

2023/0755

Applicant: Mrs Arnold

Agent: JE Architectural Ltd

Description: Subdivision of existing townhouse to 2no. one bed flats and a studio flat with external alterations including rooflight, rear dormer and external staircase.

Address: 212 Hough Lane, Wombwell, Barnsley, S73 0EX

Site Description

212 Hough Lane is a traditional two-storey building, sited to the north of Hough Lane, and constructed in smooth red facing brickwork with a mix of stone heads and sills to openings to front elevation and slender stone sills and arched brick heads to rear elevation under a natural blue-grey slate roof with white PVC windows and black composite entrance/rear doors.

In terms of the surrounding area the building is surrounded by mostly residential development consisting of traditional linear terraced housing and more contemporary semi-detached houses of differing tenures. The adjacent building has been converted into a small Chinese takeaway.

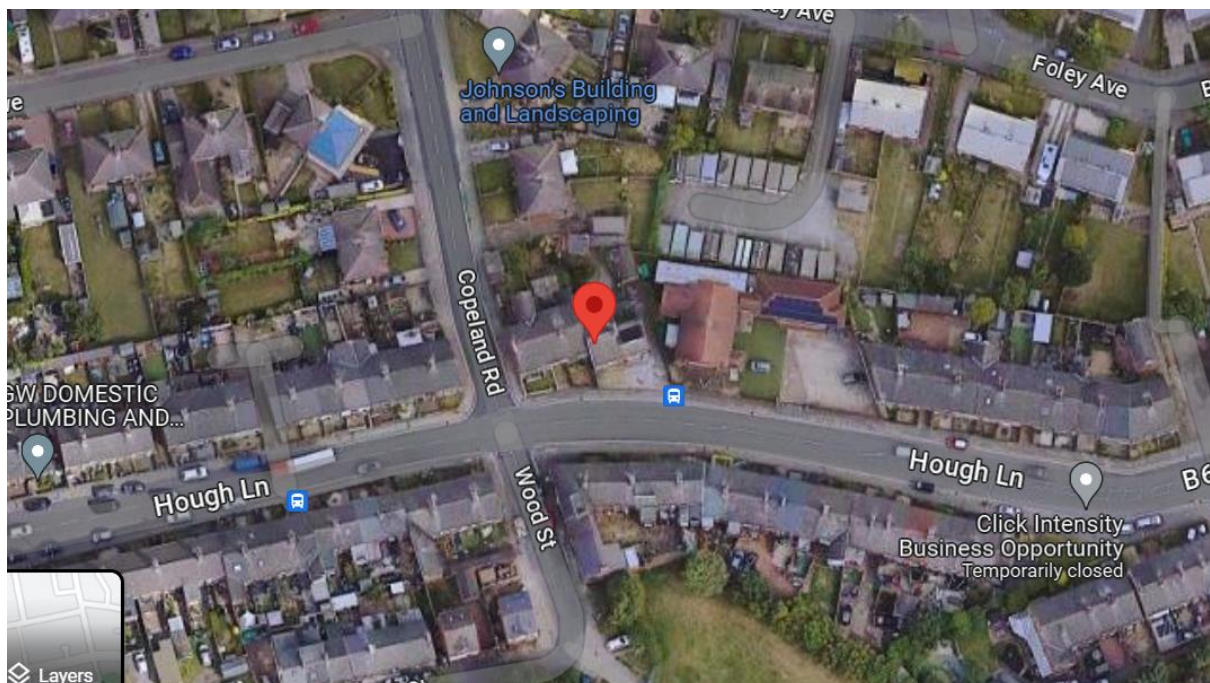
The site is not within a Conservation Area, nor is it within the Green Belt.



View from the south-east.



View from the south-west.



Google Maps aerial view of the site

Relevant Site History

No planning history

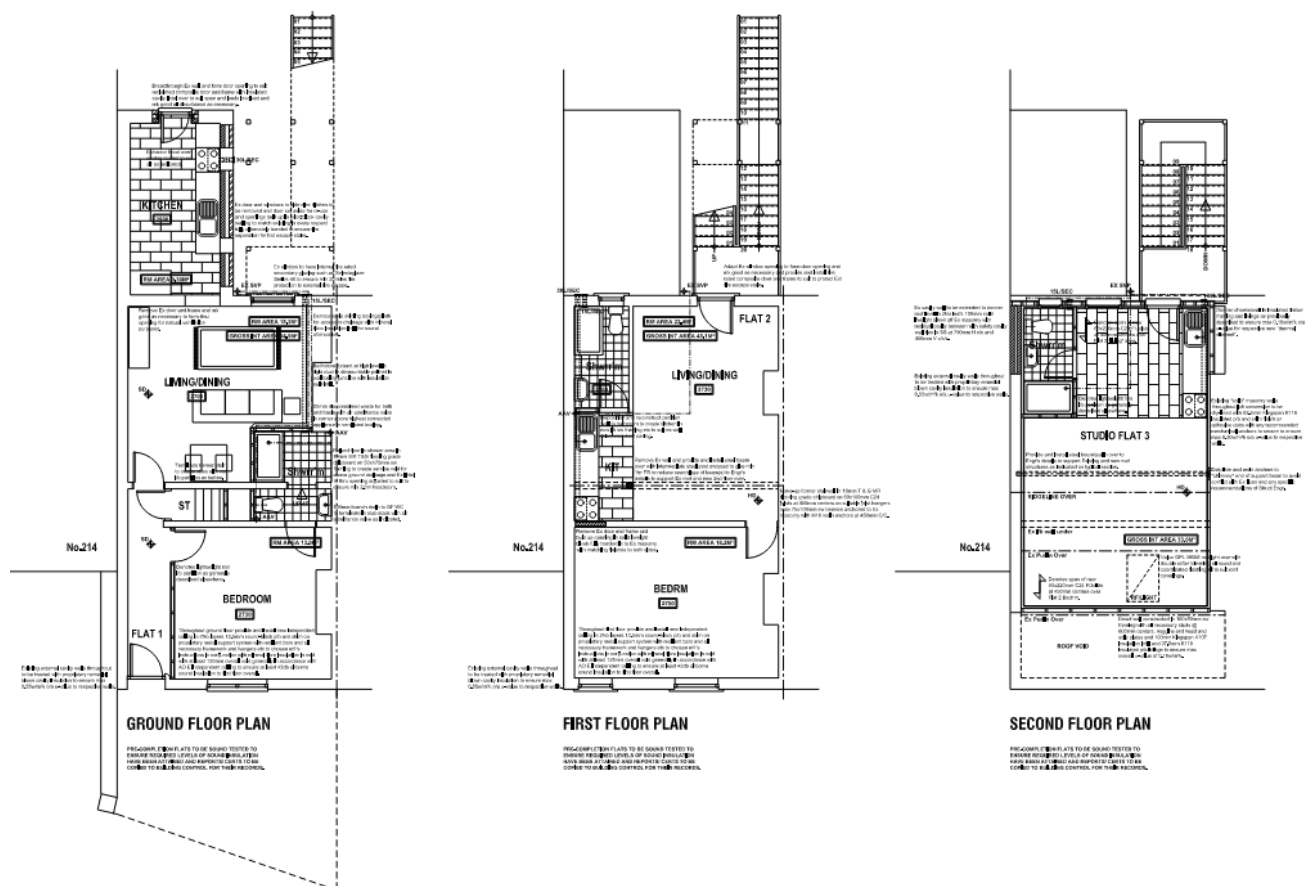
Proposed Development

The applicant is seeking approval to subdivide the existing townhouse to 2no. one bed flats and a studio flat with external alterations including rooflight, rear dormer and external staircase.

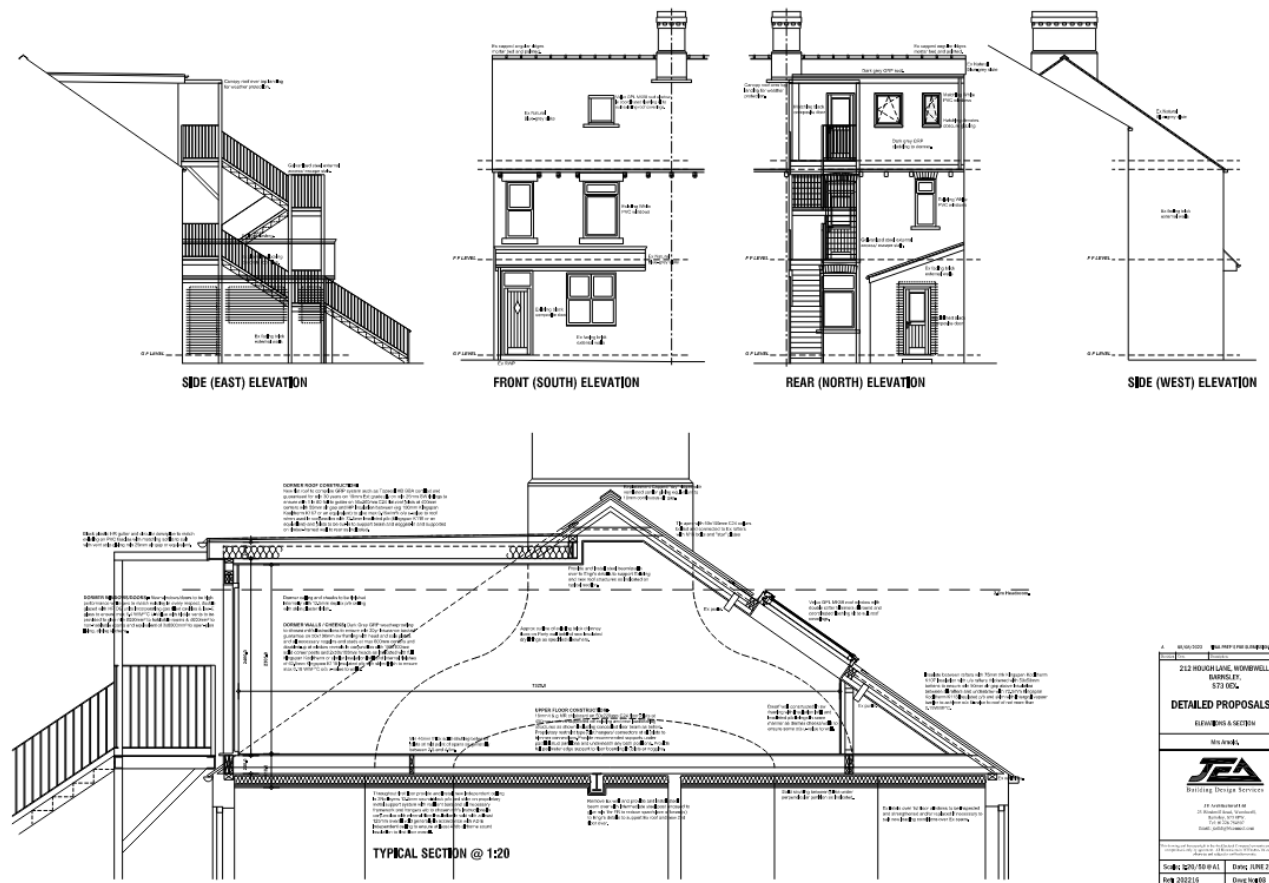
It is proposed to horizontally subdivide and alter the townhouse to create 2No. 1 bed flats at ground and first floor level and an open-plan studio flat in the proposed dormer loft conversion. The flats will be accessed via the existing entrance door and hallway off Hough Lane and from a new external stair at rear off the shared yard/amenity area.

External alterations to the design, appearance and scale of the building are limited to the dormer to the rear (north) elevation, the addition of a roof window to the front (south) elevation and some alterations to existing openings on the rear/side elevations to enable the subdivisions and to fire protect the external stairs for Building Reg's purposes. The rear yard is part flagged and part landscaped and on part of the hard-landscaped area a shared bin store will be created.

The townhouse does not enjoy off street parking currently and no parking is proposed to be provided.



Proposed Floor Plans



Proposed Elevations and Section

Policy Context

Planning decisions should be made in accordance with the development plan unless material considerations indicate otherwise and the NPPF does not change the statutory status of the development plan as the starting point for decision making. The Local Plan was adopted in January 2019 and is also now accompanied by seven masterplan frameworks which apply to the largest site allocations (housing, employment and mixed-use sites). In addition, the Council has adopted a series of Supplementary Planning Documents and Neighbourhood Plans which provide supporting guidance and specific local policies and are a material consideration in the decision-making process.

The Local Plan review was approved at the full Council meeting held 24th November 2022.

The review determined that the Local Plan remains fit for purpose and is adequately delivering its objectives. This means no updates to the Local Plan, in whole or in part, are to be carried out ahead of a further review. The next review is due to take place in 2027 or earlier if circumstances, require it.

Local Plan

In reference to this application, the following policies are relevant:

GD1 – General Development – Development will be approved if there will be no significant adverse effect on the living conditions and residential amenity of existing and future residents.

D1 – High Quality Design and Place Making – Development is expected to be of a high-quality design and will be expected to respect, take advantage of and reinforce the distinctive, local character and other features of Barnsley.

T3: New Development and Sustainable Travel – New Development will be expected to:

- Be located and designed to reduce the need to travel, be accessible to public transport and meet the needs of pedestrians and cyclists.
- Provide at least the minimum levels of parking for cycles, motorbikes, scooters, mopeds and disabled people set out in the relevant Supplementary Planning Document.

T4: New Development and Transport Safety – New development will be expected to be designed and built to provide all transport users within and surrounding the development with safe, secure and convenient access and movement.

Poll1: Pollution Control – Development will be expected to demonstrate that it is not likely to result, directly or indirectly, in an increase in air, surface water and groundwater, noise, smell, dust, vibration, light or other pollution which would unacceptably affect or cause a nuisance to the natural and built environment or to people. Developers will be expected to minimise the effects of any possible pollution and provide mitigation measures where appropriate.

H4: Residential Development on Small Non-allocated Sites – Proposals for development on sites below 0.4 hectares (including conversions of existing buildings and creating dwellings above shops) will be allowed where the proposal complies with Other Relevant Policies in the Plan.

H6: Housing Mix and Efficient Use of Land – Housing proposals will be expected to include a broad mix of house size, type and tenure to help create mixed and balanced communities. Proposals to change the size and type of existing housing stock must maintain an appropriate mix of homes to meet local needs.

H9: Policy H9 Protection of Existing Larger Dwellings Development within the curtilage of existing larger dwellings will be resisted where it will have an adverse impact on the setting of the original dwelling, and the size of the remaining garden area. The loss of existing larger dwellings will be resisted. Support will be given to the re-establishment of Houses in Multiple Occupation into single family sized houses.

Supplementary Planning Documents

- Design of housing development
- Parking
- Sustainable Travel

NPPF

The National Planning Policy Framework sets out the Government's planning policies for England and how these are expected to be applied. At the heart is a presumption in favour of sustainable development. Development proposals that accord with the development plan should be approved unless material considerations indicate otherwise. Where the development plan is absent, silent or relevant policies are out-of-date, permission should be granted unless any adverse impacts of doing so would significantly and demonstrably

outweigh the benefits, when assessed against the policies in the Framework as a whole; or where specific policies in the Framework indicate development should be restricted or unless material considerations indicate otherwise.

In respect of this application, the policies above are considered to reflect the 4th Core Principle in the NPPF, which relates to high quality design and good standard of amenity for all existing and future occupants of land and buildings. They also reflect the advice in paragraph 124 (general design considerations) and paragraph 130, which state that 'permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions'.

Other material considerations

South Yorkshire Residential Design Guide - 2011

Consultations

Highways DC – Object due to substandard parking.

Ward Councillors – Object on the basis that there is no parking and the dwelling should remain as a family home.

Drainage – Details to be checked by Building Control

Pollution Control – No objection subject to conditions

Yorkshire Water- No comment received.

Representations

Neighbour consultation letters were sent to neighbouring residents. 1 representation has been received objecting to the proposal, which is summarised as follows:

- Nowhere for residents to park.
- Deliveries could cause traffic problems.

Assessment

Principle of development

The site is located in the Urban Fabric with residential being the long-established use of the area. The proposed development is to sub-divide the existing residential dwelling into 2no. one bed flats and a studio flat, which does not change the use of the building. Local Plan Policy H4 states that proposals for residential development on sites below 0.4 hectares (including conversions of existing buildings and creating dwellings above shops) will be allowed where the proposal complies with other relevant policies in the Plan.

The above being said, assessment against Local Plan Policy H6 is also necessary which requires the Local Planning Authority to take a judgement as to whether the proposal will contribute successfully to achieving an appropriate mix of homes to meet local needs. The proposal would result the conversion of the existing 3-bedroom dwelling into 2no one bed flats and a studio flat. The proposal would not result in the loss of a larger home (4 bed or more) and would not be in conflict with policy H9 of the Local Plan. It would provide additional smaller units of accommodation which would enhance the overall mix of housing in the area. The proposal is therefore considered acceptable when measured against policy H6 of the Local Plan, however it is subject to an assessment with other policies as below.

Visual Amenity:

The scheme proposes a number of external alterations, including a rooflight, rear dormer and external staircase. The rooflight is a minor amendment, considered to be permitted development and therefore acceptable.

The House Extensions and Other Domestic Alterations SPD states that the design of the dormer window should reflect the character of the area, the surrounding buildings and the age and appearance of the existing building.

The dormer should not dominate existing roof lines and should retain its original form and as such the SPD states that dormers should be set within the roof plane and not be built off an external wall. The proposed dormer sits within the roof pane and the top of the dormer is set down below the ridge by 0.57m in accordance with the SPD. The SPD states that dormers should be at least 0.5m away from party walls with adjacent properties, however this is not complied with in this instance. Furthermore, the windows are not positioned to line through vertically with the openings below. In terms of materials, the dormer would be constructed from dark grey cladding and a grey GRP roof. This would not be in keeping with the existing building and would be contrary to the SPD which states that cladding to the front of a dormer should be minimised. As such, the dormer does not comply with the SPD, would be at odds with the existing building and would be seen from dwellings and public vantage points to the rear of the dwelling and not assimilate well with the existing area. It is therefore poor design which is unacceptable in this instance. It is noted that the adjacent dwelling currently benefits from a front dormer, however this is not a recent addition to the building and there have been updated policies in place as discussed above since the adjacent dormer was constructed.

In terms of the staircase, it appears to be clunky and would dominate the rear of the property and obscured views may be possible from the adjacent dwelling. Policy D1, High Quality Design and Place Making, states that design should respect, take advantage of and reinforce the distinctive local character and features of Barnsley. The staircase is not in accordance with this policy, being again at odds with the existing building and local area. It is considered that a different approach could have been taken to obscure the staircase or alternatively an internal design solution should have been considered.

As such, the proposal introduces design which is not acceptable and would impact on the visual amenity of the property itself and surrounding area, contrary to the House Extensions and Other Domestic Alterations SPD and policy D1 of the Local Plan.

Residential amenity

The submitted floor plans show that the overall units meet the overall floor areas as stipulated by the guidance in the South Yorkshire Residential Design Guide, however there is concern with individual room sizes and the amount of adequate daylight and sunlight the units are going to receive.

Flat 1's kitchen is undersized at 9sqm when it should be 13sqm. The living/dining room does not benefit from a window and for a main habitable room which would be arguably used mostly within the flat it would not be acceptable to have no natural daylight/sunlight. Flat 2's kitchen is incredibly small at 3.84sqm, almost 10sqm smaller than the stipulated standard. Again, the living/dining room does not have a window and nor does the kitchen. Flat 3 has not denoted all the rooms, however the kitchen is small and it appears any living space/bedroom would be served by a single rooflight which would not be acceptable. It is considered that the internal layout and lack of windows would lead to inadequate living standards for future occupiers which would not be in accordance with policy.

The SPD for Design of Housing Development requires at least 50sqm for one residential flat plus an additional 10sqm as balcony space or added to the shared private space per unit. In terms of the external amenity space, presently the flat has an area of private amenity space to the rear yard. The covering letter states that the yard will be part flagged and part landscaped to allow for both amenity space and a bin store. The larger area to be used as amenity space measures at approximately 52sqm. Whilst this falls short of the recommended 70sqm for the three flats, there is a small area of amenity space to the front of the proposed flats which would measure 21sqm. Furthermore, the flats are within 0.2 miles of Wombwell Park which justifies the small shortfall. The proposal therefore provides an acceptable amount of external amenity space.

In terms of the residential amenity of future occupiers, the existing use of the building would not change and the existing dwelling has habitable openings to both front and rear elevations which are carried over to this proposal. Notwithstanding this, there is adequate separation to the dwelling opposite the road from the proposed site of 16sqm (should be 12m as stipulated in the Design of Housing SPD) and to the rear there is only one residential property (59 Copeland Rd) which appears to have non habitable windows. In terms of additional disturbance, the proposal includes the addition of two further units, which will be accessed via a new external staircase. The dwellings are for one occupier only and the existing house could have had up to 4/5 people and as such this should not cause any significant disturbance to the adjacent attached dwellings over and above what has already been experienced at the existing dwelling.

Whilst some elements of the proposal are acceptable, individual room standards are not met and there is a lack of window openings to habitable rooms. The proposed development is therefore unacceptable when measured against the Council's SPD for New Housing Development in that it provides insufficient internal and external amenity. The proposal is therefore unacceptable when measured against Local Plan Policy GD1 which seeks to protect the living conditions of future residents.

Highways Safety

The proposed development does not comply with Local Parking Standards, specifically the Council's SPD for Parking which required at least one parking space per unit.

Highways have been consulted on the proposal. The proposal to convert the existing three-bedroomed dwelling to three single-bed flats would increase the requirement for off-street parking provision from two spaces to three. At present, the site does not benefit from any suitable off-street parking; even the small area of hard standing to the front of the property would not be accepted as a parking space to modern standards as it would require a vehicle to reverse on or off a B-classified road.

There does not appear to be any scope to improve the parking provision of the site, and Hough Lane is protected by a "no waiting at any time" restriction at this point, meaning there is no opportunity for residents or their visitors to park on street.

Each flat would require refuse storage, and a suitable bin store has not been demonstrated in the submitted plans, however this could be conditioned as per any approval.

It should be noted that splitting the home into three flats is likely to significantly increase the number of visitors and service and delivery vehicles to that site. Also, that supplying fewer than three off-street spaces would leave at least one dwelling lacking in parking provision.

The parking facilities for the site are already substandard when assessed to modern standards, and Highways DC officers would not wish to see this exacerbated by increasing

the demand for parking at this location. As such, the proposal does not accord with Policy T4 of the Local Plan, New Development and Transport Safety, due to inadequate parking.

Recommendation

Refuse