

### 3.4 Connectivity Strategy

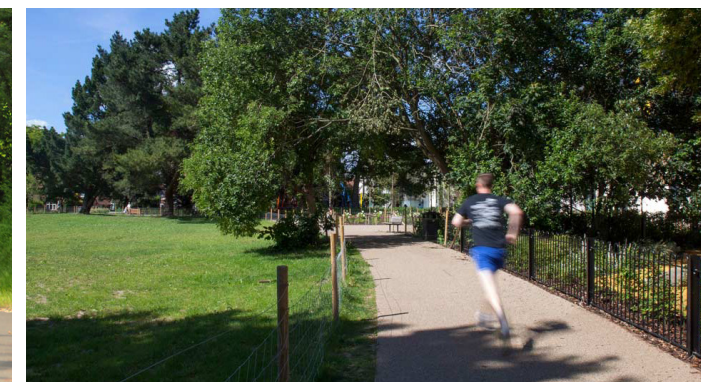
The pedestrian and cycle network promotes active travel within the development. The internal path network is well linked to the wider public rights of way and connects the new neighbourhoods with the existing surrounding communities of Higham, Barugh Green, Gawber and Pogmoor.

The strong pedestrian and cycle connection through the development aids both permeability and navigation and offers safe routes to schools, local facilities and the wider footpath network.

Active travel routes form strategic links through the open spaces and employment site with interconnecting footpaths. Where strategic links pass through development areas, segregated cycle lanes will be considered to ensure safe and direct routes for all.

Key principles of the connectivity strategy include:

- The landscape masterplan has been developed to maximise pedestrian and cycle permeability and provide horse-riding routes through the site.
- Key-off road/ segregated active travel routes are designed for pedestrians, cyclists, skaters, wheelchair users, joggers and other users, with a minimum width of 3m. The maximum gradient is 1:21, while 1:14 (7%) may be acceptable for steeper contour for the distance up to 30m. It should be in line with LTN1/20 Cycle Infrastructure Design and Sustrans guidance on traffic-free routes.
- On-road active travel routes have minimum width of 2m footways along both side of the carriageway and on-street cycling due to low traffic flows.
- Recreational routes with minimum 2m width are mostly for pedestrians with steps in steeper areas. 300mm tread depth with min. 250mm and 130-150mm height of riser with max. of 170mm is preferred for steps. Max. number of steps in one flight is 12 with resting places every 12 steps.
- Existing connections into the site are enhanced by creating improved 'gateways' into the scheme, such as seen along the western edge of site in which the development introduces 1:21 sloped footpaths which connect the new developments to the existing PROW network
- The site has a number of embankments at 1:3 slope (or steeper) which have level terraces shown along them. Where these have been shown new multi user routes have been introduced to aid the access permeability of site.



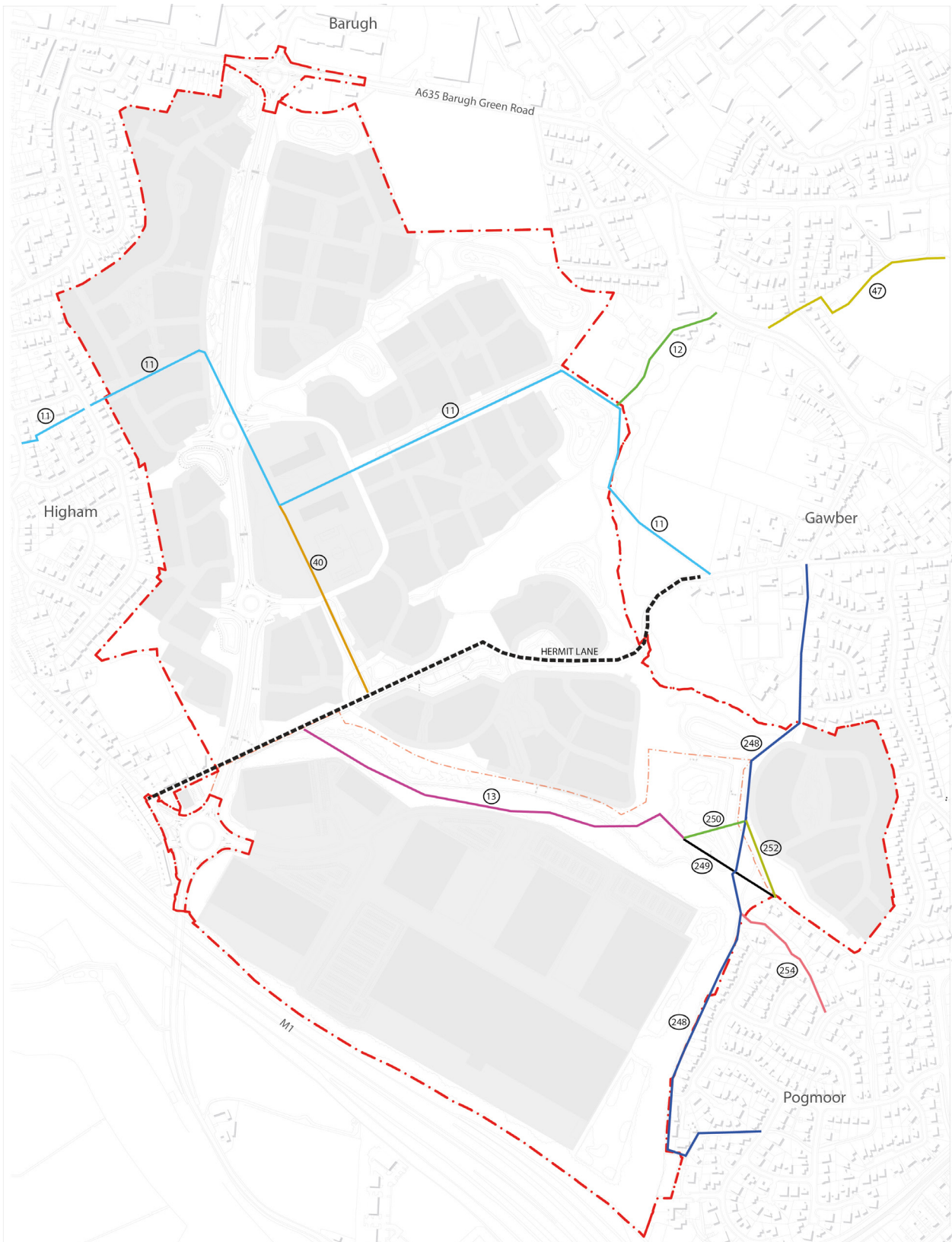


# 3.5 Public Rights Of Way (PRoW)

## 3.5.1 EXISTING PROW NETWORK

The following plan represents the existing PROW network.

A number of the existing PROW routes with the site will be impacted by the masterplan development, however these routes will be incorporated into the layout and enhanced through the delivery of the landscape proposals.



# 3.5 Public Rights Of Way (PRoW)

## 3.5.2 PROPOSED PROW NETWORK

The masterplan has been developed to ensure PRoW access and connectivity are enhanced across the site.

All PRoW outside of the application boundary will remain as existing and sections of routes 11, 40, 13, 248, 249, 250 and 252 will require diversions to tie into the wider landscape proposals.

The Hermit Lane will be retained as a bridleway between the eastern boundary and the new link road. There are also opportunities for routes 13, 248 and 249 to be improved to a bridleway; providing links to Pogmoor and west of Hermit lane. A new alignment will be provided to ensure gradients are suitable with a safe crossing over the link road. The bridleway will connect back into Hermit Lane, west of the application boundary.

