

05 DESIGN EVOLUTION

05.7 CASE STUDY CONCEPT DEVELOPMENT

GATEWAY 36 | BARNSELEY



Planning External Materials Specification		
Ref	Specification	Colour
1	Profiled metal cladding	Olive Green RAL 100 30 20
2	'Mansafy' triple skin glass reinforced polyester (GRP) Roof lights 15% target maximum	
3	Photovoltaic Panels 15% target	
4	Composite Wall Panel, horizontally laid with preformed corners. Flashings to be pressed metal Plastisol HPS200 coated with preformed corners. Panel thickness to suit M&E U-Value calculations.	Olive Green RAL 100 30 20
5	Composite Wall Panel, horizontally laid with preformed corners. Flashings to be pressed metal Plastisol HPS200 coated with preformed corners. Panel thickness to suit M&E U-Value calculations.	Khaki Green RAL 100 60 20
6	Built up profiled metal cladding (laid horizontally). Flashings to be pressed metal Plastisol HPS200 coated with preformed corners.	Goosewing Grey RAL 090 70 05
7	Feature flat panel rain-screen metal cladding (laid horizontally)	4 no. Colours
	Colour 1	Olive Green RAL 100 30 20
	Colour 2	Sand Brown RAL 100 70 20
	Colour 3	Pale Beige RAL 100 60 10
	Colour 4	Traffic White
8	P.P.C. Aluminum thermally broken anti sun double-glazed doors & screens. Spandrel glazing panels shown hatched.	Goosewing Grey RAL 090 70 05
9	P.P.C. Aluminum thermally broken anti sun double-glazed Windows. Opening windows to be provided where necessary. Spandrel glazing panels shown hatched.	Goosewing Grey RAL 090 70 05
10	Fire exit doors to be High Security doors, vandal-resistant heavy duty steel door sets Plastisol HPS200 coated	Goosewing Grey RAL 090 70 05
13	Parapet flashing to be pressed metal Plastisol HPS200 coated with preformed corners	Goosewing Grey RAL 090 70 05
14	Level access door to be plastisol-coated, insulated, sectional overhead shutters, Size - 3500 x 4,500mm. Panel thickness to suit M&E U-Value calculations.	Khaki Green RAL 100 60 20
15	Dock leveller doors to be plastisol-coated, insulated, sectional overhead shutters, 2,800 x 2,750 mm. Panel thickness to suit M&E U-Value calculations.	Khaki Green RAL 100 60 20
16	Dock Shelter	
18	Stainless steel bollards	
19	Heavy Duty Bollard	



Cladding Panels
Colour: Olive Green



Cladding Panels
Colour: Khaki Green



Cladding Panels
Colour: Sand Green



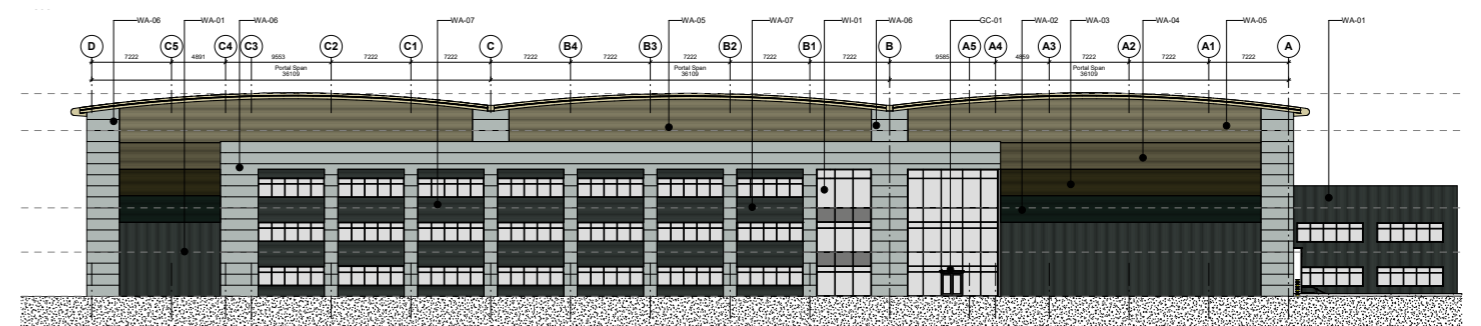
Cladding Panels
Colour: Pale Beige

Gateway 36 is a high-quality industrial and logistics scheme located in Barnsley - immediately adjacent to J36 M1 - where Phase 2 of the development is currently under construction.

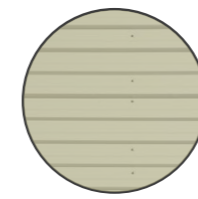
The first three units, comprising 110,000 sq. ft of Grade-A trade, industrial and logistics space, marketed as 'R-Evolution 36' are now complete and available for immediate occupation.

Aesthetically a range of cladding configurations have been combined as illustrated on the above CGI and elevation. The use of green cladding aids in blending the building into the immediate surrounding green landscaping. The combination of pixelated cladding and solid horizontal cladding adds visual interest and creates a more impactful elevation.

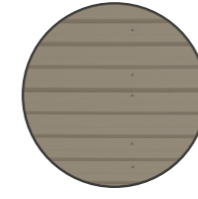
BARNSELEY 340 | BARNSELEY



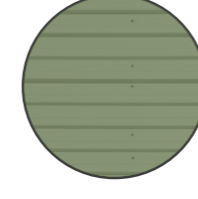
EX-01	Personnel access door	Colour: To match adjacent cladding
GC-01	Glazed Canopy	
RF-01	Profiled roof cladding	Colour: Meadowland RAL 100 80 20
WA-01	Vertically laid Built up profiled wall cladding	Colour: Merlin Grey RAL 180 40 05 BS18B25
WA-02	Horizontally laid Built up profiled wall cladding	Colour: Ivy RAL 170 20 10
WA-03	Horizontally laid Built up profiled wall cladding	Colour: Olive Green RAL 100 30 20
WA-04	Horizontally laid Built up profiled wall cladding	Colour: Moorland Green RAL 100 60 20
WA-05	Horizontally laid Built up profiled wall cladding	Colour: Meadowland RAL 100 80 20
WA-06	Horizontally laid 'flat' composite cladding system	Colour: Pure Grey RAL 000 55 00
WA-07	Horizontally laid Built up profiled wall cladding	Colour: Merlin Grey RAL 180 40 05 BS18B25
WA-08	Prowall Concrete	
WA-09	Translucent Panel	Colour: Grey Tinted
WI-01	Glazing system & framing	



Cladding Panels
Colour: Meadowland



Cladding Panels
Colour: Moorland Green



Cladding Panels
Colour: Olive Green

Barnsley 340 sits within the Gateway 36 development, strategically located near to J36 M1.

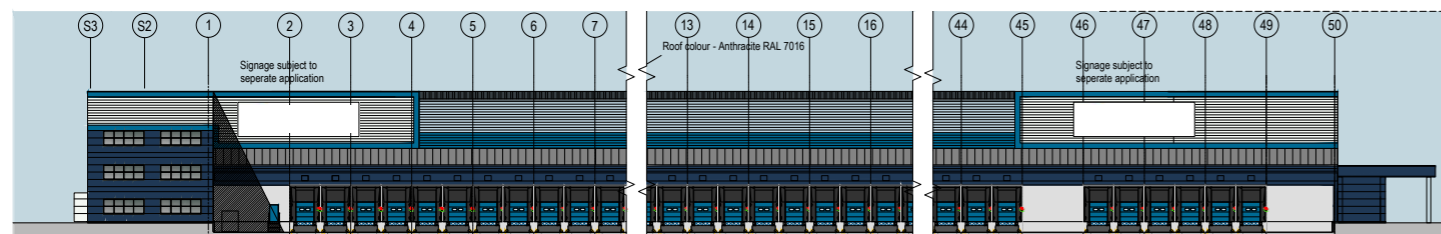
The unit will provide 340,000 sq. ft of Grade-A logistics space in a building targeting BREEAM rating 'Excellent'.

Aesthetically the cladding utilises long stretches of horizontal cladding with breaks instigated by subtle vertical sections on each elevation. The chosen colours are visually effective in terms of blending into the surrounding context, yet also creating a visually emphatic facade.

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05.7 CASE STUDY CONCEPT DEVELOPMENT

EQUITES PARK | BARNLSLEY



SCHEDULE OF MATERIALS

Office Elevations;
Office cladding, horizontally laid plastic coated metal microrib MR300 cladding (or similar approved):
External finish and colour:

- Solent Blue: RAL 240 40 40
- Sargasso Blue: RAL 5003
- White: RAL 9003

 (Colour options from standard HPS200 Ultra range)

PPC frame glazed entrance doors and windows with grey tinted glazing.

- External colour: Anthracite: RAL 7016

 Lookalike glazing panels where required, indicated darker grey (anthracite film or spandrel panels)

Main Warehouse;
Graded horizontally fitted trapezoidal profiled built up metal cladding system: external finish and colour: HPS 200 ultra:

- Ice Blue (top) RAL 230 80 10
- Solent Blue: RAL 240 40 40
- Clear polycarbonate translucent 2m strip
- Sargasso Blue: RAL 5003
- Roof - Anthracite RAL 7016

 500mm wide x 100mm deep Aluminium feature flashings / framing to wall cladding.

- Solent Blue: RAL 240 40 40

 Precast concrete panel walling to loading dock surround

Insulated composite panel Dock and Level Access doors

- Solent Blue: RAL 240 40 40

 Steel faced fire escape / personnel doors,

- Solent Blue: RAL 240 40 40

- Occupier signage, design and size to be confirmed. Subject to separate Signage Application.

Cladding Panels
Colour: Solent Blue

Cladding Panels
Colour: Sargasso Blue

Cladding Panels
Colour: White

Cladding Panels
Colour: Anthracite

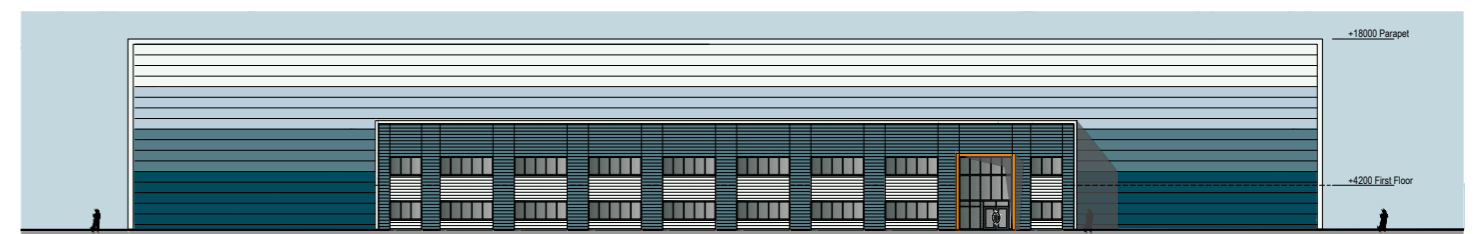
Equites Park is located immediately adjacent to junction 36 of the M1 motorway.

The building is Evri's largest European parcel sorting distribution centre providing 340,000 sq ft of space.

The cladding primarily consists of horizontally laid cladding panels providing stretches of blue and white along the facade of the building.

Aesthetically, the chosen colour palette provides a gradient effect with the sky limiting visual impact.

ARROW POINT | BARNLSLEY



SCHEDULE OF MATERIALS

Office Elevations;
Office cladding, horizontally laid plastic coated metal microrib MR300 cladding (or similar approved):
External finish and colour:

- Wedgewood Blue: RAL 220 50 15
- White: RAL 9003
- Feature entrance surround Orange: RAL 2003

 (Colour options from standard HPS200 Ultra range)

PPC frame glazed entrance doors and windows with grey tinted glazing.

- External colour: Anthracite: RAL 7016

 Lookalike glazing panels where required, to be anthracite film or spandrel panels.

Main Warehouse;
Graded horizontally fitted trapezoidal profiled built up metal cladding system: external finish and colour: HPS 200 ultra:

- White: RAL 9003
- Ice Blue: RAL 230 80 10
- Wedgewood Blue: RAL 220 50 15
- Ocean Blue: RAL 230 30 25

 500mm wide x 100mm deep Aluminium feature flashings / framing to wall cladding.

- White: RAL 9003

Roof

- Anthracite RAL 7016

 Precast concrete panel walling to loading dock surround

Insulated composite panel Dock and Level Access doors

- Anthracite RAL 7016

 Steel faced fire escape / personnel doors,

- Anthracite RAL 7016

- Occupier signage, design and size to be confirmed. Subject to separate Signage Application.

Cladding Panels
Colour: Ocean Blue

Cladding Panels
Colour: Wedgewood Blue

Cladding Panels
Colour: Ice Blue

Cladding Panels
Colour: White

Arrow Point is located immediately adjacent to Junction 36 of the M1 Motorway. Arrow Point Barnsley offers direct access into Barnsley, Wakefield and Leeds to the North; and access to Sheffield, and the wider East Midlands in the South.

The cladding configuration consists of horizontally laid panels providing a gradient aesthetic with the sky and surrounding context. The use of shades of blue minimises aesthetic impact by blending the façade of the building in with the surrounding context.

The cladding around the office is characterised by smaller cladding panels and the use of flashing, to enhance visual interest and provide visitors direction when arriving at the building.

05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION



Option A | Elevation A





Option A | Elevation B

Pixelated Horizontal Cladding:

-  **Horizontal Cladding**
Colour: Light Grey
-  **Horizontal Cladding**
Colour: Mid Grey

-  **Horizontal Cladding**
Colour: Dark Grey
-  **Horizontal Cladding**
Colour: Blue Grey

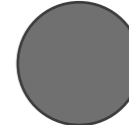
Main Wall Vertical Cladding:

-  **Vertical Cladding**
Colour: Night Grey
-  **Vertical Cladding**
Colour: Dark Grey

Office Composite Panels:

-  **Composite Panels**
Colour: White

Feature Banding and Panelling:

-  **Feature Banding**
Colour: Dark Grey



05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION



Option B | Elevation A



Option B | Elevation B

Pixelated Horizontal Cladding:

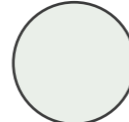
-  Horizontal Cladding
Colour: Light Grey
-  Horizontal Cladding
Colour: Mid Grey

-  Horizontal Cladding
Colour: Dark Grey
-  Horizontal Cladding
Colour: Blue Grey


Main Wall Vertical Cladding:

-  Vertical Cladding
Colour: Green
-  Vertical Cladding
Colour: Dark Grey

Office Composite Panels:

-  Composite Panels
Colour: White

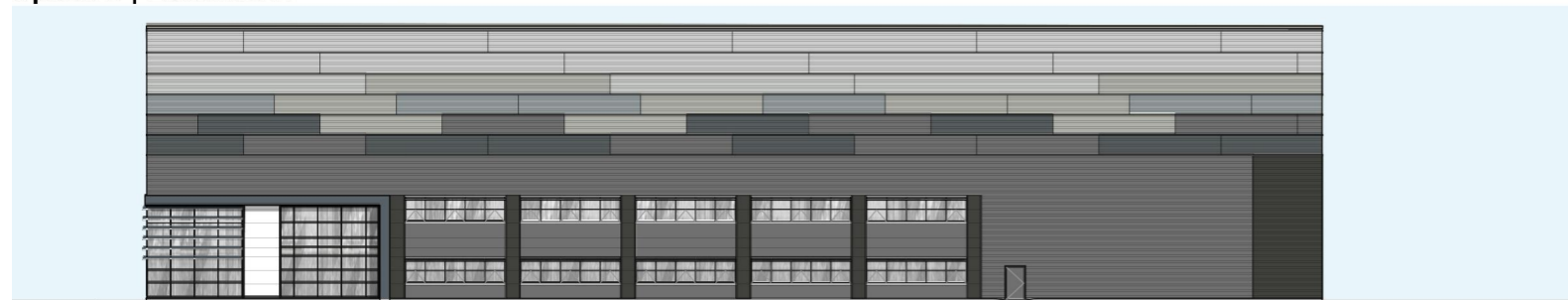
Feature Banding and Panelling:

-  Feature Banding
Colour: Dark Grey

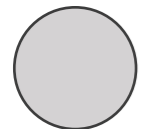


05 DESIGN EVOLUTION

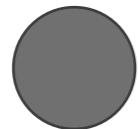
05.8 ELEVATIONAL CONFIGURATION



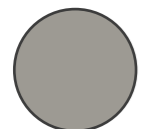
Pixelated Horizontal Cladding:



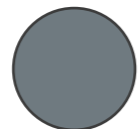
Horizontal Cladding
Colour: Light Grey



Horizontal Cladding
Colour: Dark Grey



Horizontal Cladding
Colour: Mid Grey

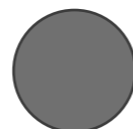


Horizontal Cladding
Colour: Blue Grey

Main Wall Vertical Cladding:

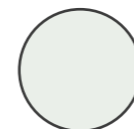


Vertical Cladding
Colour: Night Grey



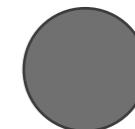
Vertical Cladding
Colour: Dark Grey

Office Composite Panels:



Composite Panels
Colour: White

Feature Banding and Panelling:



Feature Banding
Colour: Dark Grey



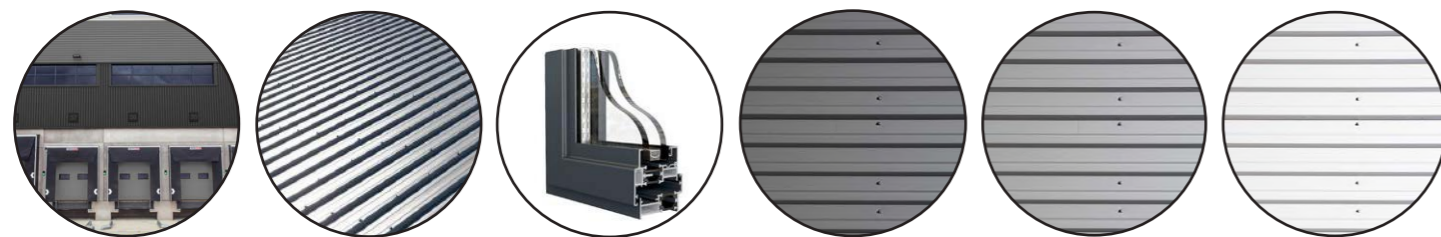
05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION

PLOT 112 | PETERBOROUGH

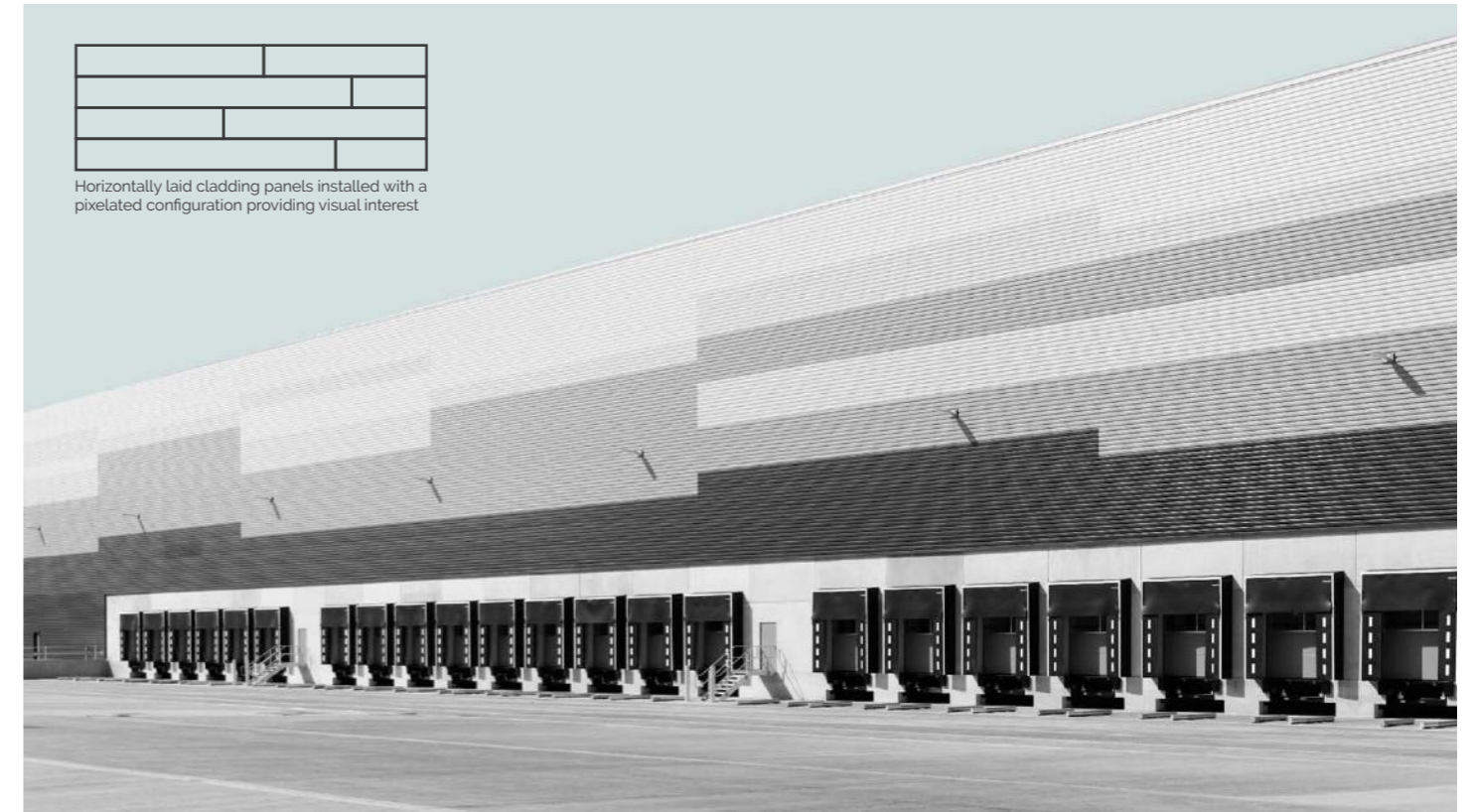


- 1) Vertically laid profiled wall cladding system
- 2) Trapezoidal roof panels
- 3) Anthracite curtain walling, windows & doors
- 4) Horizontally laid micro rib cladding system (Dark Grey)
- 5) Horizontally laid micro rib cladding system (Grey)
- 6) Horizontally laid micro rib cladding system (White)

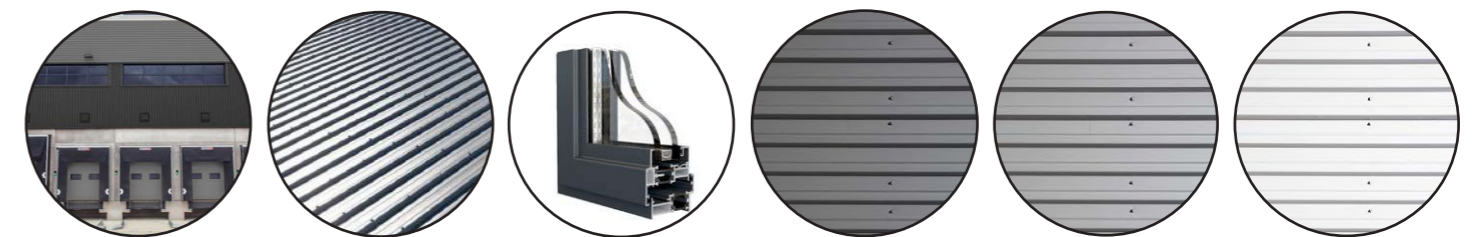


- 1) Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.
- 2) Differing cladding materials to add visual interest and break up the visual building massing.
- 3) Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.
- 4) The use of darker grey tones help anchor the facades back into the landscape whilst delivering visual interest and break up the overall mass of the building.
- 5) Differing cladding materials to add visual interest and break up the visual building massing.
- 6) Upper levels of the façade clad in white horizontal cladding to provide a gradient with the sky

DSV LOGISTICS | MERCIA PARK



- 1) Vertically laid profiled wall cladding system
- 2) Trapezoidal roof panels
- 3) Anthracite curtain walling, windows & doors
- 4) Horizontally laid micro rib cladding system (Dark Grey)
- 5) Horizontally laid micro rib cladding system (Grey)
- 6) Horizontally laid micro rib cladding system (White)



- 1) Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.
- 2) Differing cladding materials to add visual interest and break up the visual building massing.
- 3) Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.
- 4) The use of darker grey tones help anchor the facades back into the landscape whilst delivering visual interest and break up the overall mass of the building.
- 5) Differing cladding materials to add visual interest and break up the visual building massing.
- 6) Upper levels of the façade clad in white horizontal cladding to provide a gradient with the sky

05 DESIGN EVOLUTION

05.9 MASTERPLAN COMPLIANCE ASSESSMENT

Pre-application advice was provided by Barnsley Metropolitan Borough Council outlining key points to consider prior to the application.

Comments Provided	Proposed Response
Urban Design, Height and LVIA	
<p>When the Goldthorpe Masterplan was adopted, the Council nonetheless recognised that the site would likely appeal to occupiers seeking larger units. A pragmatic decision was therefore taken to tolerate buildings with heights of up to 18m. However, the location, scale and massing of these buildings will invariably need careful consideration and mitigation in the form of substantial screening will be necessary to ensure that landscape and visual impact is minimised. This is because, for the reasons articulated above, we are not prepared to tolerate the same level of visual and landscape harm that we allowed at Hoyland West.</p> <p>Urban design comments have been received and our officer is keen to know what the development will look like from the A635 Barnsley Road, which given the volume of traffic will clearly be an 'everyday' view of the development for a lot of people.</p> <p>It will also be important to see what the development will look like from the edges of existing housing and the new housing development sites (HS44 to the east and HS51 to the south).</p>	<p>Each of the units will be designed in accordance with the Goldthorpe Masterplan Framework Document and in line with the comments received from the Local Planning Authority. The Goldthorpe Masterplan Framework Document states that 'buildings should not exceed 15m to the highest point where the footprint is 20,000sqm or less and should not exceed 18m to the highest point where footprints are over 20,000sqm.' Each of the proposed buildings within the development will be designed to provide a maximum ridge height of 18m.</p> <p>A photographic study will be undertaken which will investigate the key views of the site and determine the overall impact to the surrounding context.</p>
Economic Considerations	
<p>Enterprising Barnsley actively support this application and they considered that the development will help to create much needed, new appropriately designed modern industrial units that the market requires (re height), in a strategically important area of the borough, that will help to both attract future inward investors and retain growing indigenous local companies. It is considered that this development is essential in helping BMBC and SYMCA to create future employment opportunities (jobs) as per the existing strategic economic and corporate plans and helping to provide a future delivery of jobs at all levels for local residents to access.</p> <p>It is argued that without such appropriately sized (height) buildings, there is a real possibility that BMBC could approve a scheme that is unfit for occupier market and could result in economic malaise, as opposed to being a clear economic stimulant.</p>	<p>The proposed units will provide suitable space for the occupiers providing employment opportunities for the local residents to access.</p>

Comments Provided	Proposed Response
Highways	
<p>Access to the proposed housing allocation HS51 requires access through the south-east section of this site. This is to ensure that Lacewood Primary School is not detrimentally affected by any future development in terms of highway safety. This access should be provided as part of an early phase of the development and the layout should ensure no ransom strip is retained.</p> <p>What impact the additional traffic will have through the village of Hickleton in Doncaster given that a substantial proportion of the anticipated HGV movements will be to and from junction 37 of the A1M.</p>	<p>Access to the proposed housing allocation HS51 has been provided to the South-East side of the site.</p> <p>The cover letter submitted by JEH Planning states <i>'from a highway perspective, it is our view that the A635 and the local highway network in general is suitable for both existing and future traffic generation of the scale anticipated from the development without any off-site highway mitigation measures being required.'</i></p> <p>The main access to the site will utilise the newly proposed roundabout access point from the A635.</p>
Air Quality	
<p>The AQMA was declared because existing nitrogen dioxide levels through Hickleton are already substantially elevated. It is therefore expected that mitigation will be necessary in order to fully offset the air quality impact from the traffic associated with the ES10 site.</p>	<p>Options for possible mitigation have already been discussed with Doncaster and these are limited due to the close proximity of a number of dwellings in Hickleton to the carriageway. Conversely, a mitigation scheme will be agreed.</p>
Conservation	
<p>ES10 includes the eastern edge of the (former) RSV1 and the southern two thirds of the (former) D1. Those sites were assessed for their likely heritage impact in terms of setting and direct impacts (archaeology).</p>	<p>The northern most part of D1 is over 800m to the southeast of the Conservation Area and this distance allied with only limited long distance views results in a limited contribution to the setting and low potential for harm.</p>

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05.9 MASTERPLAN COMPLIANCE ASSESSMENT

Comments Provided	Proposed Response
Biodiversity	
<p>Due to the proximity of the site to the SSSI there is potential for adverse impacts on the SSSI, in the absence of mitigation.</p> <p>Due to the proximity of the proposal to the Dearne Valley Wetlands SSSI, development of the site may result in hydrological impacts which may also indirectly affect Bolton upon Dearne Wetlands LWS.</p> <p>The development of the site may negatively impact species associated with the Dearne Valley Wetlands SSSI and other notable species, including marsh harrier.</p> <p>As the proposal site is a masterplan framework site (ES10 – Land South of Dearne Valley Parkway) a minimum 10% Biodiversity Net Gain is required.</p>	<p>The proposed scheme will incorporate a landscape framework comprising of the conservation of existing hedgerows and trees where possible, reinforced by significant new trees, hedgerow and shrub planting.</p> <p>The proposal will include the formation of new earthworks and mounding to the main site's perimeter which will include native planting species.</p> <p>Largely native trees and shrubs would be used to reflect those in existing locality. A mix of planting sizes and densities would be adopted to create different habitats.</p> <p>The proposal will look to retain the site's ecological value and enhance the site providing a biodiversity net gain of at least 10% as required within the Goldthorpe Masterplan Framework Document.</p>
Trees	
<p>The need for a tree survey to BS59837:2012, and an Arboricultural Impact Assessment.</p>	<p>The scheme will look to protect as many of the existing trees as reasonably possible.</p> <p>The Arboricultural Impact Assessment will identify tree protection measures necessary and any relevant construction techniques which may be required to facilitate the retention of trees within the site.</p>
South Yorkshire Mining Advisory Service	
<p>The site encompasses Coal Authority 'High Risk' referral areas due to the presence of former opencast coal operations, shallow coal (Shafton seam – where not removed via the opencasts) and two old mine shafts/bell pits (very likely to have been removed however by the opencast workings).</p>	<p>A Coal Mining Risk Assessment will be undertaken which considers the full implications of the mining legacy position.</p>

Comments Provided	Proposed Response
Drainage Strategy and Flood Compensation	
<p>The Masterplan document states that built development should be avoided within Flood Zones 2 and 3. The pre-app contradicts this in that Plot 1 proposes an extended development platform in the Flood Zone 3 to the north.</p> <p>There should be no increase in surface water runoff from the new development.</p> <p>Any balancing facility should be designed to accommodate a 1 in 30-year flow from the site below ground and a 1 in 100-year flow retained within the site (including an allowance of 30% for climate change), without causing any flooding to buildings.</p>	<p>The proposed scheme will aim to minimise the effect it has on the flood zone area. Balancing facilities will be designed to accommodate a 1 in 30-year flow from the site below ground and a 1 in a 100-year flow retained within the site (including an allowance of 30% for climate change), without causing any flooding to buildings.</p> <p>The scheme will utilise Sustainable Urban Drainage (SUDS) techniques to help tackle surface water run-off problems at source using features such as soakaways, permeable pavements, grassed swales, infiltration trenches, ponds and wetlands to attenuate flood peak flows, produce water quality improvements and environmental enhancements.</p> <p>A flood risk assessment in accordance with NPPF will be produced.</p>
Pollution Control	
<p>Noise from road traffic through Hickleton is an issue as increased HGVs, particularly throughout the night, will be a further cause for concern.</p>	<p>A Noise Impact Assessment will be undertaken.</p> <p>A Framework CEMP will undertaken.</p>
Public Rights of Way	
<p>Concerns over the diversion to PROW.</p>	<p>The existing PROW footpath is in poor condition and not suitable for a variety of users. The proposed scheme would upgrade the PROW to provide 2 metre wide access for pedestrians on a safer surface. The additional proposed pedestrian routes to site would encourage sustainable transport.</p>

05 DESIGN EVOLUTION

5.10 DESIGN PANEL REVIEW

The submission of the application follows extensive engagement which emphasises the Applicant's commitment to developing the most suitable scheme for the site. Below are comments provided by Barnsley's Design Review Panel (DRP), and the proposed response.

Comments Provided	Proposed Response
Height	
<p>The Panel could see both points of view in terms of the Council holding their stance on the 18m maximum height set out in the Masterplan Framework document and Newlands seeking an increased height parameter in response to market demand for larger buildings.</p> <p>The Chair, Tom Lonsdale, criticised the 18m restriction as being 'unscientific' and not predicated on any market rationale. It was highlighted that deviations from the Masterplan could be justified through a planning application and this would be achieved through the submission of market evidence.</p>	<p>The building will be designed to provide a 15m clear internal height and a ridge height of 18m in line with the Goldthorpe Masterplan Framework Document.</p> <p>Photomontage analyses will provide evidence of key view points and the impact the proposed height will have on the surrounding context.</p> <p>A market analysis is being undertaken considering the rationale of the proposed building height.</p>
Colour	
<p>The Masterplan Framework refers to grey and blue cladding on Page 38.</p> <p>Elizabeth Motley queried whether grey is the right colour for the buildings as 'there isn't a lot of grey in nature'. She suggested that other neutral colours should be considered and went on to suggest looking at cream.</p> <p>Paul Testa understood the rationale behind the proposed cladding treatment in reducing the perceived massing but noted that the gradient may only be effective where the light banding at the top is seen against the sky rather than other viewpoints where it's seen against the landscape.</p>	<p>The proposed colour palette will be thoroughly considered to minimise the visual impact on the surrounding context. A range of external materials and colour palettes will enhance building elevations, and to soften the appearance and break up the visual proportions of the larger building elevations.</p>
Distinctiveness	
<p>Graham Roberts suggested the buildings require more distinctiveness rather than being 'the same as all the rest'. Reference was made to the trend of Mill buildings getting larger through time and some good examples of architecture that came out of this (Temple Mill (see attached) was referenced by Elizabeth Motley as well as reference to developing a factory shop for returns). A further exploration of architecture was called for by Tom Lonsdale away from the current rectilinear form.</p>	<p>This would be considered in a future Reserved Matters Application. The use of an amended cladding configuration provides distinctiveness to the facades and increases the overall aesthetic appeal of each of the proposed buildings.</p>
Roof Treatment	
<p>Elizabeth Motley suggested that the warehouses could have green roofs or some sort of pattern to break it up. The Panel queried whether the roof could be seen from any viewpoints and FPCR confirmed that it wouldn't.</p>	<p>This would be considered in a future Reserved Matters Application.</p>

Comments Provided	Proposed Response
Renewables	
<p>Graham Roberts wanted to see solar and wind incorporated into the development. He noted that it was currently 'missing a trick' by not including a decentralised energy network and also alluded to the use of ground source heat pumps.</p>	<p>This will be addressed through the Sustainability Statement submitted as part of the Hybrid application with PV included as part of Newlands' basebuild spec. Stantec to advise on feasibility of ground source heat pumps.</p>
Connectivity & Public Accessibility	
<p>Graham Roberts queried whether public connectivity would extend through the Green Blue Corridor down to the Old Moor RSPB area. Allen Parks queried whether the Flood Compensation areas would be fenced off or publicly accessible.</p> <p>There were also comments about user conflict between HGVs and pedestrians so crossing points to be considered.</p>	<p>The proposed site access strategy aims to provide safe access across the site for both vehicles and pedestrians. HGV yard access to each of the units has been segregated from the car park access to avoid congestion. Across the site, pedestrian crossing points have been provided alongside 2m wide and well-lit footpaths to ensure safe pedestrian access.</p>
Earthworks/Bunding/Cut & Fill	
<p>Tom Lonsdale noted that without further detail on the three-dimensional representation of the site, topography and earthworks strategy, it was difficult for the panel to pass more detailed comment as the section where considered to be too conceptual/diagrammatic.</p>	<p>Please refer to Structural Engineer and Civil Engineer drawings.</p>
Air Quality	
<p>Elizabeth Motley made reference to the air quality impacts of having the parking area in the south-east corner adjacent to the School.</p>	<p>The proposed building to the South-East corner of the site has been orientated to provide parking away from the South-East boundary. Between the school and the rear of the building there is a proposed green buffer zone which will mitigate some of the concerns raised regarding air quality.</p>
Flood Compensation	
<p>There appeared to be a lack of knowledge on flood compensation storage and how this is a perfectly acceptable solution to developing within the flood zone.</p>	<p>Please refer to FRA Statement.</p>

05 DESIGN EVOLUTION

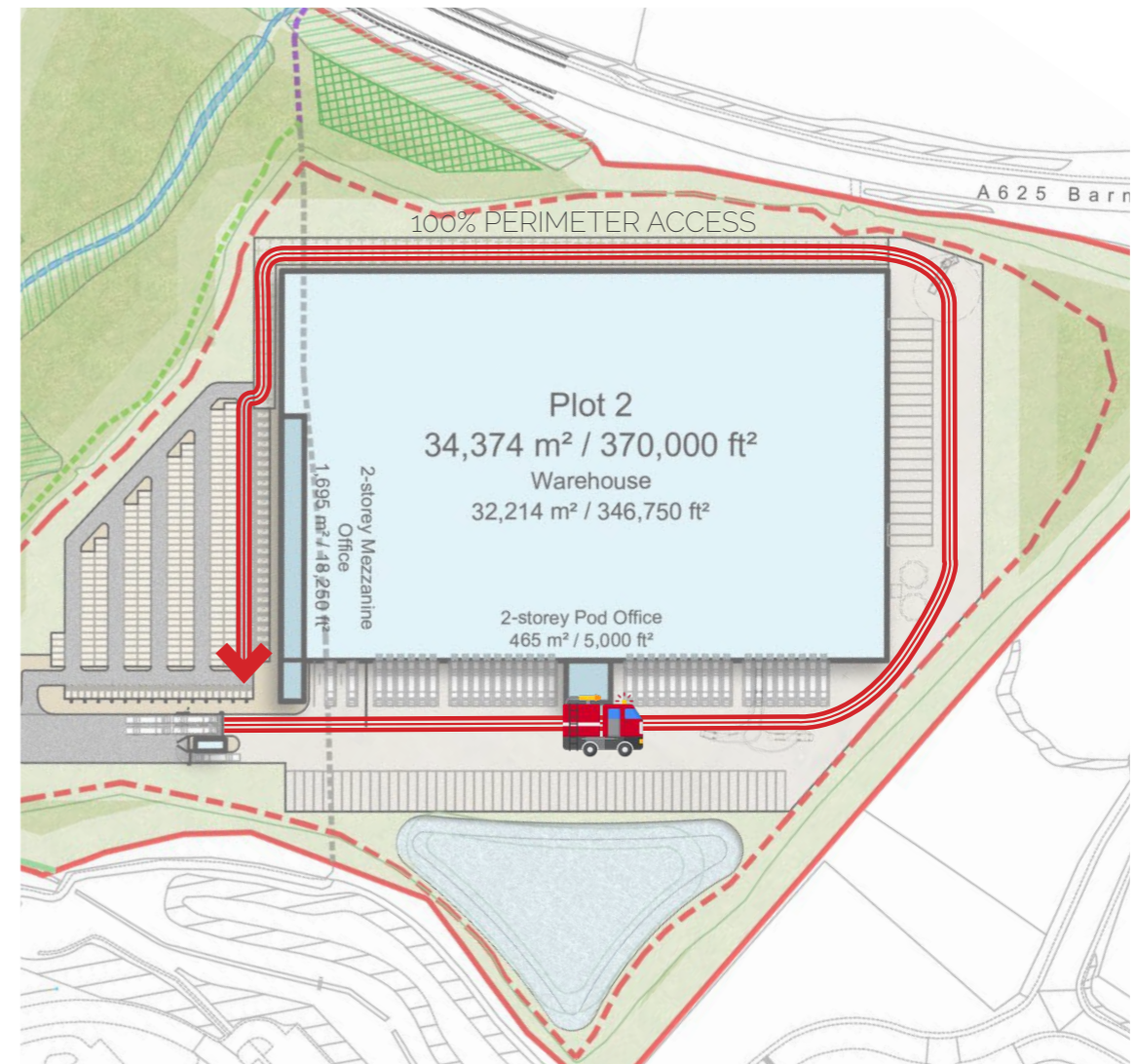
05.11 FIRE STATEMENT

The proposed buildings within the development will be designed in accordance with Approved Document Part B. Table 15.1 of Approved Document Part B makes reference to fire and rescue vehicle access to buildings. Based on the buildings indicated on the Illustrative Masterplan exceeding 24,000m², each building will be required to provide access to fire vehicles around 100% of the perimeter of the building. This will be subject to further development within a Reserved Matters application.

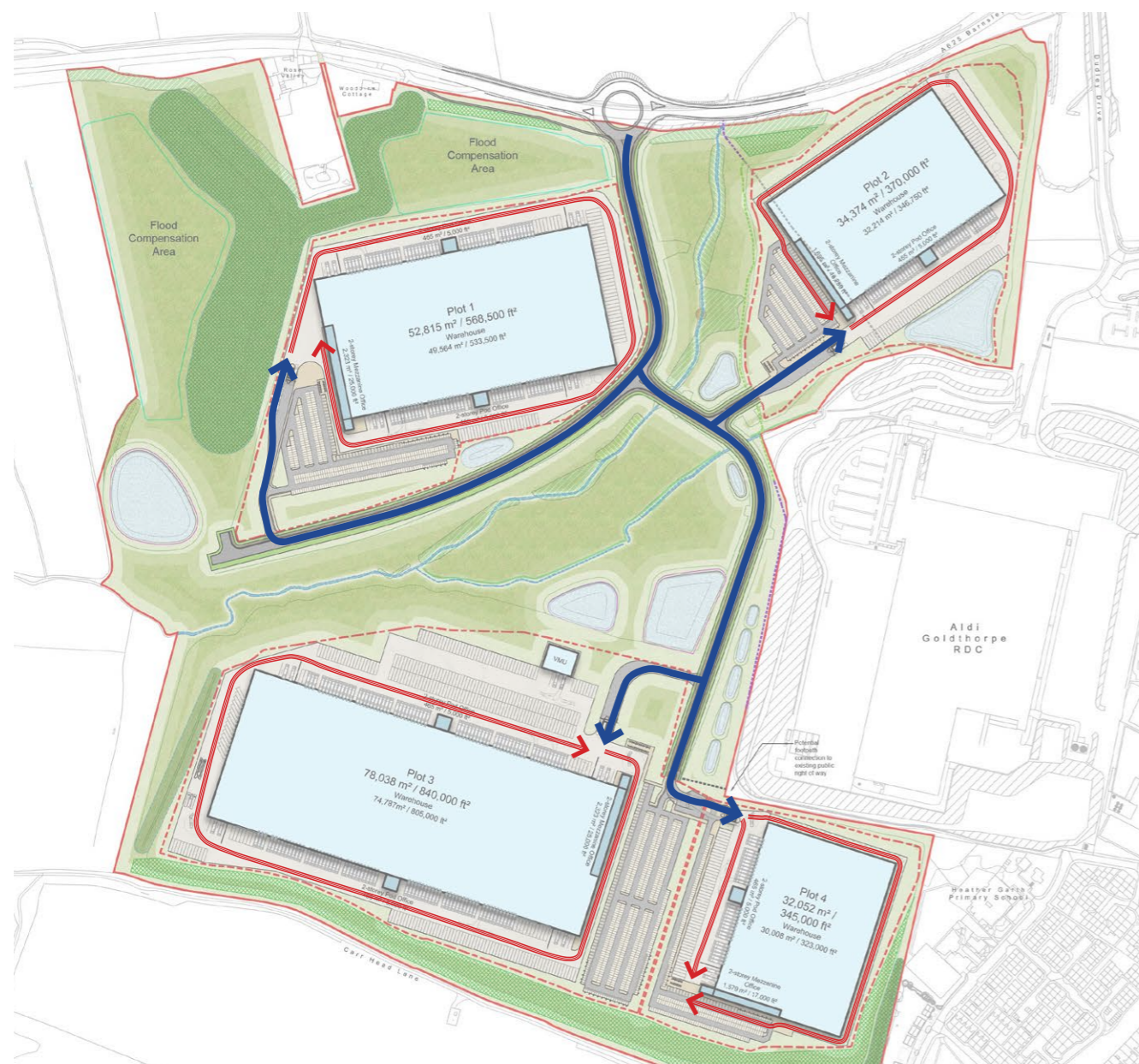
Each of the buildings have been provided with 100% perimeter access, alongside the provision of adequate road widths and turning circle distances in accordance with Table 15.2 of Approved Document Part B.

Future Reserved Matters Applications will provide evidence of the scheme's compliance with Approved Document Part B.

The site wide fire strategy will also provide adequate access to fire hydrants. As stated in Approved Document Part B, buildings not fitted with fire mains, hydrants should be within 90m of an entrance to the building.



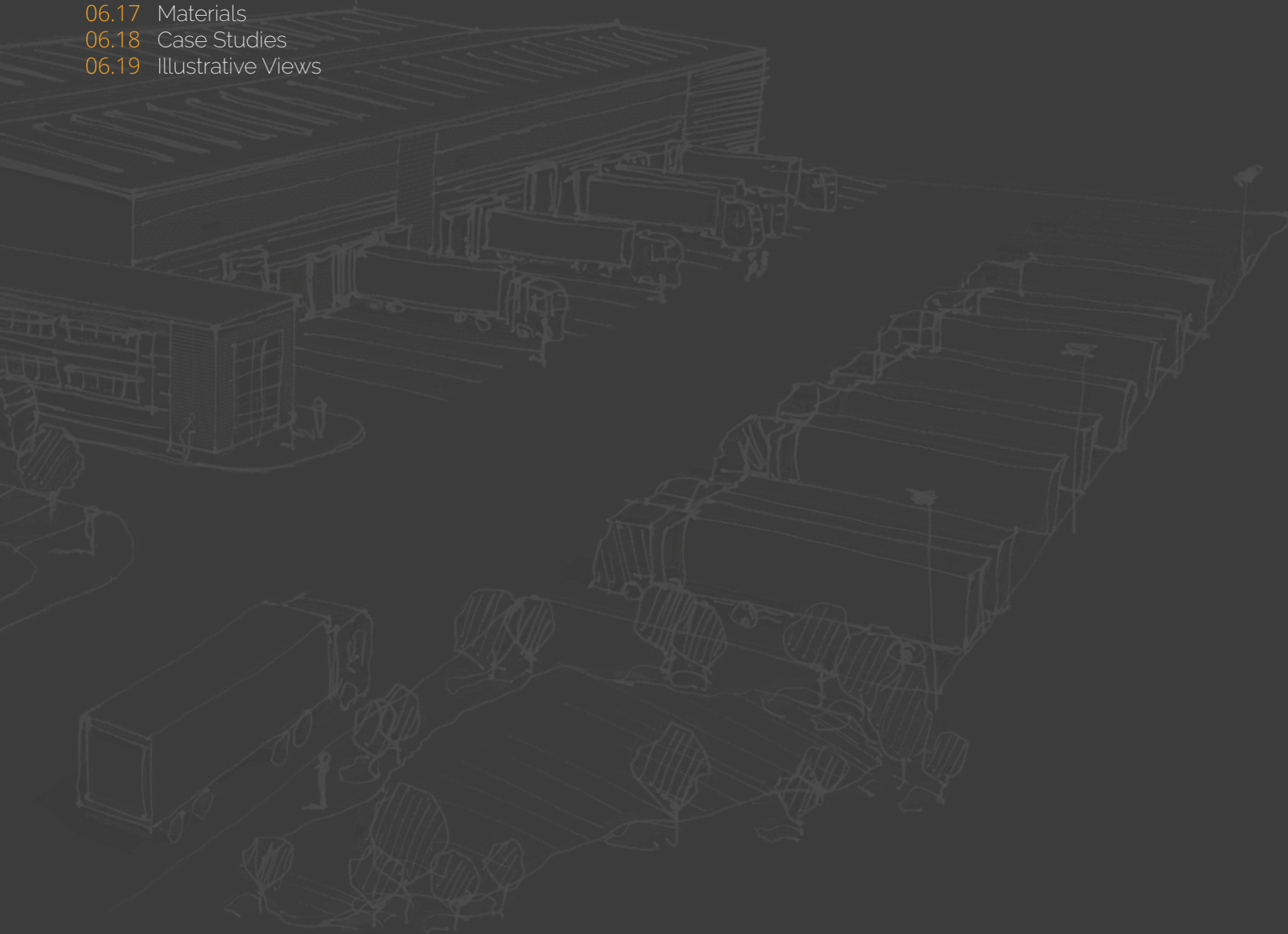
100% Perimeter Fire Vehicle Access



Site Fire Vehicle Access Strategy

06 DEVELOPMENT PROPOSALS

- 06.1 Green Infrastructure
- 06.2 Green Infrastructure Proposal
- 06.3 Blue Infrastructure
- 06.4 Blue Infrastructure Proposal
- 06.5 Connectivity
- 06.6 Connectivity Proposal
- 06.7 Illustrative Masterplan
- 06.8 Goldthorpe Masterplan Framework Compliance
- 06.9 National Design Guide Compliance
- 06.10 Proposed Plateau Levels
- 06.11 Plot Design
- 06.12 Plot Hard Landscaping
- 06.13 Proposed Culverts
- 06.14 Scale and Massing
- 06.15 Site Sections
- 06.16 Appearance
- 06.17 Materials
- 06.18 Case Studies
- 06.19 Illustrative Views



06 DEVELOPMENT PROPOSALS

06.1 GREEN INFRASTRUCTURE

The site presents opportunities to connect into the wider Green Infrastructure network beyond the site boundaries. A specialist will undertake strategic landscaping work to provide a more developed outcome to the overall landscaping design.



Existing Trees and Hedges

The application site lies in an area of low lying existing agricultural land consisting of fields with perimeter hedgerows.



Green Infrastructure Strategy

Appropriate green buffer zones will be provided along the Northern, Southern, and Eastern boundary in order to reduce visual impact as well as noise and light impacts to the surrounding context.

The introduction of attenuation ponds located within the site provide additional wildlife habitats increasing the biodiversity value of the area. These areas seek to link into the existing green infrastructure that can currently be found within and around the site.



Sketch Proposal

The sketch proposal shows a layout which responds to the site's topography and provides a structural landscape strategy.

The peripheral landscape treatment aims to define the site boundary whilst also integrating the site into the wider countryside.

 Illustrative Existing Trees

06 DEVELOPMENT PROPOSALS



Green Infrastructure Proposal

06.2 GREEN INFRASTRUCTURE PROPOSAL

The green infrastructure of the proposed masterplan offers a high quality landscaped setting to the employment site.

- Provides a sense of place through a structured and planned landscaping strategy.
- The proposals seek to establish the landscape and visual baseline in terms of its existing character and its sensitivity to the development proposals, whilst mitigating visual impact to wider surroundings
- Naturally mitigates noise and light pollution by providing appropriate natural buffer zones.
- The proposals increase the biodiversity value of the site.

Overall the proposed green infrastructure will be an important feature of the development, creating a sense of place throughout the development and integrating the proposal into the existing surrounding landscape whilst providing benefits for visitors and employees of the site including landscape and wildlife value, sustainable drainage, cycling and walking routes.

06 DEVELOPMENT PROPOSALS

06.3 BLUE INFRASTRUCTURE

The blue infrastructure network refers to the network of such features and bodies of water across the proposed site. These consist of the existing Carr Dike which runs across the site.



Existing Blue Infrastructure

There is an existing watercourse (Carr Dike) which enters the site from the Northern boundary and exits on the Western boundary. The scheme will aim to respond to the existing watercourse through the implementation of a considered zoning strategy.



Zonal Strategy

From a hydraulic and spatial perspective, the site presents opportunities to create sustainable urban drainage solutions and therefore additional habitats and natural buffers to the surrounding context.



Sketch Proposal

Infrastructure attenuation ponds are located in seven locations around the site in order to mitigate flood risk. Additional on plot attenuation can be afforded where appropriate via balancing ponds or attenuation tanks situated beneath service yards.