

Application Reference: 2025/0887

Site Address: 159 Weetshaw Lane, Cudworth, Barnsley, S72 8BL

Introduction: This application seeks full planning permission for the erection a detached garage in the rear garden (Amended Description)

Relevant Site Characteristics:

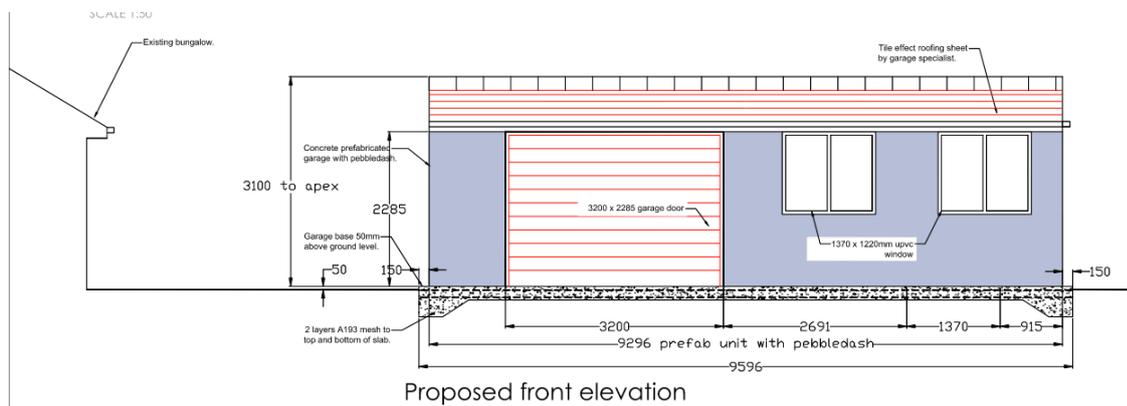
The dwelling is a detached red brick bungalow set within a large curtilage, located in a quiet area of Cudworth. The street scene is varied and with both neighbouring dwellings larger detached houses. A driveway extends from the highway, through the dwelling's front garden and car port, leading into a large, concreted driveway in the rear garden.

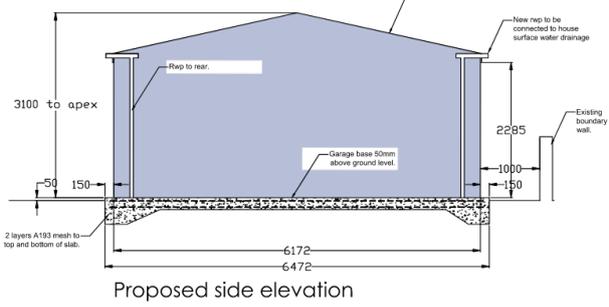
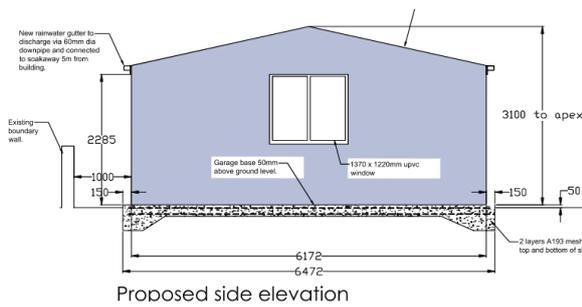


Detailed description of Proposed Works

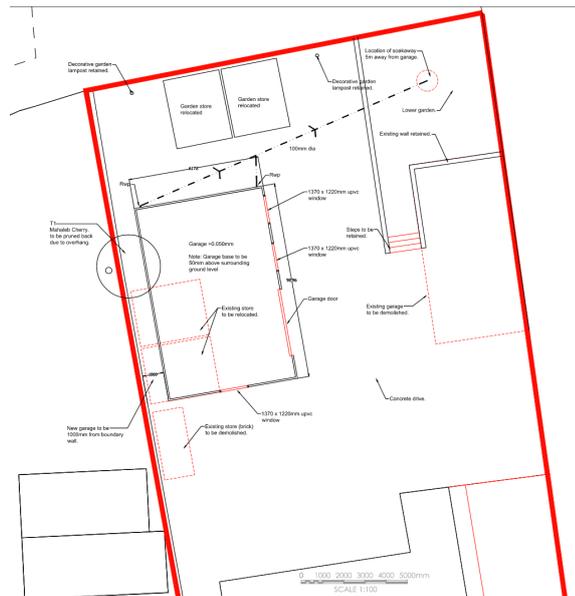
The proposal has been revised from the original proposal which was for the demolition of the existing garage and a new, detached to be located in the front garden. The proposal now seeks to erect a detached garage within the rear garden, whilst the existing garage is also demolished,

Proposed Elevations





Existing and Proposed Site Plans



Site History

Application

2007/1634
B/81/0743/CU

Details

Erection of a car port with a pitched roof
Erection of 60 ft, high radio aerial

Status

Approved
Approved

Relevant Policies

The Development Plan

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires proposals to be determined in accordance with the development plan unless material considerations indicate otherwise. The development plan for Barnsley consists of the Barnsley Local Plan (adopted January 2019).

The Local Plan review was approved at the full Council meeting held 24th November 2022. The review determined that the Local Plan remains fit for purpose and is adequately delivering its objectives. This means, no updates to the Local Plan, in whole or in part, are to be carried out ahead of a further review. The next review is due to take place in 2027, or earlier, if circumstances require it.

The following Local Plan policies are relevant in this case:

- Policy SD1: Presumption in favour of Sustainable Development.
- Policy D1: High Quality Design and Place Making.
- Policy GD1: General Development.
- Policy T4: New Development and Transport Safety

National Planning Policy Framework (NPPF) and the National Planning Practice Guidance

In December 2024, The Government published a revised National Planning Policy Framework ("NPPF") which is the most recent revision of the original Framework, published first in 2012 and updated a number of times, providing the overarching planning framework for England. It sets out the Government's planning policies for England and how they are expected to be applied. The NPPF must be taken into account in the preparation of local and neighbourhood plans and is a material consideration in planning decisions. This revised document has replaced the earlier planning policy statements, planning policy guidance and various policy letters and circulars, which are now cancelled.

Central to the NPPF is a presumption in favour of sustainable development which is at the heart of the framework (paragraph 10) and plans and decisions should apply this presumption in favour of sustainable development (paragraph 11). The NPPF confirms that there are three dimensions to sustainable development: economic, social and environmental; each of these aspects are mutually dependent. The most relevant sections are:

Section 2 - Achieving sustainable development

Section 4 - Decision making

Section 12 - Achieving well-designed places

The National Design Guidance (2019) is a material consideration and sets out ten characteristics of well-designed places based on planning policy expectations. A written ministerial statement states that local planning authorities should take it into account when taking decisions.

Supplementary Planning Guidance

In line with the Town and Country Planning (Local Planning) (England) Regulations 2012, Barnsley has adopted twenty eight Supplementary Planning Documents (SPDs) following the adoption of the Local Plan in January 2019. The most pertinent SPD's in this case are:

- House extensions and other domestic alterations
- Parking

The adopted SPDs should be treated as material considerations in decision making and are afforded full weight.

Consultations

The application has been advertised in accordance with Article 15 of the Town and Country Planning Development Management Procedure (England) Order 2015. Any neighbour sharing a boundary with the site has been sent written notification and the application has been advertised on the Council website.

No comments have been received

Highways: Highways comments were based upon the original proposal located within the front garden. However they remain valid and more than adequate for the proposed new location, which would utilise the same access but be constructed on an existing area of hardstanding,

Forestry: The Forestry Officer confirmed that the proposal, due to its construction, and the existing wall and hard standing, would pose no harm to the neighbouring tree, which has already been severely pruned. Additionally, any appropriate and necessary pruning of overhanging branches (only) would not harm the tree. If required, any pruning of non-overhanging branches would require the tree/land owners consent.

Planning Assessment

For the purposes of considering the balance in this application, the following planning weight is referred to in this report using the following scale:

- Substantial
- Considerable
- Significant
- Moderate
- Modest
- Limited
- Little or no

Principle of Development

The site falls within Urban Fabric which has no specific land allocation. Extensions and alterations to a domestic property are acceptable in principle provided that they remain subsidiary to the host dwelling, are of a scale and design which is appropriate to the host property and are not detrimental to the amenity afforded to adjacent properties

Scale, Design and Impact on the Character

The original proposal for a detached garage to be located in the front garden of the dwelling, between the dwelling and the road was deemed unacceptable and not in accordance to paragraph 7.27 of House extensions and other Domestic Extensions (SPD), which states 'In most cases, it will not be appropriate for a garage to be sited between the house and the road'. Whilst not cited as rational for the proposal, there are instances of garages located in front gardens, further along Weetshaw Lane. However these were appear to original and integral aspects of the dwelling's design and development.

The proposal has been amended to move the garage into the rear garden. Unusually the rear garden is predominantly concreted, with adequate drainage provision and fully accessible from the highway down the driveway which extends past the side elevation of the dwelling through the existing car port.

Whilst the garden is somewhat cluttered with existing outbuildings, the existing garage is proposed to be removed, and regardless of this garage's removal, the totality of new and old outbuilds and garages would not exceed 50% of the dwelling's curtilage.

In terms of scale, the garage is very large, with a footprint of approximately 57 sqm, which is larger than the garage it is proposed to replace, which had an approximate 37 sqm footprint. The approximate width and length of the garage externally are 6.17m and 9.47m respectively, whilst internal measurements are slightly smaller at approximately 6.2m and 9.14m respectively, with an internal footprint of 55 sqm.

In paragraph 7.28 of the SDD, it outlines how garages will be assessed in accordance with the South Yorkshire Residential Design Guide, which defines a larger, double garage as having a floor area of 6.5m x 6.5m, which equates at a footprint of 42.25m. With these measurements assumed to be the internal floor space size, the proposal would exceed these measurements by approximately 13sqm in internal floorspace, be significantly longer on length, but feature a shorter width. However, as mentioned above, the footprint would not exceed 50% of the curtilage and therefore an outbuilding or garage of this size could be erected under permitted development rights should the height not have exceeded 2.5m.

The garage is prefabricated and is finished in pebble dash. This does not match the host dwelling however garages and outbuildings are not always match entirely. Given the garage would be to the rear of the house there would be limited views from the street scene and it is not considered that it would have a harmful impact on the overall character and appearance of the dwelling or wider street scene.

Overall, the proposal would be considered to have a limited impact on the Scale, design and Character of the dwelling, and would be in accordance with local policies GD1 and D1. This carries moderate weight in favour of the application.

Impact on Neighbouring Amenity

It is unclear if the loss of the existing garage would be of benefit to the neighbouring dwelling of No.157, but there would be no other impact on their amenity by this proposal. Equally with no neighbouring dwellings to the rear, there is also no concern.

For the neighbouring dwelling at 161, the proposal may have an impact on their rear garden, with the longest side of the proposal being erected within 1m of their boundary treatment. However, given the height is relatively low, being only 3.15m at the ridge and 2.5m to the eaves, the impact is considered to be relatively limited. Especially given that it is set 1m away from the boundary. As such, it is unlikely that any harmful overshadowing or loss of light would occur as a result of the proposal.

There is no window to this elevation and so the proposal would not introduce overlooking or loss of privacy to that neighbour. Any other windows and doors face directly into the applicants own garden area at ground floor level and therefore are adequately screened from neighbouring properties by way of boundary treatments.

As such, the proposal is not considered to cause harm to neighbour amenity and would be compliant with local policy GD1. This weighs modestly in favour of the proposal.

Highways

With initial comments, and no objection to the original proposed location of the garage within the front garden, the proposed new location upon an existing area of hardstanding, would not materially alter this opinion, especially as existing and adequate parking provision remains and in accordance with the highways condition, any hard surfacing utilised by vehicles, if not already must be up to highways specification, including drainage provision, as outlined in the attached condition.

A second Highways condition remains relevant, in requiring the garage to be used for the parking of vehicles or other ancillary purposes incidental to the use of the dwelling house.

Planning Balance and Conclusion

For the reasons given above, and taking all other matters into consideration, the proposal complies with the relevant plan policies and planning permission should be granted subject to necessary conditions. Under the provisions of the NPPF, the application is considered to be a sustainable form of development and is therefore recommended for approval.

RECOMMENDATION: Approve subject to conditions

Justification

In dealing with the application, the Local Planning Authority has worked with the applicant to find solutions to the following issues that arose whilst dealing with the planning application:

- The original proposal for the garage to be located within the front garden was not considered acceptable. The garage was subsequently relocated.

**STATEMENT OF COMPLIANCE WITH ARTICLE 35 OF THE TOWN AND COUNTRY
DEVELOPMENT MANAGEMENT PROCEDURE ORDER 2015**

Due regard has been given to Article 8 and Protocol 1 of Article 1 of the European Convention for Human Rights Act 1998 when considering objections, the determination of the application and the resulting recommendation. it is considered that the recommendation will not interfere with the applicant's and/or any objector's right to respect for his private and family life, his home and his correspondence.