
2021/0425

Mr G Burns

Demolition of existing stables and equestrian storage buildings and erection of 1 no dwelling (resubmission)

Land at Barnsley Road, Brierley, Barnsley, S72 9LQ

Site description

The site is located to the North-Eastern edge of the village of Brierley with the entrance off Frickley Bridge Lane. The site in total measures around 3 hectares and is sited within the Green Belt. The majority of the site is used for stables/equestrian facilities and there is one ad-hoc building on site, built from timber, housing the above. Due to the designation of the site, it is relatively open with green fields to the North and East but there are residential dwellings to the South and West of the site.

There is an existing livery business on site.



Site History

B/85/1129/HR – Erection of bungalows and garage (approval of reserved matters) – Approved

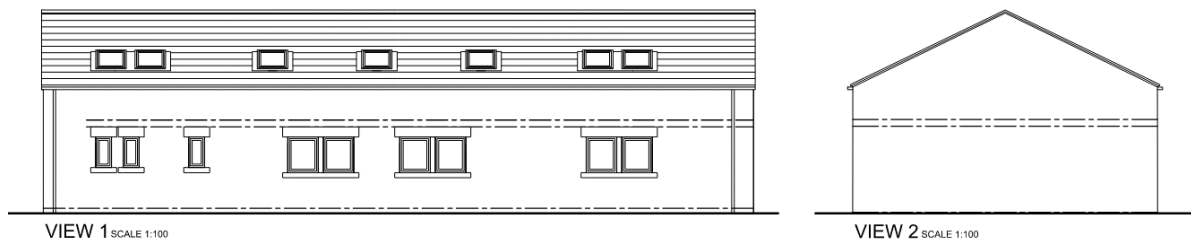
2020/1132 – Erection of 1no. dwelling following demolition of existing stables and equestrian storage buildings – Approved

Proposed Development

The applicant is seeking approval for the erection of 1 detached residential dwelling following the removal of the existing stables/equestrian storage facilities. The proposed dwelling will take the shape of a detached bungalow with rooms in the roof space. The existing access into the site will be used off Frickley Bridge Lane and a smaller curtilage will be formed within the site with the majority of it remaining in agricultural use.

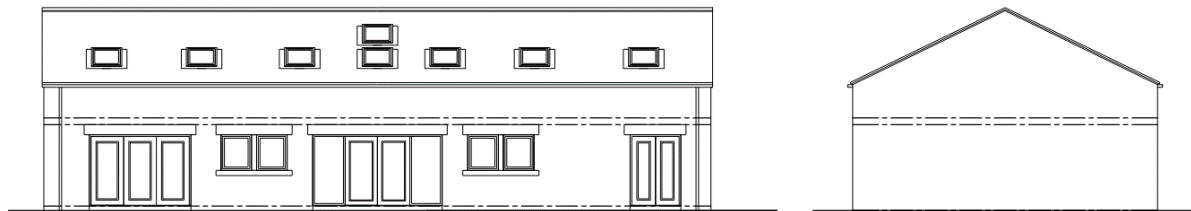
The dwelling is to measure 9m by 19m with a height of 3.6m to the eaves and 6m to the ridge, utilising room within the roof space. It is to provide living accommodation at ground floor level and 4 bedroomed sleeping accommodation within the roof space.

The garage is to measure 6m by 6m with a height to the eaves and ridge of 2.4m and 4m respectively.



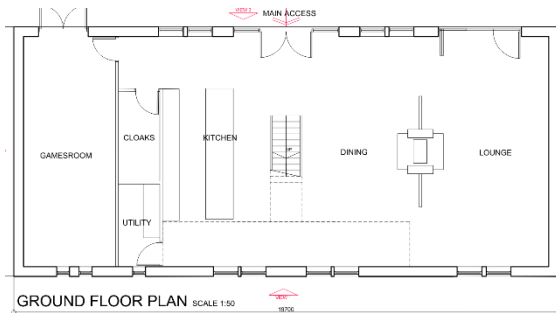
VIEW 1 SCALE 1:100

VIEW 2 SCALE 1:100

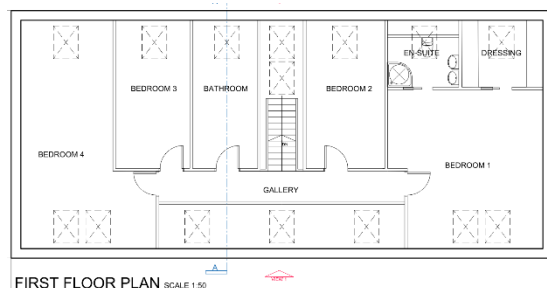


VIEW 3 SCALE 1:100

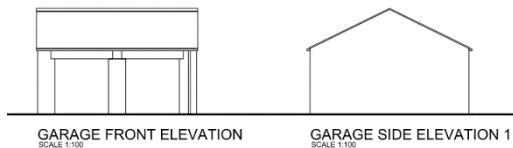
VIEW 4 SCALE 1:100



GROUND FLOOR PLAN SCALE 1:50

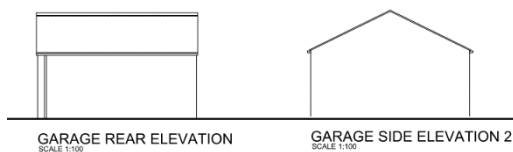


FIRST FLOOR PLAN SCALE 1:50



GARAGE FRONT ELEVATION SCALE 1:100

GARAGE SIDE ELEVATION 1 SCALE 1:100



GARAGE REAR ELEVATION SCALE 1:100

GARAGE SIDE ELEVATION 2 SCALE 1:100

Policy Context

The new Local Plan was adopted at the full Council meeting held 3rd January 2019 after it was found to be sound by the appointed Planning Inspector following the examination process. This means that it now takes on full weight for decision making process in planning law terms as the development plan for the Borough, superseding the remaining saved policies from the Unitary Development Plan (adopted in the year 2000) and the Core Strategy (adopted in 2011). In reference to this application, the following policies are relevant:

The site is allocated as Green Belt within the Local Plan Proposals Maps and therefore the following policies are relevant:

Policy GB1 Protection of Green Belt
Policy T4 New development and Transport Safety
Policy GD1 General Development
Policy D1 High Quality Design and Place Making.
Policy Poll1 Pollution Control and Protection
Policy BIO1 Biodiversity and Geodiversity
Policy CC2 Sustainable Design and Construction
Policy SD1 Presumption in favour of Sustainable Development

SPDs:

Supplementary Planning Document – Design New Housing Development
Supplementary Planning Document – Residential Amenity and Siting of Buildings
Supplementary Planning Document – Parking
Supplementary Planning Document – Sustainable Travel

Other:

South Yorkshire Residential Design Guide

NPPF

The National Planning Policy Framework sets out the Government's planning policies for England and how these are expected to be applied.

At the heart is a presumption in favour of sustainable development. Development proposals that accord with the development plan should be approved unless material considerations indicate otherwise. Where there are no relevant development plan policies, or the policies which are most important for determining the application are out of date, unless the application of policies in the Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed or any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies of the Framework taken as a whole.

Paragraphs 147 - 150 of the NPPF are relevant to this application:

Proposals affecting the Green Belt
Inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances.

When considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will

not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

A local planning authority should regard the construction of new buildings as inappropriate in the Green Belt, however there are exceptions to this, which amongst others include:

g) limited infilling or the partial or complete redevelopment of previously developed land, whether redundant or in continuing use (excluding temporary buildings), which would

- Not have a greater impact on the openness of the Green Belt than the existing development or
- Not cause substantial harm to the openness of the Green Belt, where the development would re-use previously developed land and contribute to meeting an identified affordable housing need within the area of the local planning authority

Previously developed land is defined within the NPPF as: Land which is or was occupied by a permanent structure, including the curtilage of the developed land (although it should not be assumed that the whole of the curtilage should be developed) and any associated fixed surface infrastructure. This excludes land that is or was last occupied by agriculture or forestry buildings.

Consultations

Highways DC – No comments received

Pollution Control – No objections

Air Quality Officer – No objections subject to inclusion of electric vehicle charging point

Highways Drainage – No objection – details to be checked by Building Control

SYMAS – No objections

Biodiversity – A bat survey has been submitted. No objection subject to the recommendations within the report being complied with.

Yorkshire Water – No objections

Ward Councillors – No comments received

Representations

Neighbour notification letters were sent to surrounding residents and a site notice placed nearby, two letters of objections were received in relation to:

- Loss of privacy/overlooking
- Loss of a view

Assessment

Principle of development

The site is located within the Green Belt where Local Plan Policy GB1 protects against inappropriate development in line with National Planning Policy. Whilst the erection of new buildings within the Green Belt are considered inappropriate development, the re-development of previously developed land is considered an exception under National Planning Policy and subject to compliance with Local Plan Policies.

All new dwellings proposed must ensure that living conditions and overall standards of residential amenity are provided or maintained to an acceptable level both for new residents and those existing, particularly in respect of the levels of mutual privacy. In addition, development will only be granted where the development would maintain visual amenity and

not create traffic problems or prejudice the possible future development of a larger area of land.

As outlined in the site history section above, a dwelling has already been approved on the site, and the permission for this is still extant, as such, the principle of residential development for a single dwelling on the site has been set. This application will focus on the proposed amendments over and above that previous approval.

Green Belt

The original proposal comprised of a single-storey dwelling measuring 237.6sqm (footprint only) but also had rooms in the roof space. To facilitate this, the height of the initial proposed dwelling was 3.65m to the eaves and 6.85m to the roof ridge. Concerns were expressed with the overall size and scale of the dwelling and the agent submitted amended plans reducing the dwelling to a footprint size of 200sqm and reducing the height to remove the rooms in the roof space, to approximately 2.6m to the eaves 4.2m to the ridge.

This amended application sees the increase in the eaves and ridge to 3.65m and 6m respectively with the inclusion of a detached garage measuring 6m by 6m with a height of 4m to the ridge.

Whilst it was requested during the determination of the earlier application to reduce the height of the proposed dwelling the agent has requested it be determined as it is. Overall, as seen from the calculations, there is a significant reduction in built form (the proposal constitutes just 61% of the building to be removed) which in this case is considered to offset the increase in height. It is therefore considered that in this instance the increase in height is acceptable, given that it is still 0.8m lower than originally sought.

This application also sees the inclusion of a detached garage, measuring 6m by 6m with a height of 2.4m to the eaves and 4m to the ridge. It is acknowledged that the inclusion of the garage would introduce an additional building, however, it would be located in an area of the site where there is an existing building and the proposed would be no higher than that which it replaces and therefore there will be minimal impact on the openness and character of the Green Belt

There are residential properties immediately to the south west, and as such, despite its Green Belt location, the area is somewhat suburban itself. Therefore, it is considered that the increase in height and the inclusion of the detached garage would not have a greater impact on the openness of the green belt than the existing buildings and as such is in compliance with Local Plan Policy GB1.

Visual Amenity

The proposed dwelling is a large, single-storey bungalow utilising rooms in the roof space and is not dissimilar to the dwelling which was approved under the earlier application.

The footprint of the building is also significantly less than the existing building on site (stables and equestrian storage) which measures approximately 442.42sqm. As such, given the changes made to the overall size and scale of the development, the proposed building itself is smaller in size than the existing building on site.

An adequate amount of private amenity space has been provided which has also been kept to a minimum; the amenity space does not extend any further into the green belt than the extent of the existing hard surfaced areas surrounding the buildings.

The access appears to be in reasonable condition, albeit grassed over in some parts, any treatment to this could have an impact on the openness of the green belt, however a landscaping condition requiring hard and soft landscaping details will be included and it will be expected that any treatment to the access will be covered by this condition. In addition, the area surrounding the site is quite open and therefore it is considered prudent to include conditions requiring boundary treatment details to be submitted upon commencement, to ensure that the area does not become too urbanised.

The development would see the removal of a poor quality equestrian and livery buildings and it is therefore considered that on balance the re-development of the site would enhance its immediate setting and as such would not have a detrimental impact on the openness of the Green Belt or upon the character of the area and is considered in compliance Local Plan Policies GB1 and D1

Layout and Residential Amenity

The dwelling is oriented within the site with the main front and rear elevations being on a north-east/south-west axis, the same orientation as the existing building.

The dwelling is situated approximately 7.8m from the southern boundary of the site and 16.5m from the boundary with Dorwil Cottage.

Whilst Dorwil Cottage is orientated on a north/south axis with the rear elevation facing the side elevation of the proposed property there is in excess of 30m between the blank side gable and the rear elevation of Dorwil Cottage, well in excess of the 12m required between habitable room windows and blank gable walls between two storey dwellings.

As mentioned, there are no windows proposed on the side elevation of the dwelling, with the boundary treatment providing suitable screening in addition to the separation distances.

There are no residential properties to the front or rear of the site which the proposal could be overlooked, those located on Frickley Bridge Lane are located approximately 100m from the south western elevation. Equally, the proposal would not have a significant overshadowing or overbearing impact upon to the neighbouring properties, with the nearest residential property (Dorwil Cottage) being located to the South of the proposed dwelling.

Due to the utilisation of the roof space the full floor area will not be available for use due to the limitations on head height. The floor area has been calculated using a minimum head height of 1.7m. This ensures that bedrooms 1 and 4 exceed the limitations for a double bedroom, whilst beds 2 and 3 fall short of the minimum requirements for a double room they do exceed them for a single room. The downstairs space far exceeds the minimum requirements for a dwelling of this size.

It is therefore considered that the proposed dwelling and detached garage would not have a significant detrimental impact upon residential amenity of neighbouring residential dwellings and the proposal is acceptable in terms of residential amenity and in compliance with Local Plan Policy GD1

Highway Safety

The site is accessed via the existing entrance into the site which is a fairly informal and unmarked access from Frickley Bridge Lane.

Given that the access currently serves the existing livery business it is not considered that the use of the access to serve 1no dwelling would have a significant impact on highway safety or the free and safe flow of traffic.

The site is clearly large enough to accommodate sufficient off-street parking for a dwelling of this size, with the inclusion of the garage and external parking areas to the front of it. The access and parking will need to be hard surfaced to meet highways requirements, but this can be covered under a pre-commencement condition.

Highways DC were consulted on the application; however, no response was received. The site plan shows that the gate is to be set back approximately 6m from the back edge of the highway to allow vehicles to exit the highway to gain entrance to the site, in addition an area at the entrance has been allocated for the storage of refuse bins on bin collection days as the dwelling is located in excess of the carry distance from the highway.

It is considered that the proposed dwelling would not have a detrimental impact on highway safety or on the highway network and as such is acceptable and in compliance with Local Plan Policy T4.

Impact on Ecology/Biodiversity

A bat survey report was submitted which the Ecology officer assessed. The Ecology Officer stated that any permissions should refer to the recommendations in the report. Provided that the development does not also affect significant areas of currently vegetated land, no further ecology information is required, and it is recommended to approve the application based on ecology considerations.

Drainage/Flood Risk

The site is not in a flood risk zone and drainage have assessed the application and have raised no objection to the details provided.

Other Issues

Objections have been made in relation to the loss of a view from the properties on Frickley Bridge Lane; there is no right to a view and is not a material planning consideration.

Recommendation

Approve with conditions