch would also adopt a scale and appearance that would be similar to an existing porch at adjacent 1 Norman Close.

The existing attached garage is proposed to be converted to a residential annexe. The existing garage door would be replaced with a new door and window to provide a separate access from the main dwelling. The access to the proposed annexe would be set back from the access of the main dwelling which is proposed to be relocated to the front elevation of the porch. As such, the access to the annexe would appear sufficiently discreet and secondary and is unlikely to detract from the significance of the character of the main dwelling. Additionally, the front wall of the garage would maintain a brick construction that would match or be of a similar appearance to the main dwelling.

A new hipped roof would be erected atop the proposed annexe and would extend to cover a proposed extension to the rear. Whilst an alternative roof type would have been preferred to ensure that a close relationship would be maintained with the main dwelling, it is acknowledged that the proposed hipped roof is a significant improvement and is likely to contribute more positively to the character of the street scene and would ensure that existing windows located on the north side elevation of the dwelling could be retained. Additionally, the roof would use grey coloured concrete roof tiles that would match or be of a similar appearance to the roofing materials of the main dwelling.

The proposed extensions to the rear of the garage and to the rear of the main dwelling would be relatively modest in scale and would adopt a form and features that would be sympathetic to the original dwelling. The rear extensions would be finished in a light-colour render; however, the extensions would not be visible from the public realm of Norman Close and would therefore have no impact on the character of the street scene.

The proposal is therefore considered to comply with *Local Plan Policy D1: High Quality Design and Placemaking* and would be acceptable regarding visual amenity.

Highway Safety

Three-bedroom properties or larger are expected to provide a minimum of two off-street parking spaces within the curtilage of the dwelling. It is acknowledged that the proposed conversion of an existing garage into living space would reduce the amount of off-street parking currently available. However, the existing driveway would be extended to accommodate a minimum of two vehicles to the front of the property. Additionally, Highways DC were consulted on the application and raised no objection.

The proposal is therefore considered to comply with *Local Plan Policy T4: New Development and Transport Safety* and would be acceptable regarding highway safety.

**Recommendation -
Approve with Conditions**