

4.0 Access

4.1 Vehicular and Transport Links

4.2 Refuse Collection, Servicing and Deliveries

4.3 Parking

4.4 Inclusive Access

4.0 Access

There are 2 main access points into the site, both from Huddersfield Road. One is located at the vehicular entrance to the site and the second is the pedestrian only access at the south of the site.

4.1 Vehicular and Transport Links

As Penistone Grammar is already a well established school with a defined and operational green travel plan, the intention is to build on the existing strategy and improve certain areas of the plan to ensure a minimal impact on the local highways.

From a pedestrian perspective, the existing site access routes will be maintained, and we understand that outside the project a new pedestrian crossing will be created at the lower entrance to ensure student safety on an otherwise busy road.

Due to the limited increase in the number of pupils accessing the school, we do not anticipate that there will be a need to change or upgrade any of the strategies around parking, access or deliveries.

The surrounding transport links include Penistone train station (0.6 miles) and a number of bus stops in the local area, two immediately outside the school gates.

In addition to public transport, the school operates its own bus transport system to



reach children in outlying areas and this is well documented and managed within the school travel planning.

4.2 Refuse Collection, Servicing and Deliveries

Due to the fact that none of the main service and delivery routes are being affected, the strategy for servicing remains exactly as per the existing school. The extension is a relatively modest size compared to the existing school and will not impact significantly on refuse or servicing.

	8AM		10AM		12NOON		2PM		4PM	
	All	Disabled	All	Disabled	All	Disabled	All	Disabled	All	Disabled
Tues 10 th							71	9	108	12
Weds 11 th					67	10	68	11	154	12
Thurs 12 th	76	11	68	11	77	12			153	9
Fri 13 th	101	10	71	12					112	10
Mon 16 th	99	11	52	12	75	12	73	10		
Tues 17 th	90	11			64	12			149	12
Weds 18 th	81	11					84	11		
Thurs 19 th			69	8	61	12	70	12		
Fri 20 th			89	12	92	12				

4.3 Parking

As part of the design development, the design team have ensured that there is a full understanding of the capacity of the existing car parking to understand any potential impact on the design for cars.

Following a detailed parking utilisation survey by the school, it is clear that there is adequate space to accommodate additional cars that may be required for additional staff on the site without causing any compromise for the school.

As a backup plan the school have alternative arrangements for busy days with the potential use of the coach parking area which is otherwise

unused during the bulk of the school day.

In conjunction with utilising surplus car parking, the green travel plan will also aim to target improvements in areas such as car sharing and use of green transport such as cycling and buses.

4.4 Inclusive Access

Due to the steeply sloping nature of the existing school site providing inclusive access for students is very important. As part of the original scheme a multilevel ramp was installed on the south facing bank to the main pupil entrance. We are advised by the school that due to the length of this ramp it has never been used for disabled access and so as part of this scheme we are making alternative

arrangements to allow pupils with limited mobility to access level 1 spaces (either through the new building or through the existing building near the construction area) which in turn leads directly to lift access providing access to all levels within the building ensuring a DDA compliant experience. Aside from being Part M compliant and offering accessible facilities, no further adjustments are being made to the existing site.

5.0 Implementation

- 5.1 Transport Access during Construction
- 5.2 Location of Contractor's Compound
- 5.3 Site Fencing and Safeguarding

5.0 Implementation

5.1 Transport Access During Construction

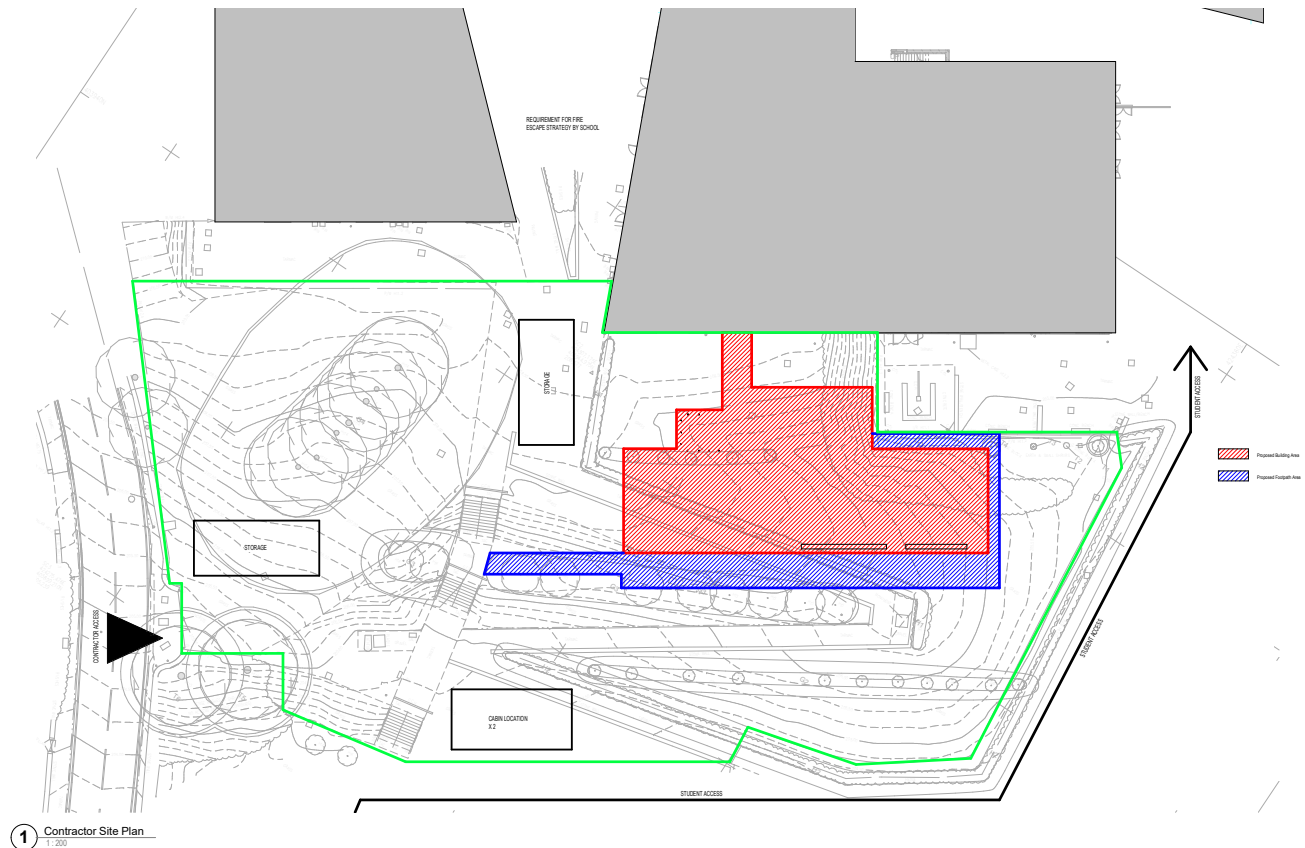
Access during construction is difficult. The site is constrained and accessed off a busy road. However, during the summer of 2018, a significant amount of refurbishment and internal remodelling was undertaken and the contractor successfully used the side entrance gate for the remodelling. It is not anticipated that any other access route will be required or will be available for the construction period.

5.2 Location of Contractor's Compound

As with the summer works, we anticipate that the contractor's compound will be located near to the side access entrance gate and that particular care and attention will need to be paid to the oak trees. From the local authority constraints plans, we do not believe these are protected by tree preservation orders, but they are mature and beneficial trees. As with the previous works, these trees will need to be safely fenced off, root protection zones will be avoided, and adequate care will be taken to protect these trees.

5.3 Site Fencing and Safeguarding

Due to the complexity of constructing in this part of the site particular care and attention will need to be paid to site fencing and safeguarding.



With support from the contractor, a more flexible arrangement may be possible, but at the planning stage we are assuming that pupils will be re-routed around the perimeter of the site and the contractors and pupils will remain separated at all times.

With the extensive existing ramps, access for disabled remains viable.

Due to the extensive nature of the compound, fire escape strategies from the school dining area and upper floors will require careful management strategies, especially when escapign to the north of the compound in the courtyard area.

6.0 Appendix

6.1 Planning Validation Tracker

6.1 Planning Validation Tracker

During the pre-application consultation, agreement was reached on the requirements of the supporting information for Validation.

This list of key pieces of information required as part of the application is described over the following pages.

Penistone Phase 2: Validation Checklist Tracker

Requirement	owner
Standard Application Form (1APP) Completed Online	10A
Location and Site Plans Plans provided with red and blue ownership boundaries.	10A
Block Plan The site at a scale of 1:200 showing the proposed development; any site boundaries; the type and height of any boundary treatment (e.g. walls and fences etc); the position of any building or structure on the other side of such boundaries and with any proposed extensions clearly identified to scale.	10A
Existing and Proposed Elevations To a scale of 1:100. These should show clearly the proposed works in relation to what is already there. All sides of the proposal must be shown in full (not part) and these should indicate, where possible, the proposed building materials and the style, materials and finish of windows and doors. Blank elevations must also be included; if only to show that this is in fact the case	10A
Existing and Proposed Floor Plans Drawn to a scale of 1:50 or 1:100. These should explain the proposal in detail. Where existing buildings or walls are to be demolished these should be clearly shown.	10A
Existing and Proposed Site Sections and Finished Floor and Site Levels Drawn to a scale of 1:50 or 1:100. These should show a cross section(s) through the proposed building(s). In all cases where a proposal involves a change in ground levels, illustrative drawings should be submitted to show both existing and finished levels to include details of foundations and eaves	10A
Roof plans Drawn to a scale of 1:50 or 1:100. A roof plan is used to show the shape of the roof and is typically drawn at a scale smaller than the scale used for the floor plans.	10A
Ownership and Agricultural Holdings Certificate	10A
Design and Access Statement For all extensions over 100sqm.	10A
Photographs / Photomontages As agreed during Planning Pre-application meeting	10A
Tree Survey Where there are trees within the application site, or on land adjacent to it that could influence or be affected by the development (including street trees).	10A
Flood Risk Assessment Confirmed as not required and referred to in the Drainage Strategy.	na
Drainage Strategy	SH

Penistone Phase 2: Validation Checklist Tracker

An updated strategy will be submitted demonstrating no requirement for attenuation tanks.	
Environmental Impact Assessment Only applies to large schemes or schemes with a significant environmental impact.	na
Landscaping Scheme A detailed landscaping scheme will be required based on a 1:1 tree replacement. This must include levels and will be required at 1:100 or 1:200	Plincke
Open Space Assessment Development does not affect Protected Open Space	na
Biodiversity Survey & Report	10A
Playing Fields Assessment For new developments which affect playing fields	na
Water Quality Assessment For applications that do not drain into mains	na
Transport Assessment Travel plan from school.	AAP
Green Travel Plan Updated Green Travel plan from school which must include evidence of take up of initiatives in the current green travel plan.	AAP
Noise Assessment Not applicable	na
Lighting Assessment	na
Air Quality Assessment Should not be required as increase in traffic is below 5% on roads with 5000 movements / day, however, Local Authority have reserved the right to request this following consultation with Regulatory Services.	na
Land Contamination Assessment SI reports included demonstrating not contamination identified.	SH
Waste Audit Site Waste Management Plan Waste Collection Strategy from client.	AAP
Coal Mining Risk Assessment For applications within the Development High Risk Areas.	na
Planning and Sustainability Statement This documents the issues relating to the NPPF for: Economic Social	AAP 10A

Penistone Phase 2: Validation Checklist Tracker

Environmental (Included in D&A Statement)	
Statement of Community Involvement Pre-App discussion has taken place with members of the Planning team, no public consultation has been arranged.	10A
Planning Obligations Major applications only	na
Carbon Budget Statement Over 1000sqm only	na
Crime Prevention Plan Over 1000sqm	na
Employment Land Assessment Not applicable.	na
Green belt impact statement Not required	na
Heritage Impact Assessment Separate report is not required but due to the proximity of the Conservation Area and visual impact of the school, a strategy demonstrating no negative impact must be developed as part of the Design and Access Statement. Photomontages will be a key element of this.	10A
Housing Development Statements Not applicable	na
Telecommunications Supporting Information Not applicable	na
Town Centre Statement Not applicable	na
Summary report Max 20 pages: Only required where application is more than 100 pages long	na
Community Infrastructure Levy This application attracts a zero rate.	na



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