

# EXPERIENCE BARNSLEY EXTERNAL WORKS

## Design and Access Statement

June 2012



**BARNSLEY**  
Metropolitan Borough Council

Development, Environment & Culture Directorate  
Highways and Engineering

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# 1. INTRODUCTION



Aerial view of Town Hall from the east showing site boundary

Site Location Plan (Not to scale)



## 1.1 Introduction

The project represents a significant opportunity to improve the public realm adjacent to Barnsley Town Hall.

The Town Hall is a grade 2 listed building and is the focus of the town's civic quarter, surrounded by a variety of civic buildings of significant heritage value. It is one of Barnsley's most striking and attractive buildings and a major landmark within the town.

Currently the Town Hall provides office space for a number of Barnsley Metropolitan Borough Council departments. In 2009 BMBC secured Heritage Lottery funding to develop a museum and community heritage facility within part of the Town Hall. In 2013 the 'Experience Barnsley Museum' will open containing galleries, exhibition space, a cafe, local studies centre and archive.

The existing external space is dominated by car parking, lacks a cohesive layout, is in a poor state of repair and has a negative impact on the surrounding townscape. With the internal developments and the cleaning of the facades, the opportunity comes to create a more appropriate setting for the Town Hall and the Experience Barnsley museum.

The key objectives of the project are to create a new public realm destination for the town, space for events and setting for the Town Hall and Experience Barnsley. In addition the scheme needs to retain access for civic and other functions within the Town Hall and respect the relationship between the Town Hall and the memorial gardens. The southern side entrance to the Town Hall, off Lancaster Gate, will become the main entrance for Experience Barnsley.

The proposed scheme will provide a new focal point for the town centre of Barnsley and a setting befitting the Town Hall in its role as a key civic building for Barnsley.

## 1.2 Location & Extent

Whilst context and outline design proposals are shown for the memorial gardens area to the east of the Town Hall this statement and planning application relates to detailed proposals for the area to the south of the Town Hall, with the memorial gardens area being shown for the purposes of illustrating potential future context.

The proposed site is located to the immediate south of Barnsley Town Hall within the town centre. The site is bounded by the Town Hall in the north, Church Street in the east, Shambles

Street in the south and St Mary's Place in the west. Site area is approximately 0.2 hectares.

### **1.3 Design Team**

The project is being delivered by the Highways and Engineering Team at the Development, Environment and Culture Directorate of Barnsley Metropolitan Borough Council, working with Estell Warren Landscape Architects.

# 2. EXISTING SITE



1852

- Medieval plot (burgage) pattern evident, with numerous narrow yards
- Site of the Moot Hall (meeting place / council chamber & law court) indicated
- Shambles Street (to south) and Church Street (to east) are main thoroughfares.



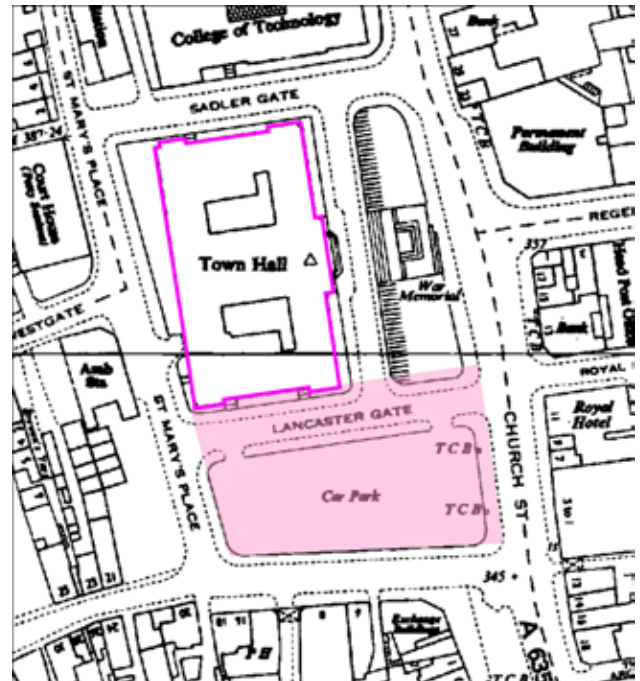
1894

- Medieval plot (burgage) pattern remains intact
- Regent Street links to Church Street from the east



1906

- Historic built form remains largely intact
- Royal Street joins Church Street



1962

- Indicating Barnsley Town Hall (opened 1933), war memorial gardens and associated modern road layout, including extension of St Mary's Place to the west



Circa 1910, view from Market Hill to Shambles Street



Circa 1948, view from Market Hill



Circa 1955



Circa 1950, view from Regent Street

## 2.1 Site History

The proposed site lies at the heart of Barnsley's town centre which has evolved from a medieval urban core. The surrounding townscape still shows a pattern of long thin tenement (burgage) plots (see 1852 map) which have partially dictated the evolving layout of the town centre.

The historic built form near the site remained largely intact until the 1930's (see 1906 map) when the building of the Town Hall and Barnsley College opened up this part of Barnsley's town centre (see 1962 map). The layout of these new civic buildings and new road network increased the sense of scale and importance of this area for the town as a whole.

The site lies within the Regent Street/ Church Street/ Market Hill Conservation Area, which was designated by Barnsley County Borough Council in November 1973, and extended in April 1977 by the present council. The Town Hall was listed as a building of special architectural or historic interest Grade II in 1986, as a result of the national re-survey. The war memorial and the piers, railings and steps on the frontage were listed as two separate additional items.



Circa 1966et



View to east frontage of Town Hall on  
Church Street



View towards Market Hill from Church Street

### 2.2 Overall Character & Setting

Although the memorial gardens and war memorial fronting onto the eastern facade of the Town Hall retain some of their distinctive original character they would benefit from repairs and a renewal of their relationship with the Town Hall, related to its future use.

To the south of the Town Hall, however, the existing space (and location of this project) has steadily deteriorated since construction of the Town Hall in the early 1930's, resulting in a current layout that does not do justice to an important town centre space and setting for the Town Hall.

The proposed site is dominated by surface car parking, accessed from St Mary's Place via Lancaster Gate, which has a negative effect on the setting of the Town Hall and restricts pedestrian access through the site. Ad hoc, low quality materials and a poor layout relate neither to the Town Hall or the wider urban setting.

The site is characterised by significant level changes, falling from west to east. Modern retaining walls and raised planting beds create an uninviting pedestrian environment and add to access difficulties.

The surrounding streets slope from west to south/east away from the Town Hall towards the shops on Market Hill affording the site a prominent raised location in the town centre.

Pedestrian access to the site is possible from St Mary's Gate, Shambles Street and Church Street, although none of these entrances is clearly defined or welcoming. A key pedestrian route from the transport interchange to the Council offices at Westgate Plaza brings people through the site along Lancaster Gate.

The wider setting of the site has the potential to form an attractive backdrop, with good examples of period buildings opposite the eastern facade of the Town Hall on Church Street and to a lesser degree on Shambles Street.

The proximity of the vibrant town centre shopping area at Market Hill, location on a key pedestrian route and new setting for Experience Barnsley underlines the potential for the site to become a key public space in the centre of Barnsley.

### 2.7 Views



Period building on Shambles Street



Period building on Church Street

2.3 Surrounding Town Centre Land Use

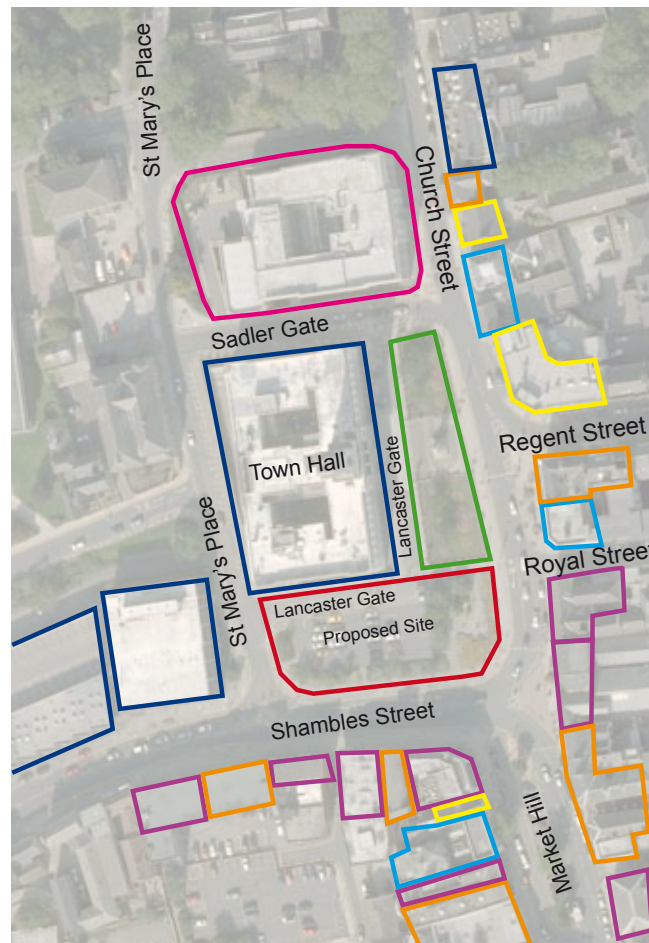
The site is surrounded by a variety of different uses. Retail and leisure use predominates to the south and east of the site, sloping down towards the retail core of the town at Market Hill and Barnsley Markets beyond. To the north and west there are important civic and educational buildings such as the central library and Huddersfield University’s Barnsley campus in close proximity. The Transport interchange lies due east of the Town Hall at the foot of Regent Street.

2.4 Town Hall Building Use

The first and second floor of the Town Hall will be used for ceremonial duties and the basement and ground floor will contain a new museum, ‘Experience Barnsley’. This new use, as a visitor attraction, will increase the usage of the building, bringing new visitors to it. The setting of the building to the south and south-eastern corner is not considered in keeping with its status as a listed building and one of the most important buildings in the Borough.

Town Centre Land Use Plan

-  Civic
-  Education
-  Public Gardens/Open Space
-  Bank/Building Society
-  Retail
-  Public House/Restaurant
-  Commercial





Lancaster Gate access/parking area

### 2.5 Vehicular Access




Existing vehicular access is available from St Mary's Place onto Lancaster Gate. This provides access to the existing surface car park, to the south of the Town Hall, and is used for formal access to the main Town Hall entrance on the eastern elevation. This route connects to a vehicle exit onto Sadler Gate, located at the north east corner of the Town Hall.

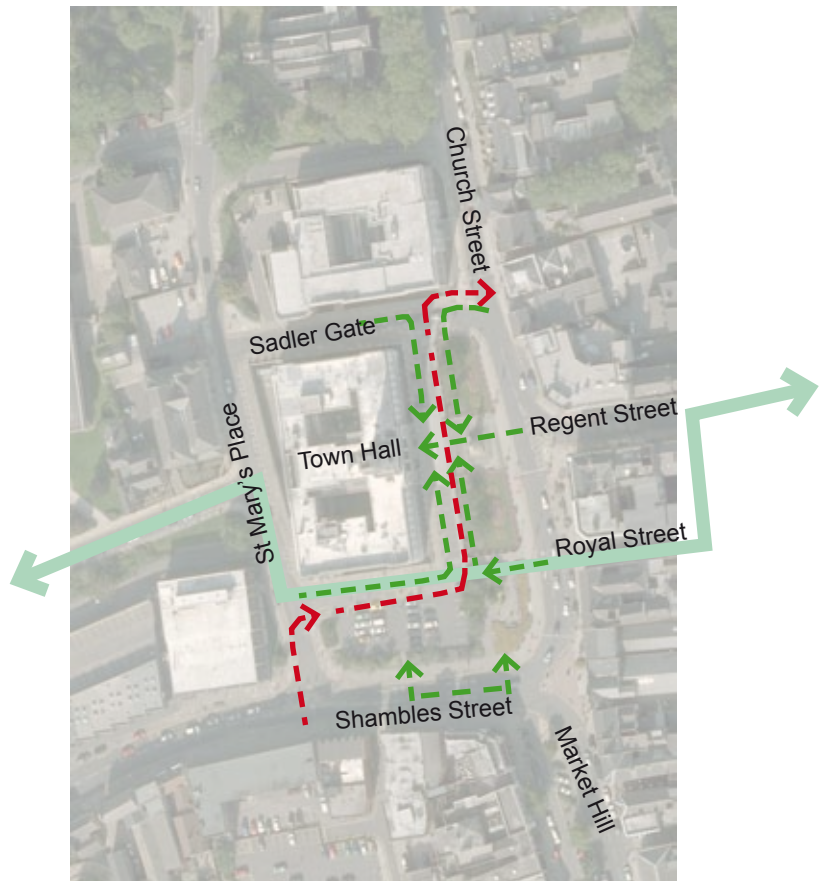
### 2.6 Pedestrian Access

There are a number of existing pedestrian access points to the site. Access from Shambles Street and the corner of Church Street/Market Hill is via a series of steps running up through raised formal planting beds. The main Town Hall entrance is accessed from Church Street via steps set around the war memorial. This approach forms part of the set piece vista of the Town Hall extending eastwards along Regent Street.

The 'Green Sprint', a route established as part of the 'Remaking Barnsley' vision, runs from Royal Street and through the site along Lancaster Gate before crossing St Mary's Place and continuing west along Westgate.

### Vehicular and Pedestrian Access Plan

-  Vehicular Access
-  Pedestrian Access
-  route of 'Green Sprint'





View to south elevation of Town Hall from Shambles Street

The view from the south east is the most prominent view of the Town Hall, with rising topography creating the impression of the building being set on a raised plinth looking out over the town centre.

The eastern frontage of the Town Hall is flanked by the memorial gardens including Yorkstone steps and war memorial. The formal civic appearance of this frontage is to be retained as it contains a number of listed features such as the railings and stone piers. Retention of these features would preserve the classic set piece view of the Town Hall from Regent Street.



Vehicular exit onto Sadler Gate

The area in front of the southern elevation of the Town Hall comprises a surface car park surrounded by raised planting beds. This detracts significantly from the overall view of the Town Hall from Shambles Street and St Mary's Place.

### 2.8 Levels, Gradient & Drainage

The site slopes considerably towards the east from 113m Above Ordnance Datum (AOD) on St Mary's Place to 107m AOD at the corner of Church Street and Market Hill, creating west to east gradients typically at 1 in 10/11.

From south to north, across the car park, gradients typically fall at 1 in 50.

Surface water is currently channelled to road gulleys within the site and drained to the highway surface water drainage system.



View to South east corner of Town Hall on Church Street



Retaining walls restrict direct pedestrian access to Town Hall



Raised planters prevent clear views across the site and restrict pedestrian movement



Steps to war memorial at existing street level

## 2.9 Boundary Treatments

The boundaries of the site include the following:

- listed Portland Stone bollards with black steel chains to the perimeter of the memorial gardens;
- raised concrete block beds to the south eastern and southern edge of the site, facing onto Church Street and Shambles Street;
- flush planting beds along the boundary with St Mary's Place.



Wide pavements along adjoining highway on Shambles Street



Column and chain arrangement along Church Street pavement



Poorly defined boundary to parking area. Vehicles dominate views along southern frontage

**2.10 Listed Features**

Barnsley Town Hall is a grade 2 listed building and is accompanied by a number of other listed features in its immediate setting including the war memorial and the piers and railings surrounding the Town Hall and the memorial gardens.



Listed stone piers and railings around Town Hall frontage



Memorial gardens column and railing arrangement



Existing formal planting along eastern frontage to be retained



Existing formal planting along southern frontage impedes direct access from Church Street/ Market Hill

**2.11 Existing Vegetation**

Existing vegetation cover consists of a mixture of formal annual bedding in the adjoining memorial gardens and around the edges of the car park, with overgrown shrubs and herbaceous planting in the raised beds (originally designed as a 'sensory garden') at the south east corner of the site.

Tree cover is limited to a small number of stunted and poorly shaped specimens located in the area south of the Town Hall and four ornamental cherry trees in the memorial gardens.



Yorkstone paving and kerbs, southern frontage



Concrete flags, war memorial terrace

## 2.12 Existing Construction Materials

The Town Hall is built from Portland Stone as are the stone piers which run along its frontages. The pavements and steps serving the Town Hall are predominantly of Yorkstone. Lancaster Gate and the car parking area are surfaced with asphalt. Below the asphalt layer original grey/ green granite setts remain along Lancaster Gate. It is believed that these setts formed part of the original 1930's design. There are also large areas of modern concrete paving and concrete block walls (at the south east corner of the site).



Yorkstone steps to Town Hall entrance asphalt access road, Yorkstone kerbs



Large Yorkstone paving slabs, eastern Town Hall frontage



Yorkstone steps to Town Hall near war memorial

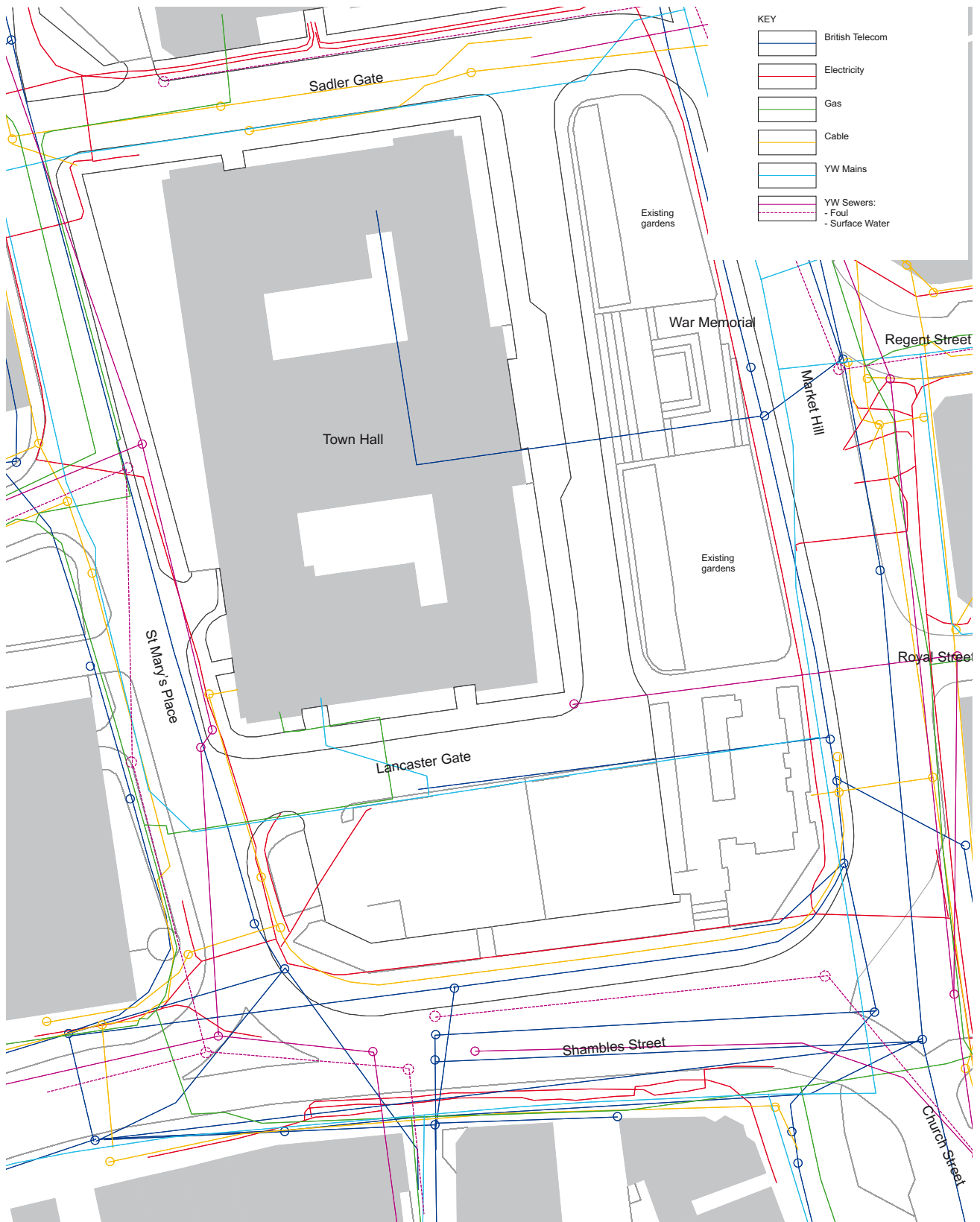


Green/grey granite setts with Yorkstone kerbs



Concrete flags and retaining walls

## 2 EXISTING SITE



### UTILITIES LAYOUT

- Main part of site relatively clear
- Surrounding area (especially pavements) heavily congested

**2.13 Utilities**

The proposed site itself is relatively clear of utilities with minor water and electricity mains running through it. These are not seen as a significant constraint to development.

Roads and pavements surrounding the site, however, are heavily congested with utilities.

At the south east corner of the site a telecommunications box and traffic signal control box are located adjacent to the site boundary, forming unattractive street scene elements.



# 3. DESIGN OPTIONS



### 3.1 Aims and Objectives

From the original brief, the following aims and objectives were identified as being key issues to address during the development of design options;

- place the Town Hall in a setting befitting its importance as a key civic location;
- create a new setting and arrival point for 'Experience Barnsley', including highlighting the existing low key entrance on the southern Town Hall facade;
- improve pedestrian access to the site and its immediate surroundings;
- create a high quality public realm focal point for the town centre.

Other factors included;

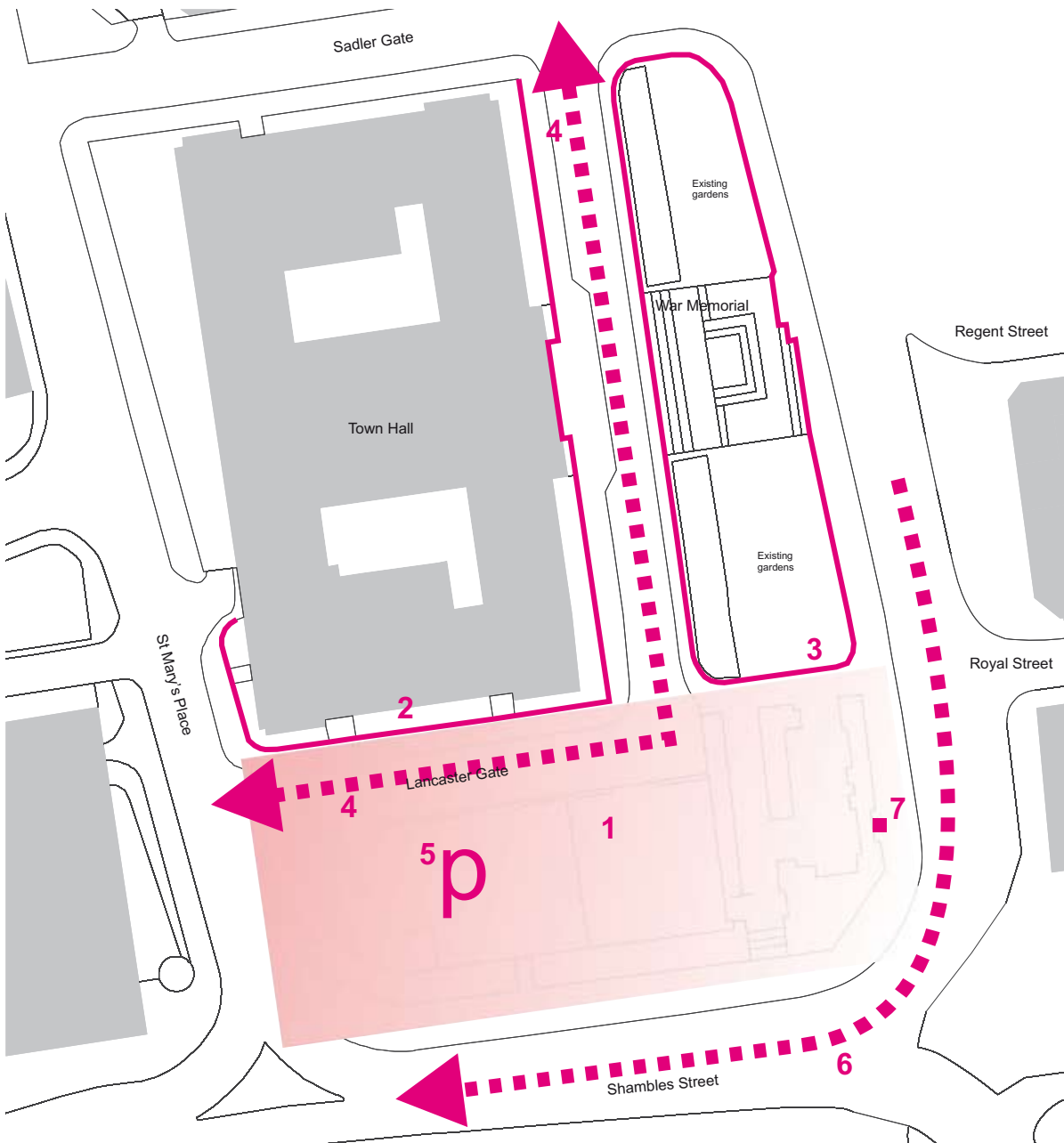
- investigate the possibility of retaining car parking within the site;
- retain ceremonial vehicular access to the front of the Town Hall for civic occasions and weddings.

The brief also identified that proposals should be guided by the design principles established within the Town Centre Public Spaces Strategy and should link closely with the public realm proposals for Royal Street, Regent Street, Eastgate, Regent Street South, Hanson Street and Eldon Street, which are being improved as part of the Urban Centre Infrastructure project.

### 3.2 Constraints and Opportunities

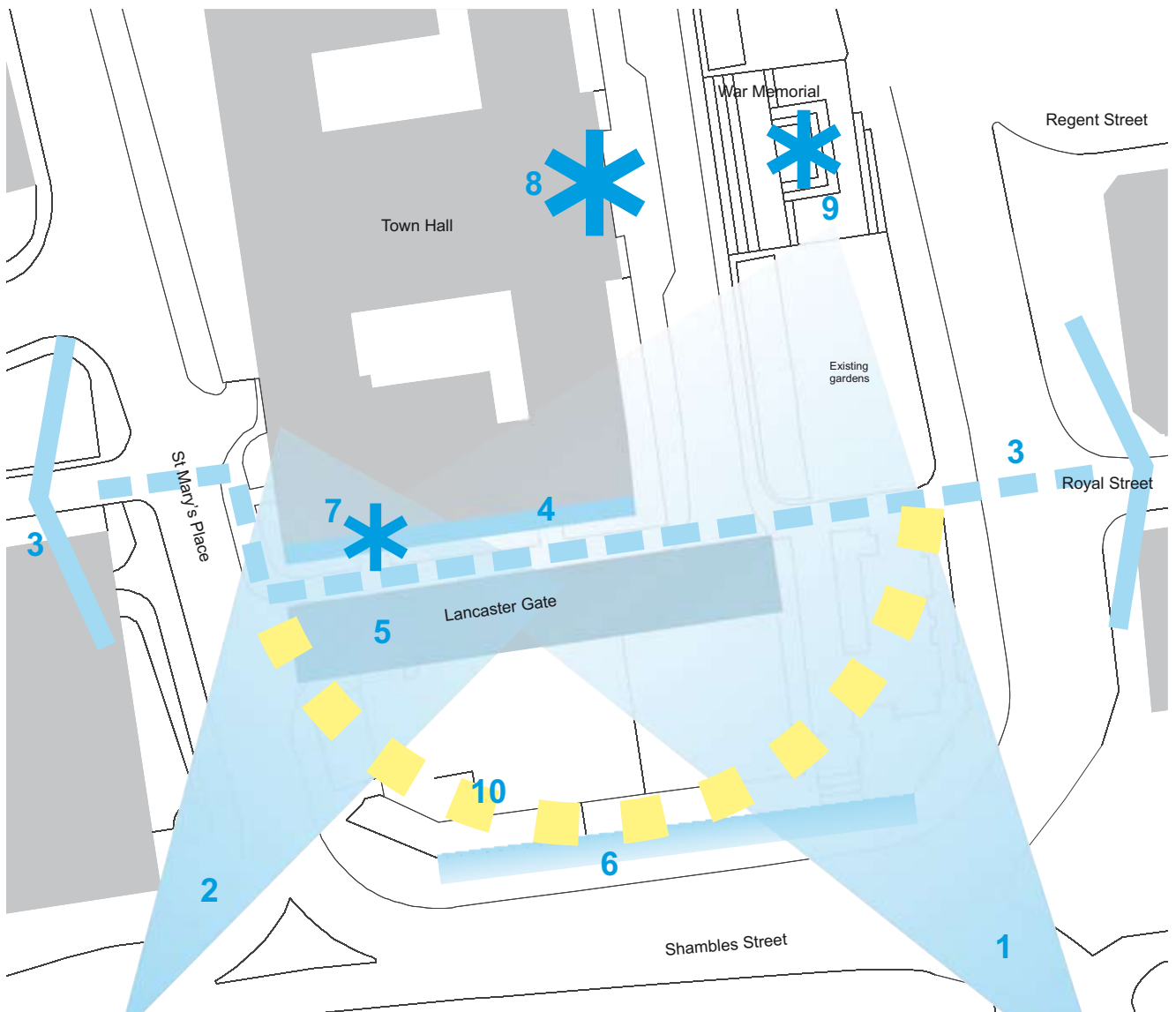
At the start of the design process an assessment was made of the key constraints and opportunities presented by the site, to help inform the general approach to design and aid the comparison of options.

Constraints and opportunities are listed and shown diagrammatically below.



**KEY CONSTRAINTS**

- 1. Severe level change across site impedes access and restricts potential for level/ usable space.
- 2. Listed frontages to Town Hall must be retained.
- 3. War memorial and memorial gardens including listed features must be retained.
- 4. Vehicle access for servicing and events is required along southern and eastern elevations.
- 5. Possible requirement for replacement car parking (p on plan) could conflict with/ limit new public realm design.
- 6. Existing busy roads create island effect, impeding pedestrian access and creating noise.
- 7. Key heritage relationship exists between eastern Town Hall elevation and Memorial Gardens



### KEY OPPORTUNITIES

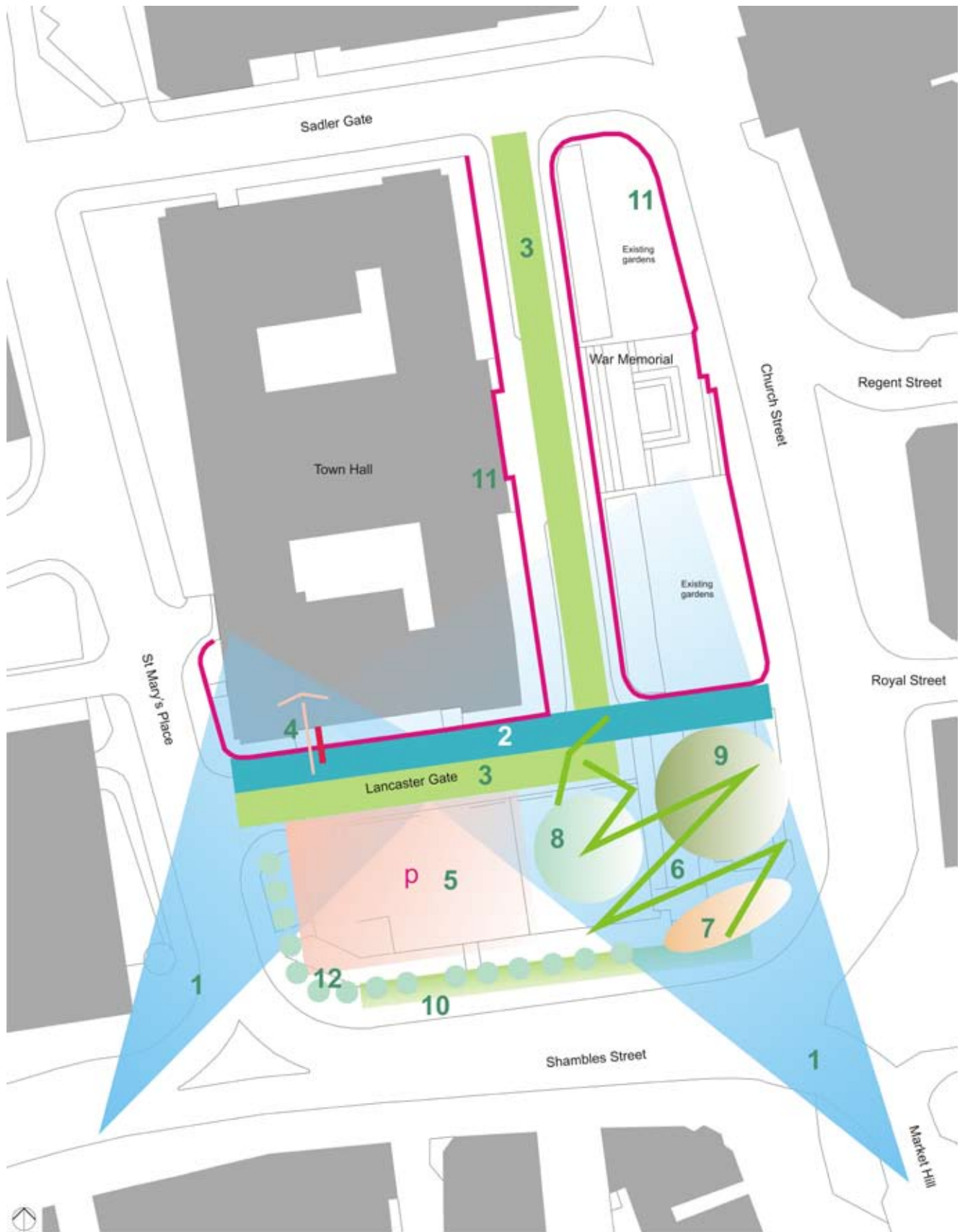
1. Iconic view from Church Street/ Market Hill to Town Hall facades and tower, could be used to encourage people into the site.
2. Surprise view of Town Hall southern facade when entering town from west.
3. Green Sprint and associated pedestrian movement between Civic offices and transport interchange.
4. Town Hall southern facade offers impressive backdrop to the site.
5. Existing dark green setts under roadway could be reused, may also inform materials choice for wider Green Sprint?
6. Wide highway pavements could be narrowed to create more space within the site.
7. New museum use creates activity on southern facade.
8. Continuing Town Hall functions and events create activity on eastern facade.
9. War memorial creates annual ceremonial activity.
10. South facing aspect of the site creates potential for attractive microclimate, lunchtime sitting out etc

### 3.3 Design Approach

From a review of the constraints and opportunities presented by the site a 'design approach' diagram was prepared (see opposite) to identify key issues that the client and design team considered should be explored in the design options.

#### DESIGN APPROACH

1. Key views to Town Hall retained, some limited tree planting may be considered within these views.
2. Dark green setts reused to create strong clear line along route of proposed Green Sprint.
3. Vehicle access zone demarcated by contrasting surface material but designed as flush surface with Green Sprint and adjoining new square. Retain parking 'p'.
4. Museum entrance highlighted by new steps which also offer direct access with retention of existing ramp.
5. New square, open space to allow southern town hall elevation to be appreciated, possible multifunctional use as parking area, possible public fountains. Design as level as possible.
6. Design south east corner as permeable space, encouraging pedestrians to flow in and use at lunchtimes, offer variety of access and space.
7. Design important south east corner as 'outcrop' or 'platform' on which Town Hall appears to be constructed.
8. Include quiet space that can be used for wedding group photos.
9. Replacement sensory garden planting.
10. Extend scheme into highway pavement, reduce pavement to 2.5m.
11. Retain important listed features including walls, minor change required to accommodate new entrance arrangements on south elevation of Town Hall.
12. Form buffer/ filter zone to reduce impact of adjoining roads on new square.



OPTION 1



Option 1

Multi-level terraced areas without car parking, formal steps provide access from Market Hill/ Church Street corner.

Pros

- Formality of curved steps ties in with existing formal gardens and civic appearance of the area
- 3 level areas created
- Layout and listed structures of formal gardens retained

Cons

- Requirement for numerous retaining structures adding extra expense and visual complexity to the site
- Series of steps to negotiate
- No direct easily accessible route to new museum entrance.
- Inflexible series of spaces, all similar size

OPTION 2



**Option 2**

Multi-level terraced areas with some car parking retained, formal steps provide access from Market Hill/ Church Street corner.

**Pros**

- Formality of curved steps ties in with existing formal gardens and civic appearance of the area
- 3 level areas created
- Layout and listed structures of formal gardens retained

**Cons**

- Space subdivided into separate areas so does not work as unified whole
- Car parking still dominates the site and detracts from the setting of the Town Hall
- Retaining wall adjoining car park restricts views to Town Hall entrances
- Car park reduces the sense of available space and interferes with pedestrian access
- Inflexible series of spaces, all similar size

OPTION 3



Option 3

Dual-level terrace with some car parking retained, formal steps provide access from Market Hill/ Church Street corner.

Pros

- Formality of curved steps ties in with existing formal gardens and civic appearance of the area
- 2 level areas created
- Layout and listed structures of formal gardens retained

Cons

- Space subdivided into separate areas so does not work as a unified whole
- Car parking still dominates the site and detracts from the setting of the Town Hall
- Retaining wall adjoining car park restricts views to Town Hall entrances
- Car park reduces the sense of available space and interferes with pedestrian access
- Inflexible series of spaces, all similar size







#### Option 4

Broad terraces create open spaces leading up from Market Hill/ Church Street corner with single large space fronting Town Hall, used for car parking and events.



#### Pros

- 28 parking bays made available, less visually dominant within design
- Good sized, flexible square for events
- Terraces create gradual transition in levels across the site and open access from south east, gentle, welcoming approach to pedestrians, multiple opportunities for sitting
- Terraces create strong 'platform' for Town Hall
- Green Sprint route clearly defined
- 2 series of entrance steps on Shambles Street
- Layout and listed structures of formal gardens retained

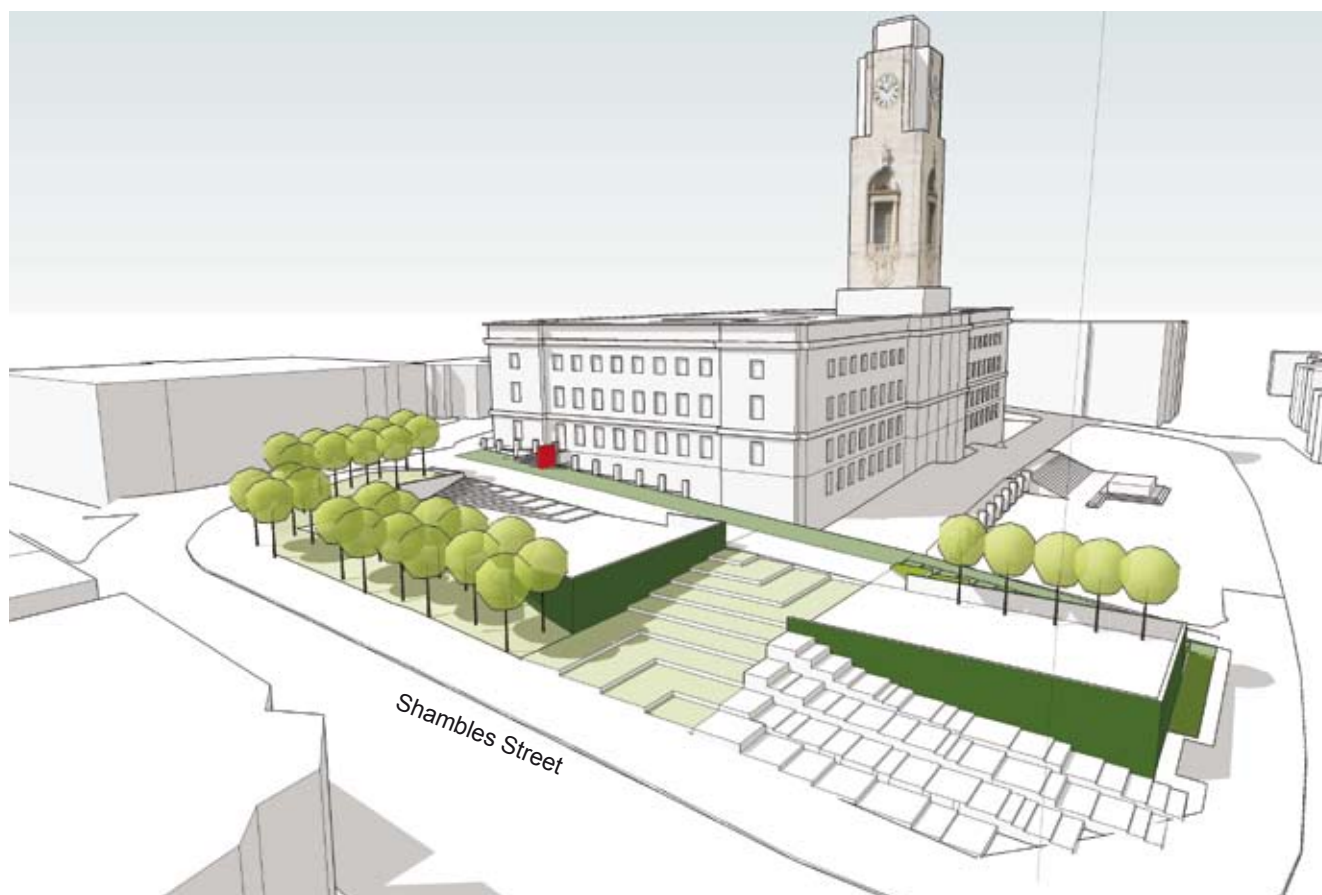
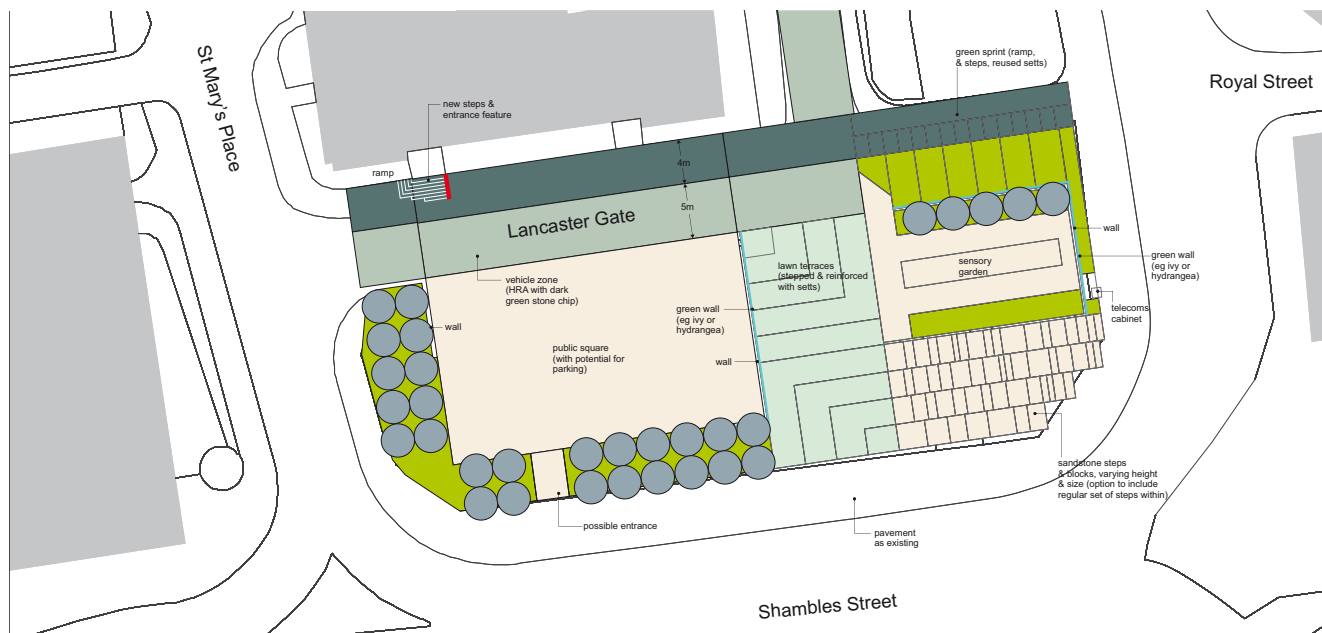


#### Cons

- 2 distinct spaces with parking divided by retaining wall
- Cars still have potential to conflict with pedestrian movement



OPTION 5





#### Option 5

Stepped corner to Market Hill/ Church Street with raised terrace and stepped lawns leading to new square. extensive use of retaining walls to achieve level space, potential to retain parking.

#### Pros

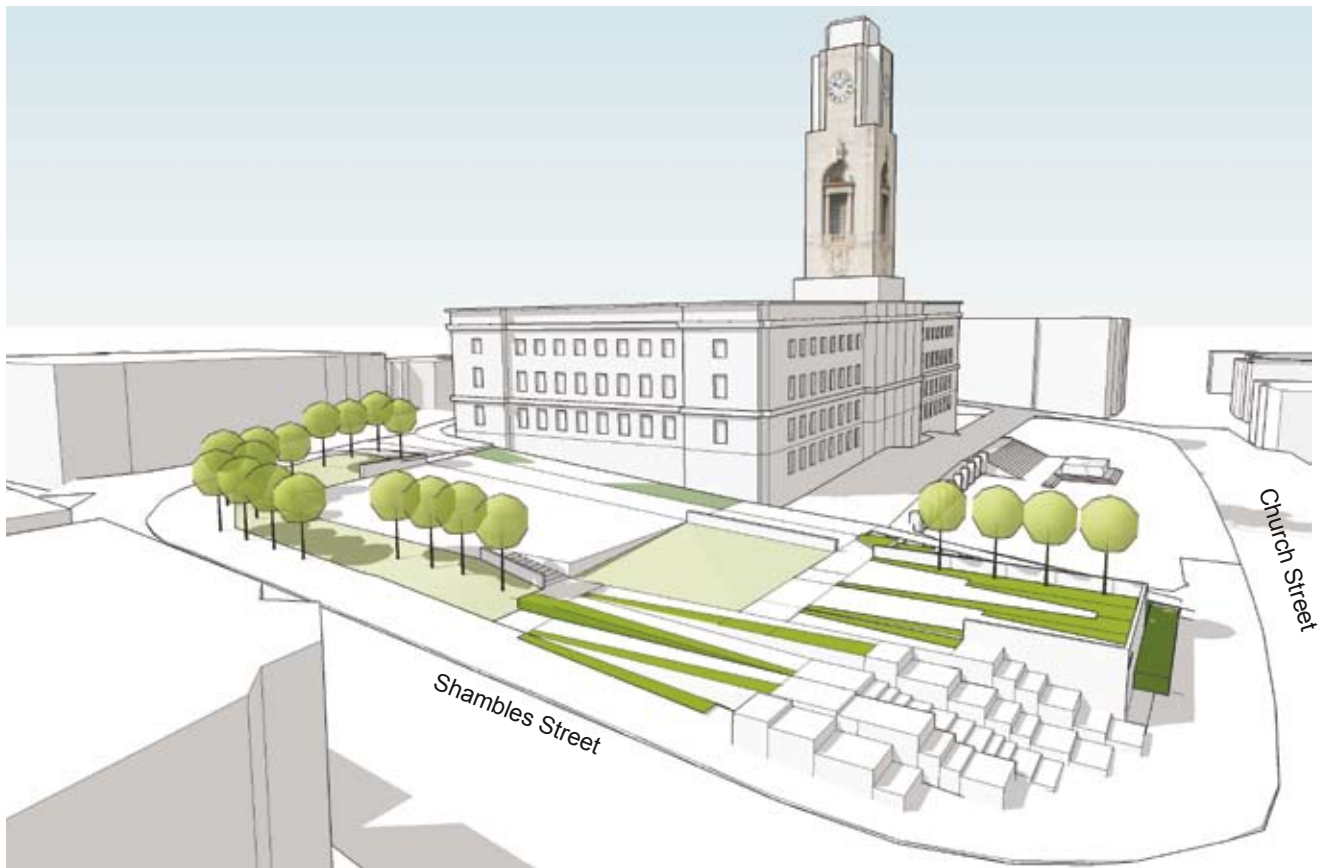
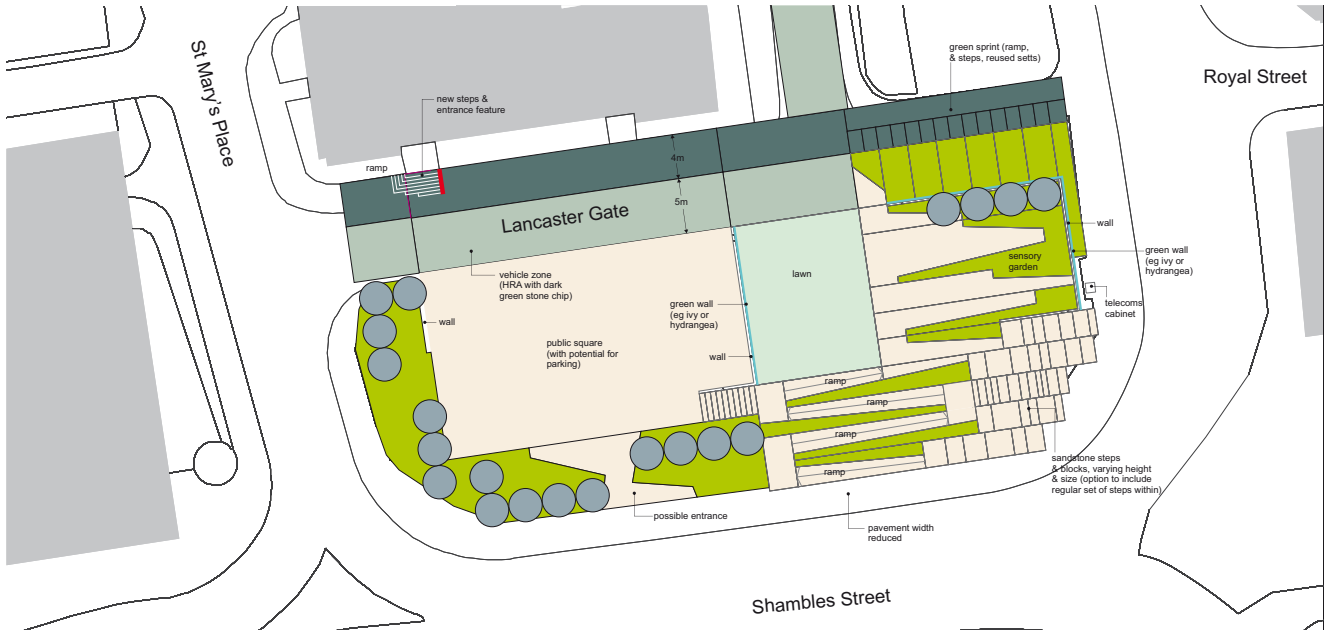
- Layout and listed structures of formal gardens retained
- Flexible square with potential for parking
- 'Green Sprint' route clearly defined
- Sensory garden reinstated
- Green walls soften backdrop to car park retaining wall and stone blocks
- Regular flight of steps within stone blocks
- Existing levels retained on pavements



#### Cons

- Site divided into 3 distinct areas
- Lawn area terraced to accommodate level change
- High retaining walls prevent clear views across the site and further subdivide spaces, making pedestrian access difficult and obscuring views to War Memorial

OPTION 6





#### Option 6

Stone blocks and steps incorporating ramp to Market Hill/ Church Street corner, with specific sensory gardens and flat lawned area leading to new square.

#### Pros

- Tiered stone blocks provide informal seating
- Raised lawn area creates soft area for play and informal seating
- Varied planting blocks for visual diversity
- Green walls softens backdrop to retaining structures
- Flexible square with potential for parking
- Wider public entrance created on Shambles Street
- 'Green Sprint' route clearly defined
- Layout and listed structures of formal gardens retained

#### Cons

- Site divided into 3 distinct areas
- Need to have ramped access creates more retaining structures
- High retaining walls prevent clear views across the site and further subdivide spaces, making pedestrian access difficult and obscuring views to War Memorial

### 3.4 Design Options Review

Each design option was considered by the design team and client body and the relative pros and cons were reviewed, as outlined above.

Option 4 was selected as the strongest scheme that most closely responded to the design objectives. Its particular strength is that it presents a cohesive space whilst enabling variety and flexibility of use.

It has a seamless transition from the tiered stone steps up to the flexible public square, removing the need for most of the retaining structures required by the other schemes.

### 3.5 Scheme Development

Option 4 was developed further in order to move towards a resolved design. In continuing discussions a number of issues emerged which would guide the further development of this preferred option.

- firstly it was agreed that there was no longer a need for parking on the public square as alternative provision could be made in close proximity to the Town Hall;
- secondly it was felt that a water feature should be incorporated within the square to animate the space and create opportunities for informal play.

A refined version of option 4 was developed and taken to Design Review Panel on 29 May 2012.

### 3.6 Barnsley Design Review Panel

The Review Panel made a series of suggestions which have been incorporated or used to guide the further development of Option 4 into the proposed scheme:

- open up the front/ bottom edge of the terraces more by removing some planting.
- integrate the square better with the terraces (now possible due to parking requirement being removed). Look to wrap the square space around the SE corner of the TH.
- include outline proposals for restoring the eastern TH frontage including a new plate of Yorkstone (setts) to

connect the Town Hall entrance to the war memorial and break up 'road' character, repair of existing Yorkstone flags, replacement of all concrete flags with new Yorkstone flags. Although this element relates to the eastern Town Hall frontage it was agreed that changes here should be shown indicatively to help set the site in a better context.

- demarcate new Experience Barnsley entrance with a vertical glass panel sited on east side of new steps, panel to read during the daytime as a feature on the Green Sprint and to be lit at night. Ideal opportunity for artist involvement to both design and manufacture the panel. Panel needs to be strong enough to incorporate a steel handrail on its western face.
- consider 'entrance feature' or marker at base of steps, perhaps reflecting Town Hall construction material (ie Portland Stone) to act as a visual reference and introduction to spaces around the TH.

Further suggestions by the Panel were considered but not taken any further by the design team on the basis that they were not appropriate for the scheme:

- regular/ curved steps rather than terraces. This would defeat the object of the preferred option which is to create an integrated space out of the terraces, something that is unique to Barnsley. Regular steps cannot respond to the varying level changes without the use of retaining walls. The wider terraced approach gives the flexibility to work with existing adjoining levels.
- consider modernising of planting in the memorial gardens – the design team agreed that this would be desirable but should form part of later proposals for improvement of the eastern Town Hall frontage.
- consider new footpath on north/ south axis through the war memorial, this is not required to appreciate gardens, does not fit with heritage fabric, difficult levels to deal with. Overall it was considered that this would over clutter/compromise the existing layout and would offer no great new benefit.



# 4. PROPOSED SCHEME

### 4.1 Proposed Scheme Description

The proposed scheme, as shown on page 46, would create two new distinctive places within the site responding to its special location:

- firstly an outward looking series of Yorkstone terraces would stretch across the south east corner of the site between Shambles Street and Church Street;
- secondly a new public square would face onto and wrap around the southern Town Hall elevation.

Whilst both places have their own strong characters the two are closely interlinked spatially and in terms of materials use.

The terraces would achieve the key design aims of creating a 'solid platform' on which the Town Hall appears to sit in the important view from the corner of Church Street/ Market Hill and an open, permeable edge encouraging pedestrian movement into and through the site. Levels across the terraces closely reflect the underlying natural topography and are seen as an abstraction, or 'outcropping' of the sandstone beds in the Coal Measures geology that lies below the town. The terraces are formed from 150mm and 300mm steps, creating a combination of easy access up or along the terraces and providing many opportunities for informal sitting. Yorkstone is chosen as the construction material for the terraces based on both underlying geology and the use of Yorkstone as a traditional town centre building material, as reflected in buildings lining Church Street. In addition Yorkstone pavements form part of the original 1930's Town Hall design and are a key component in the palette proposed for surrounding streets in the Town Centre Public Spaces Strategy for the town centre. Colourful low planting flows along some terraces, creating a softer, welcoming feel to the space and adding seasonal change. The extent of planting increases on the northern edge of the terraces to create some separation from the Green Sprint, which would run between the terraces and memorial gardens to the immediate north. A Portland Stone plinth is proposed on the eastern edge of the terraces to act as a visual anchor and marker point in views from Market Hill. More practically, the plinth would contain the existing telecommunications and signal boxes.

The new square reflects the proportions of the southern Town Hall façade, extending to approximately 30m x 40m. The square slopes from east to west following existing gradients

(approximately 1 in 11) but would fall more gently (1 in 50) from north to south. The square would be surfaced in Yorkstone setts, creating an attractive yet robust surface suitable for events or occasional parking/ vehicle access if required. The northern edge of the square runs flush across a vehicle access strip, the Green Sprint and refurbished Yorkstone flag paving up to the existing Town Hall boundary walls. The square is surrounded on its western, southern and eastern edges by trees and colourful low planting, with a series of alcoves set off the main body of the square, incorporating comfortable seats with varying aspects. The centre of the square would be animated by a series of arcing fountains, rising from strips of flush black granite (envisaged as abstract coal seams cutting through the sandstone surface). Pedestrian connections are provided to St Mary's Place, Shambles Street and the terraces, in addition to the east-west Green Sprint route running across the northern edge of the square. At the north western corner of the square the entrance to Experience Barnsley is improved and highlighted by new flight of Yorkstone steps, creating direct access off the square in addition to the existing ramp provision. A new glass sculpture is proposed to further highlight the entrance and mark Experience Barnsley as a destination point, both within the town centre and as a node on the Green Sprint. To provide additional informal seating and create a slight sense of separation for the Green Sprint through route and the main body of the square, a series of Portland Stone slabs are included on the northern edge of the square. The slabs create a visual echo of the Town Hall construction materials and would be worked to show abstract 'linen' patterns, reflecting Barnsley's industrial history and creating a further link to Experience Barnsley.

Both spaces extend onto the existing wide pavement of Shambles Street, with a minimum 2.5m footway left beyond the extent of the scheme.

Existing 'Hidden Barnsley' signage and map would be relocated within the site at an agreed new position.

Indicative proposals are shown for the memorial gardens site to the north, including refurbishment of existing paved surfaces and steps, replacement of concrete flags with Yorkstone around the war memorial and creation of clear link between the war memorial and Town Hall entrance through provision of a plate of Yorkstone setts across the existing road surface. Please note that these works would form part of a separate project in the future and are not included in this application.

PROPOSED SCHEME (not to scale)



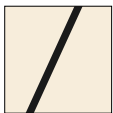
## KEY



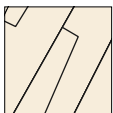
Green Sprint - reused existing green/grey setts, new sawn & flame finished setts to match or a combination of the two. New step treads and trims to match.



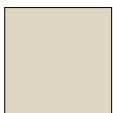
Existing Yorkstone flags lifted and relaid around Town Hall perimeter, supplemented with new flags to match where necessary. Reclaimed or new Yorkstone kerbs and trims to match.



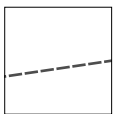
New Yorkstone setts 100mm deep to public square, suitable for vehicle loadings, with black granite trims.



Yorkstone terraces, 150mm & 300mm risers, buff front kerb, blocks and flags and blue/ grey setts/ planks as terrace infill.



Hot rolled asphalt with buff stone chippings.



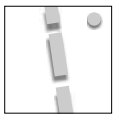
Stainless steel handrails.



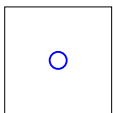
Proposed trees, small sized compact species, 2.2m clear stems.



Colourful annual & herbaceous perennial planting with some evergreen shrub structure (Box balls), designed for sensory experience and changing seasonal colour.



Indicative seat and litter bin positions.



Indicative lighting column positions, galvanised column with spotlights attached..



Portland Stone block plinths, with 'linen' artwork inscribed.

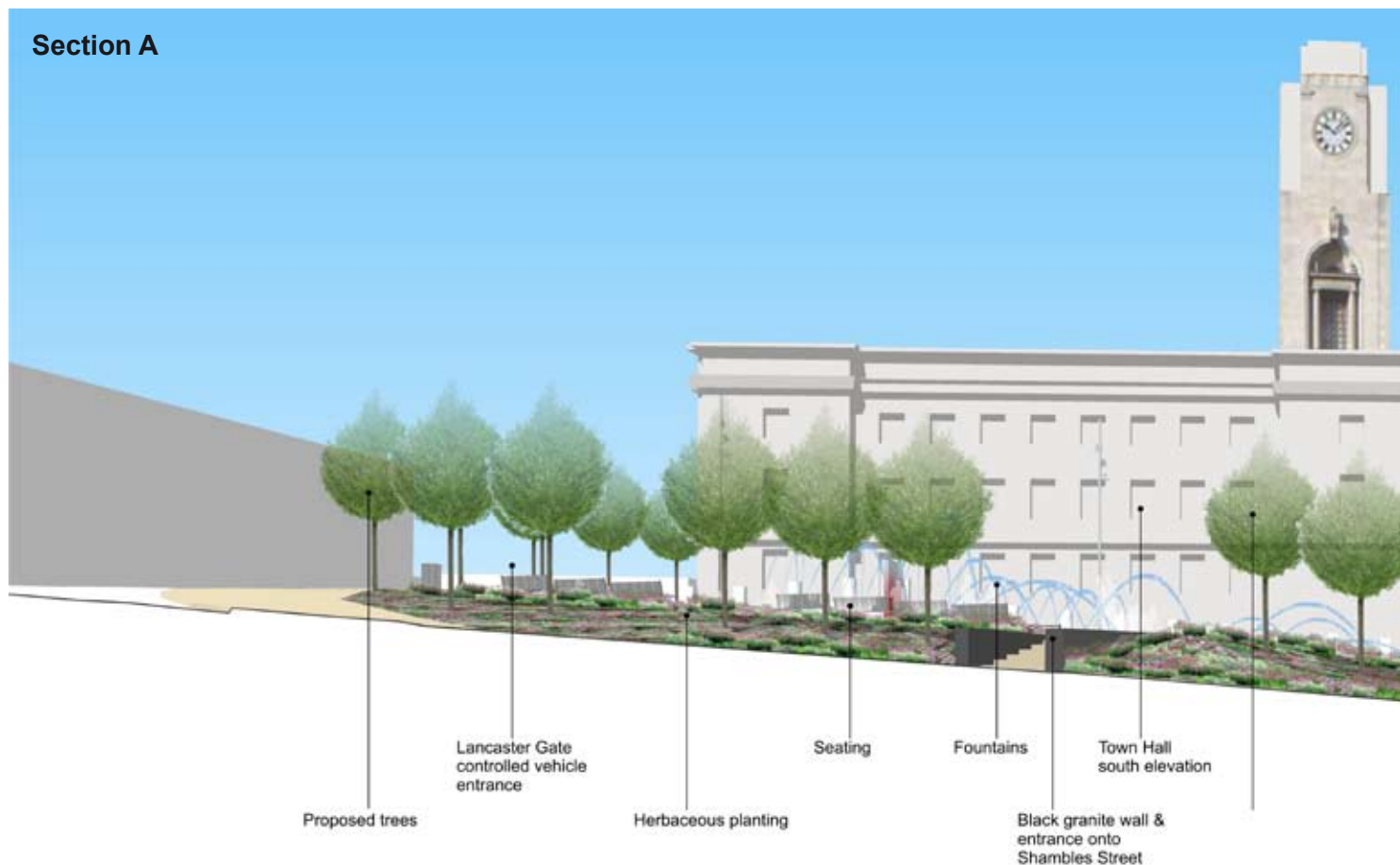
PROPOSED SCHEME VIEWS



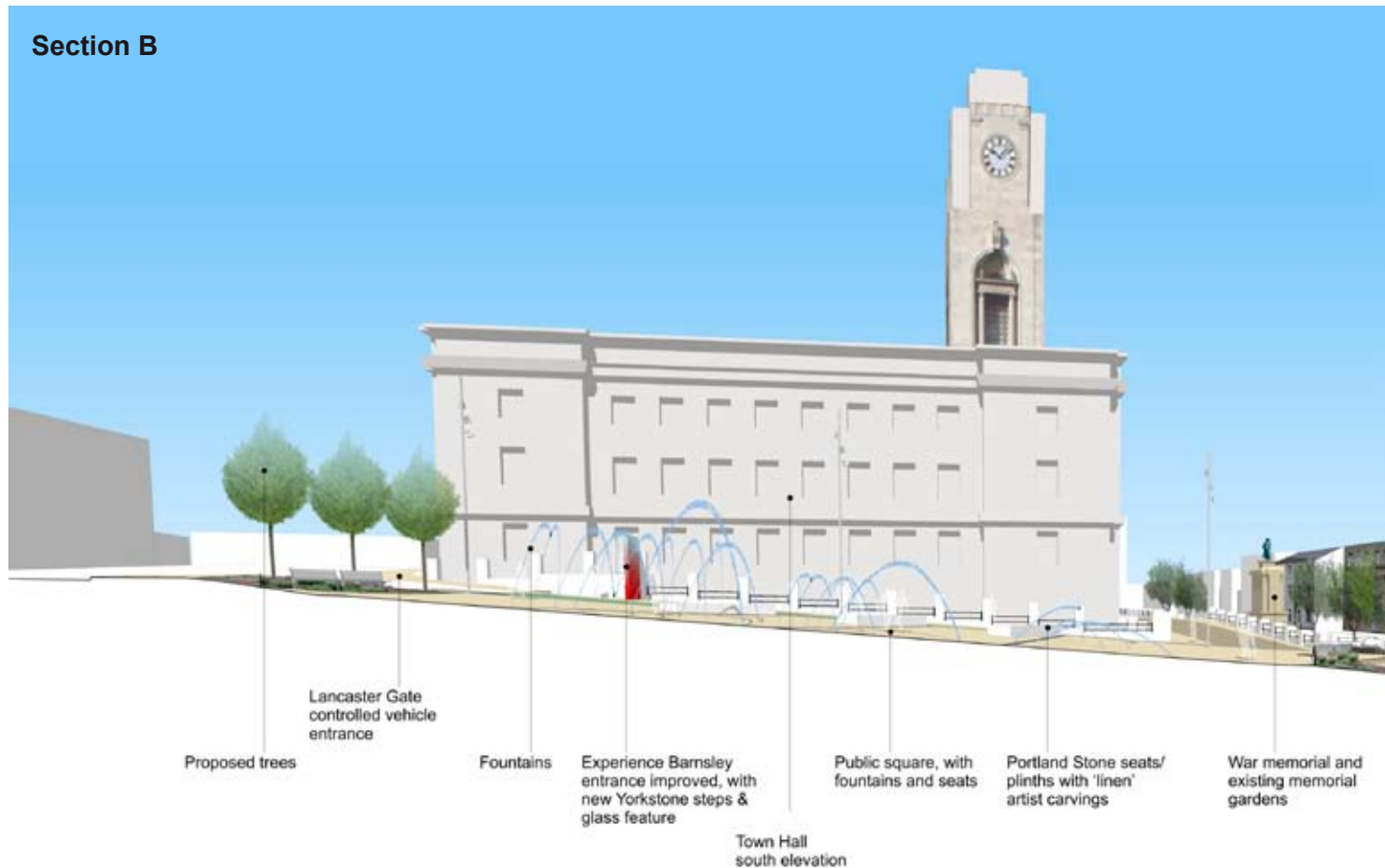
PROPOSED SCHEME CROSS SECTIONS



Section A



Section B

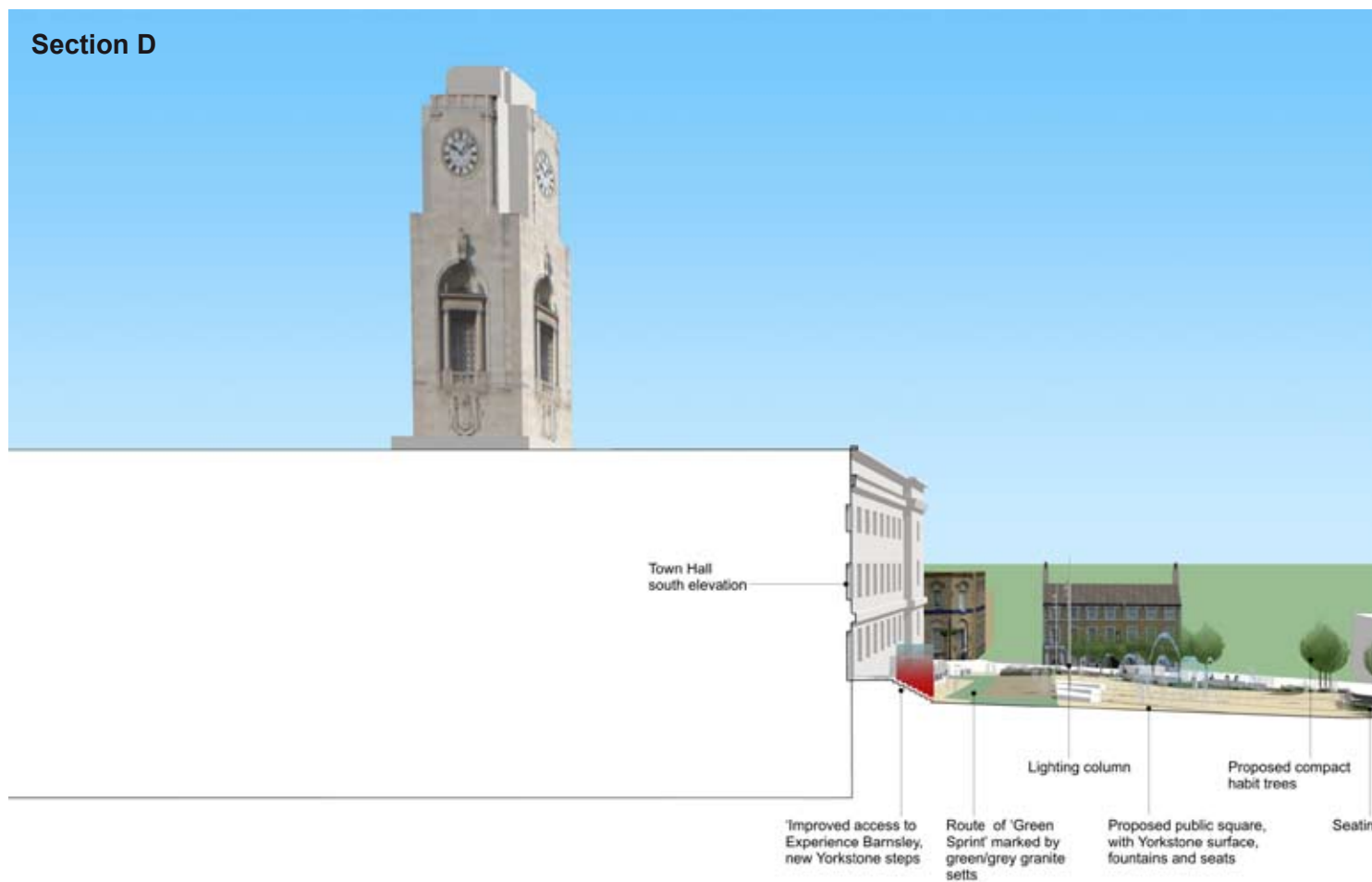




Section C

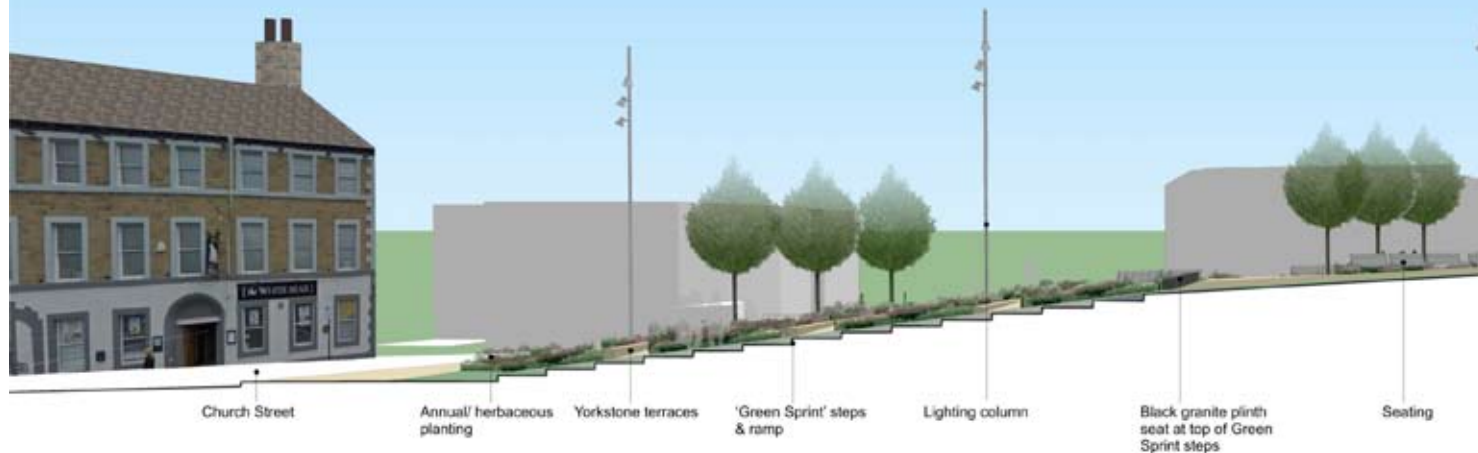


Section D



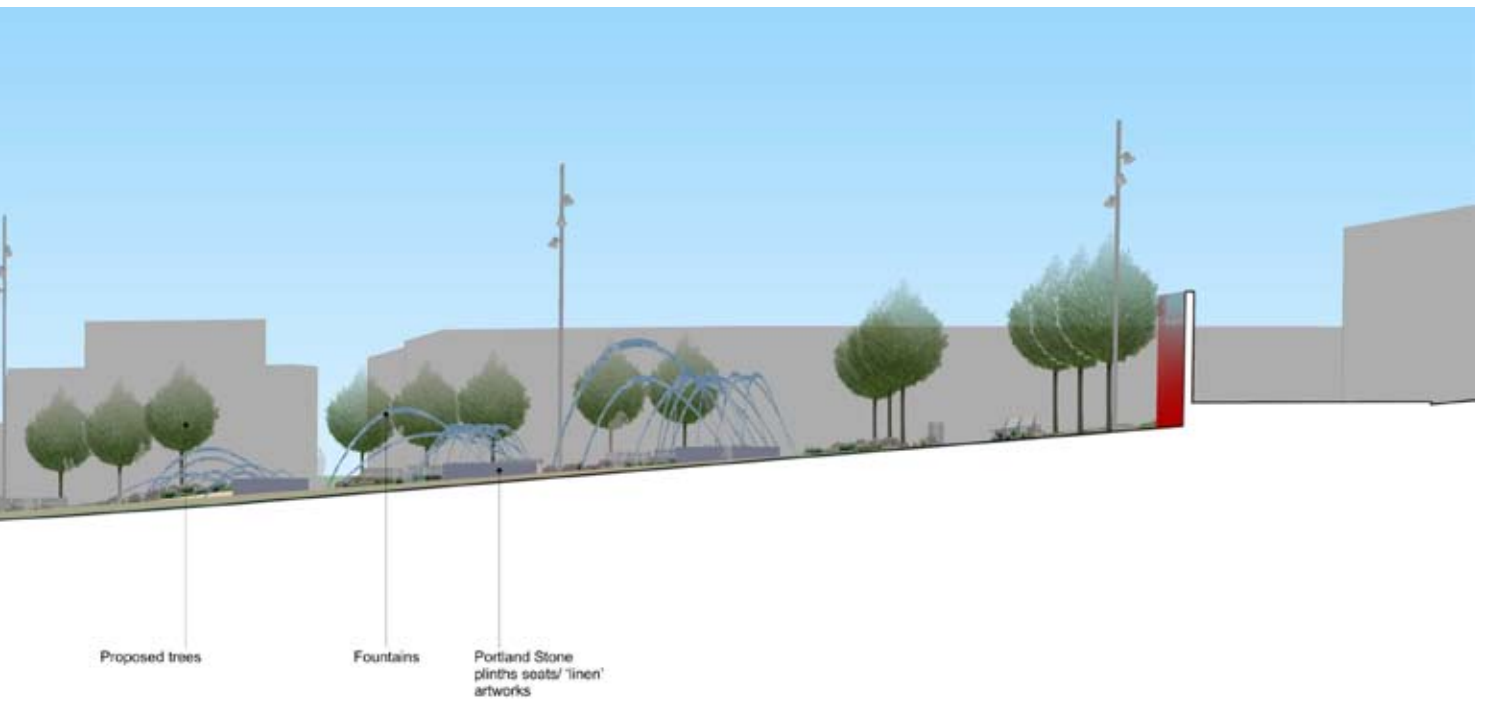


Section E



Section F





**4.2 Access**

Vehicular entry to the site from St Mary’s Place onto Lancaster Gate would remain but would be controlled, either by automatic retracting bollards or a surface gate. The St Mary’s Place entrance would remain two way entry, with the Sadler Gate entrance remaining as exit only. The vehicular route around the Town Hall would be surfaced in hot rolled asphalt with buff stone chip, providing a clear guide for drivers but in materials which are sympathetic to the new scheme. Existing Yorkstone kerbs would be reused throughout, being set flush along the southern Town Hall frontage. As part of future works along the eastern frontage kerbs would be set to 100mm height, with the exception of the new flush Yorkstone sett surface between the war memorial and main Town Hall entrance.

**4.3 Green Sprint**

The route of the Green Sprint would be marked by grey/ green granite setts. Either the existing setts under Lancaster Gate would be cleaned and reused or new stone of a matching colour and texture would be selected. At this stage it is not known how much of the existing hidden sett surface remains or would be usable.

The Green Sprint would enter the site from the east before rising up a combined ramp (1 in 10 following existing levels) and steps arrangement, with central handrail. Access from the ramp section would be possible onto several of the terraces to the south. From the top of the ramp the Green Sprint would level out where it crosses Lancaster Gate before rising (at 1 in 11 following existing ground levels) to follow the southern edge of the Town Hall. Beyond the site, a Puffin crossing would be installed in the future to create easier access for pedestrians onto Westgate.

**4.4 Vegetation**

Trees with a small/ medium compact form would be selected to filter views within and around the site, creating a degree of enclosure, softening the space and bringing seasonal colour and texture to this part of the town centre. Trees would be planted at 25-30cm girth semi-mature size with minimum 2.2m clear stems to underside of canopy allowing clear sightlines below for comfort and security. Underground guying and coil irrigation systems would be provided within tree pits backfilled with approved type subsoil and topsoil.

Drifts of herbaceous/annual planting, rising up up to waist



Vehicle surfaces - asphalt with buff stone chip



Kerbs - Yorkstone  
Green sprint - green/grey granite setts  
(to match existing)



**Acre campestre 'Elsrijk'**  
(compact, upright habit)



**Evergreen structure - Box balls**

height, would surround the square and flow through the terraces, helping to break up the expanse of stone surfaces.

Occasional groups of clipped Box balls would be inserted to provide evergreen colour and structure. Herbaceous and annual species would be selected for scent and texture, acting as a replacement for the existing sensory gardens but on a much larger scale across the whole space.

Existing soils would be removed from site and a fresh layer of 400mm deep approved topsoil placed over either existing subsoil or approved imported subsoil. Land drainage or local soakaways would be provided if drainage problems are noted with underlying soils.

Discrete stainless steel rails would be provided to prevent shortcutting through beds at vulnerable locations.

All planting would be subject to a defects replacement guarantee and initial maintenance establishment period, followed by handover to BMBC maintenance teams.

#### 4.5 Seating & Furniture

Groups of formal seats would be provided across the terraces and around the square in order to create a wide range of sitting opportunities for people, with varying degrees of aspect and shade. Formal seats would be selected for comfort and robustness, at this stage stainless steel seats are envisaged.

The terraces themselves act as informal seating opportunities, with extensive runs at 300mm height.

Litter bins would be provided at site entrances and key locations near seats. Bins would be selected to match seat design and materials.

Light columns would be tapered galvanised steel, selected for simplicity of appearance and hard wearing capability.



**Herbaceous planting**  
(High Line, New York)



**Annual planting (France)**

4.6 Water Feature

The proposed water feature would operate on the ‘dry plaza’ principle, allowing the feature to be switched off and the square to operate as a normal hard surface during events.

The feature is designed to reflect a tumbling Pennine stream, flowing westwards, downslope from the higher side of the Barnsley District. Curving jet shapes reflect this tumbling motion but are also known to provide excellent play opportunities for children, in addition to being visually appealing. Red and yellow night time lighting would add a further dimension to the water feature, reflecting the colours of molten glass, an association with a key historical and current industry in Barnsley.

Three banks of flush in-ground nozzles would create varying arcs and jet widths aimed down the slope of the square. The banks would be set to heights rising westwards, to exaggerate the natural fall of the square.

Jets would be sequenced to be all on at the same time or timed to work in bands, either randomly or moving downhill. Low power operation mode would be all off or one bank working at any one time, then increasing to full power effects for busy summer lunchtimes and weekends. In-ground flush LED units would provide effective low energy uplighting.

Water would be treated to meet standards required for use in public places, with automated pH and chemical dosing systems and regular checks by maintenance personnel.

Fountain nozzles would be set flush to ground level within rows of black granite cover grates over precast concrete channels below. Channels would house nozzle supply pipes and would also provide return drainage to a holding tank.

The holding tank would be set underground within the square (location to be determined). The water feature control room and treatment plant would either be located within the Town Hall or again set underground within the square.



Example of arcing water jets  
Market Square, Nottingham



Weir on River Dearne



Glass blowing (curve and colour)

### 4.7 Surface Water Drainage

The site would be drained via the existing surface water drainage system, connected to the highway drainage network. Gulleys would be provided along Lancaster Gate with a channel drain located at the top of the Green Sprint ramp and steps.

A surface channel and gulleys set along the southern edge of the square would collect surface water from the main body of the square although the majority of this water would be directed back through the water feature feature system, with an overflow connection to the foul sewerage network.

The terraces would drain directly to adjoining planting beds or fall directly to the highway drainage system.

### 4.8 Surface Materials

Yorkstone would be the predominant surfacing material across both the terraces and the square.

Terraces would be formed from buff (eg Crosland Hill) Yorkstone front edge blocks (typically 300 x 300mm section, changing to random block size to full terrace depth at narrower points) with buff/ grey/ blue (eg Scoutmoor) narrow Yorkstone planks within terrace treads. This approach would create a clean, regular appearance to the terraces when viewed from Market Hill, which then opens up to a greater level of detail as one moves through the terraces.

The same buff/ grey/ blue Yorkstone palette would be used in setts across the square, with the fountains bringing the colours alive. Black granite bands cutting across the square would be textured to create grip and visual interest. Granite type would be selected to match road kerbs used in the Town Centre Public Spaces Strategy.

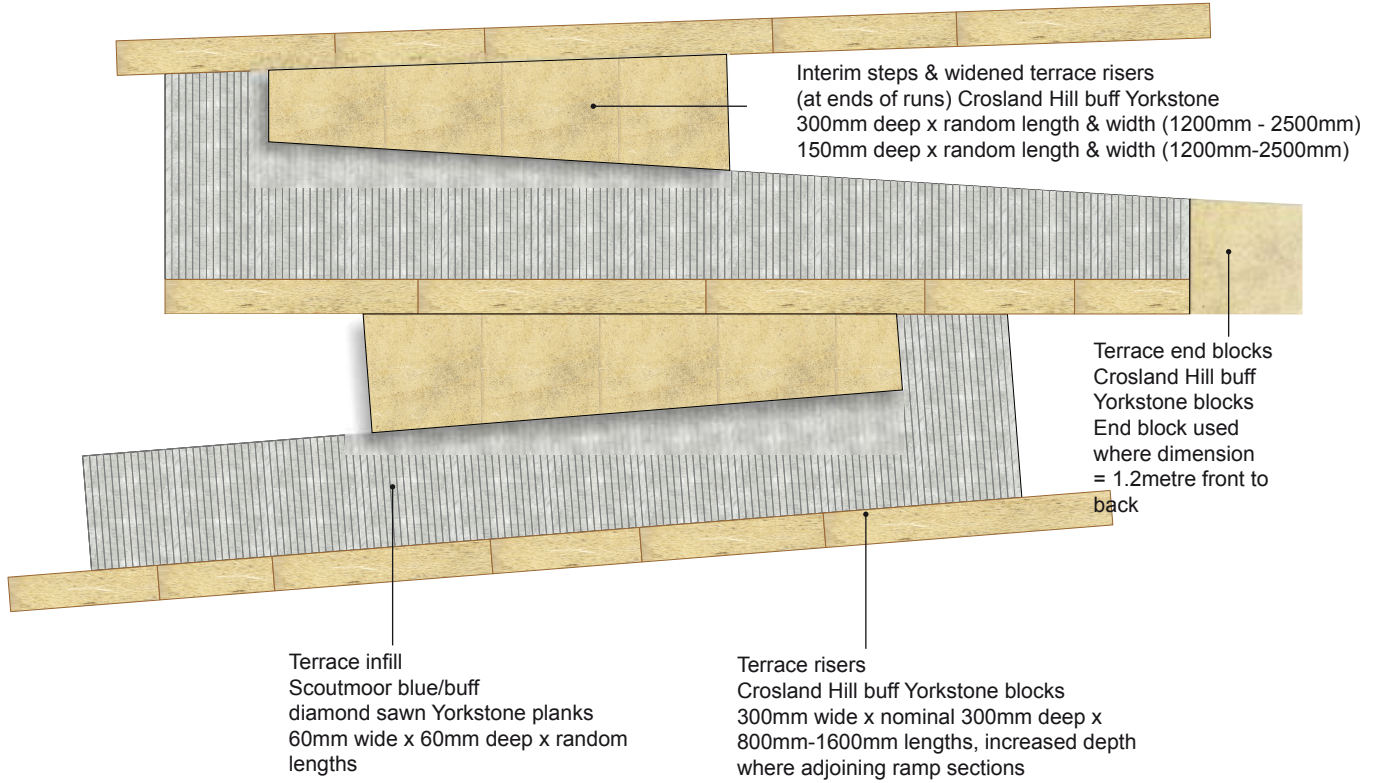
Existing Yorkstone flags and kerbs would be relaid around the Town Hall perimeter, with new Yorkstone selected to replace broken pieces.

Hot rolled asphalt with coated buff stone chip would be used within the vehicle zone around the Town Hall.

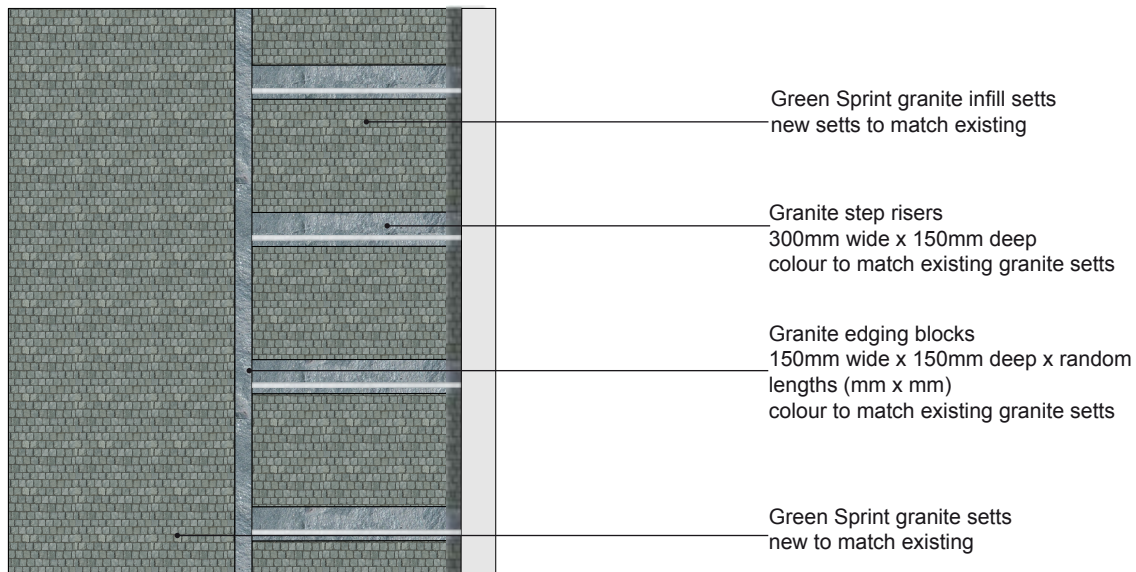
Existing green/ grey granite setts, or new to match, would be used along the Green Sprint. New green/ grey granite risers and edge trims would be used on the ramp/ steps section of the Green Sprint and where detail is needed at other locations.

Proposed Surface Materials

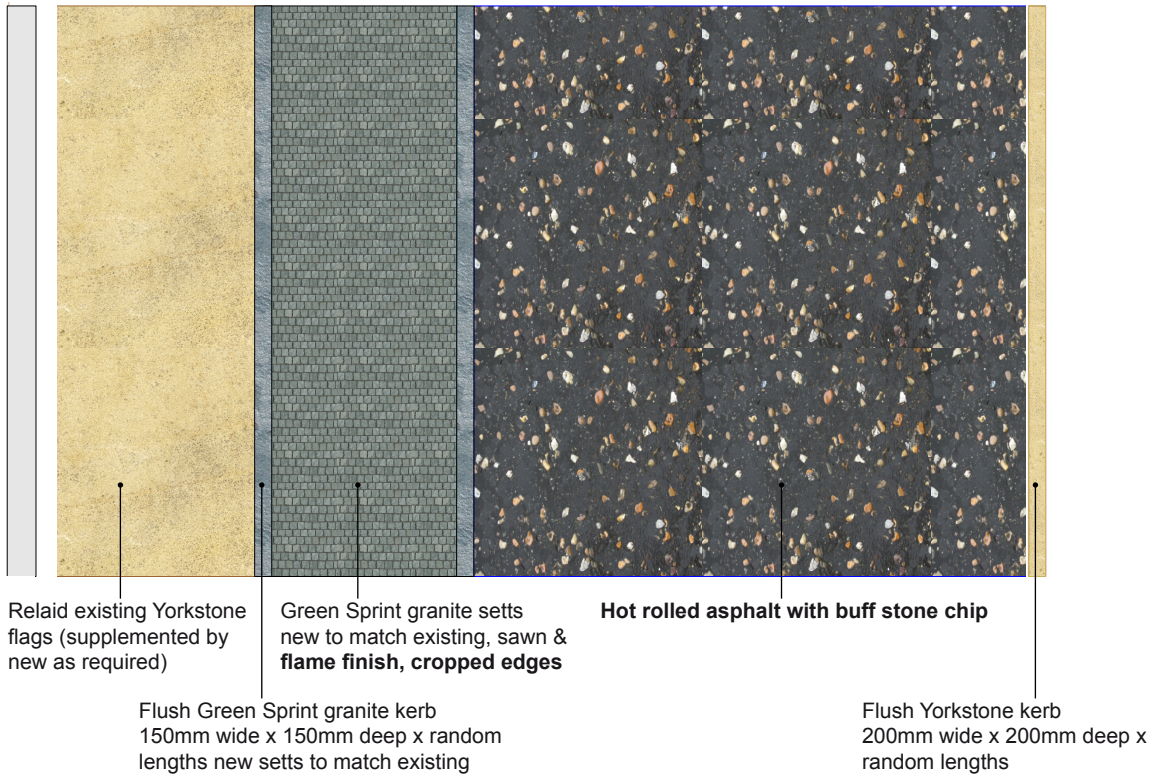
Garden Terraces



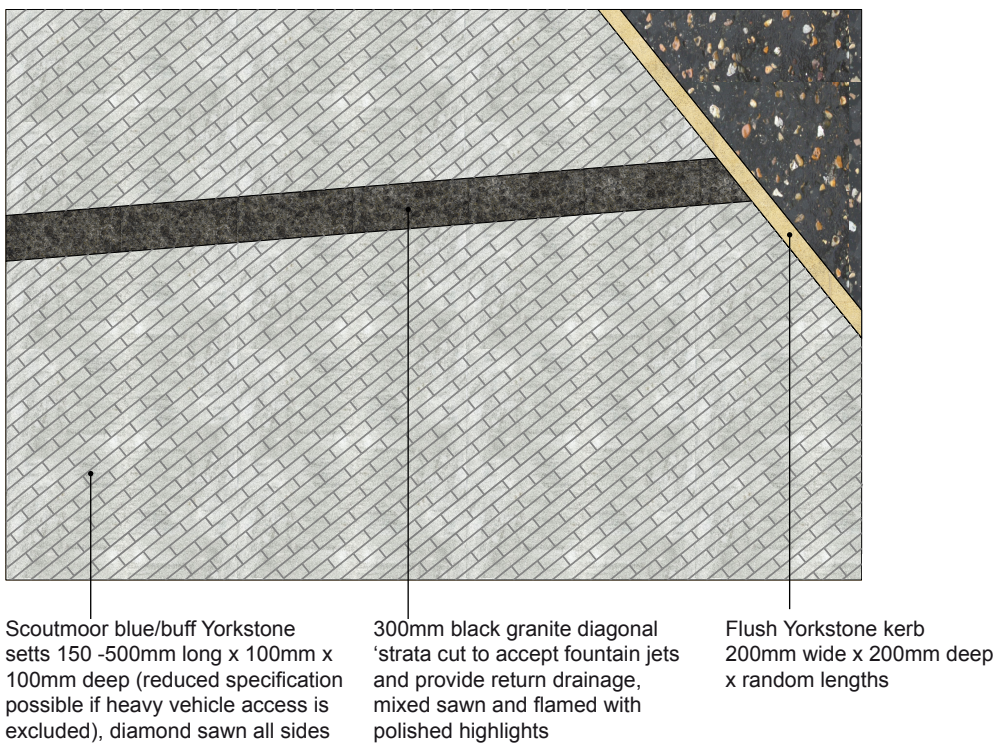
Green Sprint



### Town Hall Frontage



### Town Hall Square



### 4.9 Lighting Proposals

Primary lighting through the scheme would be provided by directional downlights supported on a line of 9-10m columns running east-west through the site along the line of the Green Sprint.

Magenta colourwash lighting directed from discretely designed and located bollards would illuminate the terraces, echoing the colour of lighting used within the Town Hall tower.

Flush in-ground LED uplighting would be provided below fountains to accentuate splash zones.

Surrounding highway lighting may also be adjusted to give some backlighting to scheme edges.

### 4.10 Car Parking

The proposed scheme removes the existing car park from the area immediately south of Lancaster Gate. This will result in the loss of 18 public parking spaces and 2 disabled spaces. The proposed scheme will also remove the adjacent 16 permit-only spaces for Council Members and Chief Officers.

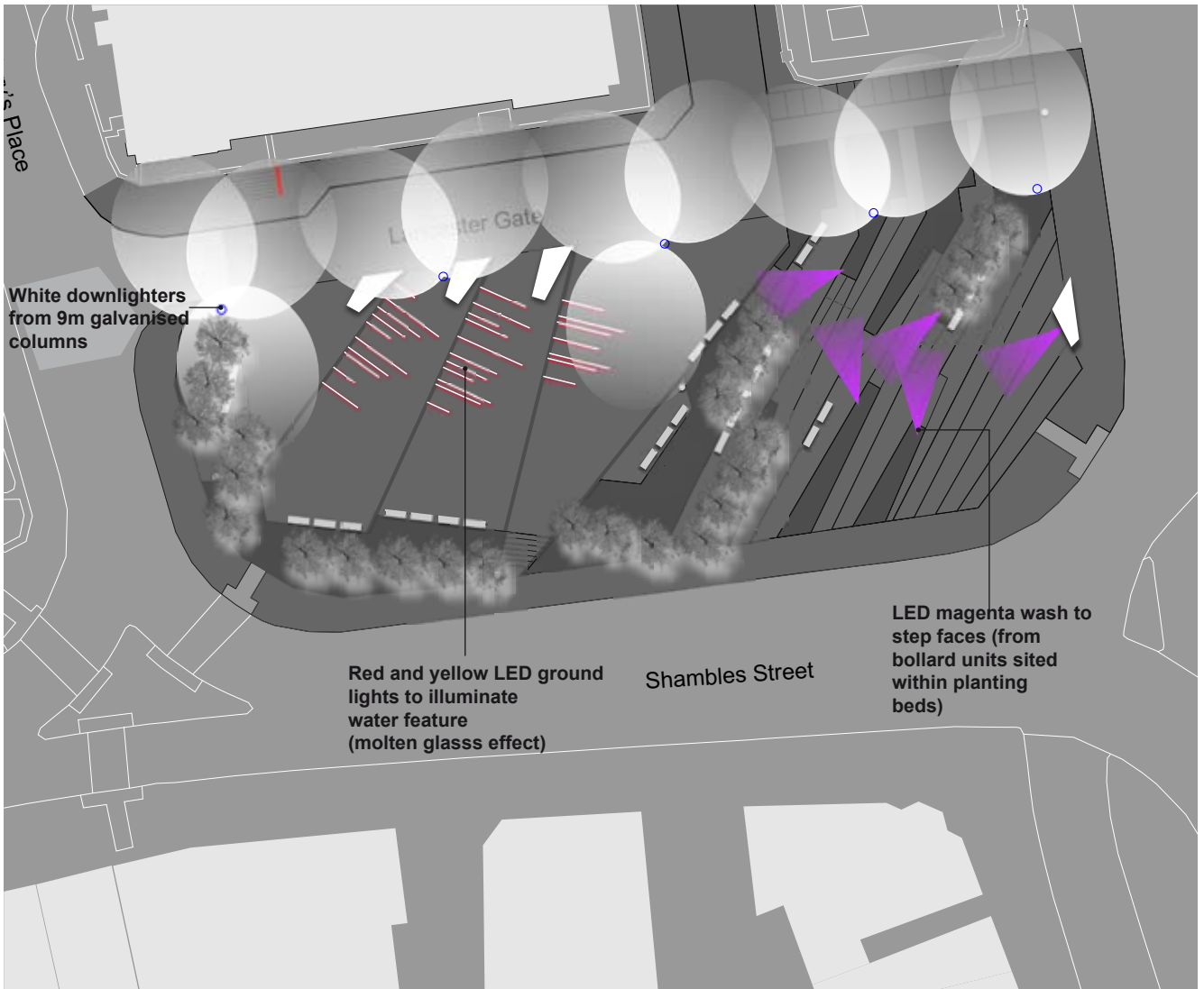
Lancaster Gate Car Park is a short stay (1hr) car park mainly utilised by shoppers and visitors to the Town Hall. There is currently an underutilised short-stay car park on St Marys Place with 12 spaces and also short stay on-street parking on St Marys Gate (9 spaces). Additional, adjacent, short stay parking is also available in the Westgate and Grahams Orchard car parks.

Alternate parking for permit holders has been found on Westgate.

Additional signing to highlight the available short stay parking in the immediate vicinity of the Town Hall will be installed prior to the closure of Lancaster Gate car park.


It is considered that removal of the parking from the site enhance the setting of the Grade 2 listed Town Hall and allow safer pedestrian flow for visitors to Experience Barnsley.

PROPOSED SCHEME LIGHTING





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<p>Scale 1:2500 at A4</p>	<p><b>Regent Street/Church Street/ Market Hill Conservation Area</b></p>	 <p><b>BARNLSLEY</b> Metropolitan Borough Council</p>
<p>Drawn by: TC</p> <p>Date: 20/09/2007</p>		<p><b>Planning and Transportation Service</b>                  Assistant Director: Ruth Middleton, BA(Hons), DipSurv, MRTPI                  PO Box 604, Barnsley, S70 9FE                  Tel: (01226) 770770 Fax: (01226) 772599</p>

### 4.11 Heritage Aspects

#### 4.11.1 Context, Prospect and Setting

The area that includes the site lies within the Regent Street/ Church Street/ Market Hill Conservation Area close to Barnsley town centre. Barnsley Town Hall is a grade 2 listed building and is accompanied by a number of other listed features in its immediate setting including the war memorial and the piers and railings of the memorial gardens.

The Town Hall sits in a raised position above the town centre with the building looking out in a south easterly direction towards the shopping area of Market Hill. The southeast corner of the Town Hall has therefore become a key view which must be respected. Due to its elevation, position, quality of architecture and design and function the Town Hall is arguably the premier historic and civic building in the town.

The Town Hall site is in close proximity to several other listed buildings within the wider context of the conservation area including period architecture on Church Street and Regent Street, which underlines the civic importance of the area for Barnsley as a whole.

The Regent Street, Church Street and Market Hill conservation area constitutes the historic core of Barnsley town centre and the centre of the boroughs political and commercial functions. The conservation area is dominated by Barnsley Town Hall and contains thirty six listed buildings of various ages and architectural styles. The conservation area can be roughly divided into two zones. Church Street / Regent Street form the civic, legal and administrative core whilst the area around Market Hill / Eldon Street lends itself to commercial uses. A number of Barnsley towns' most prominent pieces of architecture are found within the conservation area including the Town Hall, the Civic Hall (The Civic), St Mary's Church and a sequence of fine Georgian properties along Regent Street. A number of commercial properties are converted Georgian town houses and large formal Victorian office buildings. The university campus and the Cooper art gallery are also situated within the conservation area boundary. Buildings vary from two stories to five, but the topography of the area is a dominant feature; defining views and vistas both within and towards the town.

### 4.11.2 Effects on Listed Buildings and Features

The scheme as proposed will not result in any significant changes to the Town Hall building, its fabric or outlying features such as the listed piers and railings bounding the frontages of the building and memorial gardens.

Minor changes will be made to the southern Town Hall elevation to allow for a new entrance to 'Experience Barnsley'. To accommodate this change, minor alterations to the stone piers and metal railings will be required. In brief, changes will include the construction of a new and widened access (of the existing Lancaster Gate entrance) with steps marked by a glass panel entrance feature. The new steps will be constructed in Yorkstone, reflecting the existing use of this material in adjoining paved areas but marking out the new structure as a modern addition to the Portland Stone fabric of the Town Hall itself.

The proposed scheme retains the existing Yorkstone pavements and kerbs along the eastern frontage of the Town Hall and makes use of the existing green/grey granite setts. The use of new Yorkstone, bands of granite and careful use of Portland stone blocks will help tie the scheme into the immediate setting of the Town Hall and wider character of the conservation area, maintaining its current immediate character and creating an attractive open space fronting onto the south elevation of the Town Hall.

The overall effect of the proposed scheme will be to open out a significant space on the southern side of the Town Hall, de cluttering the existing scene and allowing an improved appreciation of the southern Town hall elevation.

Key views of the Town Hall from Market Hill and Shambles Street will be retained and enhanced in the proposed scheme.

### 4.11.3 Effects on Conservation Area

The proposed scheme is designed to respect and enhance the current heritage setting, creating an improved context for the Town Hall and relating it better to the overall wider space within the Regent Street/ Church Street/ Market Hill conservation area.

At present the southern frontage to the Town Hall is dominated by a surface car park which detracts from the overall setting of the southern elevation. Unattractive raised beds and

structures at the south east corner of the site would also be removed and replaced with a series of Yorkstone terraces, creating a visual 'platform' for the Town Hall in the key view from Market Hill.

Views to the Town Hall from Shambles Street would be filtered by proposed trees but would retain clear sightlines to the southern Town Hall elevation when entering the town centre from the west.

The materials palette chosen for the design responds to the traditional use of Yorkstone in this part of the conservation area (on both pavements and building facades) and also reflects the approved materials palette in the Town Centre Public Spaces Strategy, which is expected to be used in future improvements to the streets within this part of the conservation area. Granite bands in the public square will help delineate the space and break down the expanse of Yorkstone providing a transition between the existing frontage, new public square and Yorkstone terraces.

List of Heritage Assets within the setting of the site

Church Street, Regent Street, Market Hill Conservation Area.  
Designated November 1973 (BCBC), Extended April 1977

### Listed Buildings:

- 1315006 - 23 Church Street - Grade 2
- 1151142 - White Bear / The Royal Hotel - Grade 2
- 1151143 - Barnsley Town Hall - Grade 2
- 1315007 - Piers and Railings - Grade 2
- 1151144 - War Memorial in front of Town Hall - Grade 2
- 1315008 - Church of St. Mary Grade 2\*
- 1151145 - Gate piers and Walls to Church of St Mary's Grade 2
- 1286924 - 12 & 14 Market Hill Grade 2
- 1151157 - 16 Market Hill Grade 2
- 1191723 - Yorkshire Bank Market Hill Grade 2
- 1151158 - The Old Number 7 Market Hill Grade 2
- 1151124 - Cass's Warehouse St Mary's Place Grade 2
- 1286819 - Former Carriage House to Cass's Warehouse St Mary's Place Grade 2

#### 4.12 Inclusive Access

The site is heavily constrained by steep gradients, fixed around its perimeter by existing highways and listed heritage features. Within this significant constraint a number of features are provided in the new scheme to aid inclusive access into and through the site:

- improved access along the route of the Green Sprint near the memorial gardens, with choice of shallow steps or ramp (at existing 1 in 10 gradient) including new central handrail, followed by flush surfaces and removal of traffic along the southern frontage of the Town Hall;
- level terraces with 150mm interim steps giving access up from Market Hill to the proposed new square and with three terraces accessible by wheelchair between the Shambles Street pavement and the Green Sprint ramp. Handrails would be provided where steps form short flights, although it should be noted that even here steps are formed with very generous treads;
- the existing access ramp to Experience Barnsley would be retained, with a new flight of 150mm steps, with handrail, being provided to give more direct access;
- the proposed square rises generally at the existing 1 in 11 gradient but has been given a gentle 1 in 50 east-west cross fall;
- steps on the Green Sprint and running through the terraces would be marked with white and black inlaid high visibility nosing respectively;
- tactile marker paving would highlight the top and bottom of the Green Sprint ramp and steps;
- comfortable seats with armrests would be provided at frequent intervals across the terraces and around the edges of the square.

Two existing blue badge parking bays would be lost within the Lancaster Gate car park. Replacement blue badge parking is available nearby at the existing car park on St Mary's Place.

The existing sensory gardens would be removed. Planting design within the new scheme would include extensive sensory elements (eg scent, texture, bright colours) throughout and specifically near seats.

# 5. PLANNING POLICY

### 5.1 Local Planning Guidance

The design adheres and responds to the following relevant policies from the current Core Strategy of Barnsley's Local Development Framework adopted September 2011.

#### CSP 29 Design

##### Design Principles:

'High quality development will be expected, that respects, takes advantage of and enhances the distinctive features of Barnsley, including:

- topography, Green Infrastructure assets, important habitats, woodlands and other natural features;
- views and vistas to key buildings, landmarks, skylines and gateways;
- heritage, townscape and landscape character including the scale, layout, building styles and materials of the built form.

##### Development should:

- contribute to place making and be of a high quality, that contributes to a healthy, safe and sustainable environment;
- help to transform the character of physical environments that have become run down and are lacking in distinctiveness;
- enable all people to gain access safely and conveniently, providing, in particular, for the needs of families and children, and of disabled people and older people contribute towards creating attractive, sustainable and successful neighbourhoods.

### CSP 30 The Historic Environment

‘We will positively encourage the management, conservation and enjoyment of Barnsley’s historic environment and make the most of the heritage assets which define Barnsley’s local distinctiveness. Development which affects the historic environment and Barnsley’s heritage assets and their settings will be expected to protect or improve:

- the character and/or appearance of Conservation Areas;
- the character and/or appearance of Scheduled Ancient Monuments;
- the character and/or appearance of Listed Buildings (including any locally listed buildings or buildings of archaeological significance);
- archaeological remains of local or national importance;
- the character and/or appearance of historic parks and gardens and other historic landscapes including key views from and within these landscapes’.

### 5.2 National Planning Guidance

In addition the proposed design meets the aims of national guidance as set out in the National Planning Policy Framework which states that local policy should encourage schemes that:

- function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- establish a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live, work and visit;
- respond to local character and history, and reflect the identity of local surroundings and materials;
- create safe and accessible environments;
- are visually attractive as a result of good architecture and appropriate landscaping.