

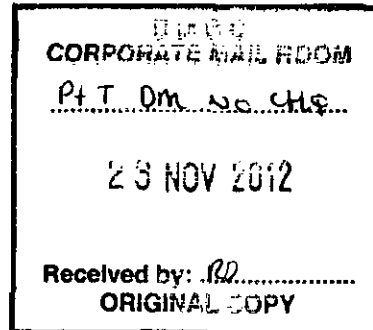
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23rd November 2012

Our ref: - 231112-4707-1-PL2

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Andrew Burton
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Barnsley Metropolitan Borough Council
Planning and Regulatory Services
PO Box 604
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Dear Andrew

**RE: Application Reference: 2012/1054 – Residential development of 41No.
Dwellings at Higham Lane, Dodworth.**

Following a meeting to review the consultation comments on 08th November 2012 with David Connole – Redrow Homes, Mark Johnson - Dacres and Andrew Burton, Barbara Wilson, Wayne Atkins and Nik King – Barnsley Metropolitan Borough Council please find enclosed amended/ additional information to assist with the above planning application.

We have summarised all comments from the highway officer, drainage officer and urban design officer below along with Redrows response.

Highway Officer Comments

Parking should be in accordance with the adopted SPD

- The revised site layout drawing no. 4707-16-02-01A has been amended to comply with the adopted SPD. We have reduced the number of parking spaces provided for plot 11; the additional space has been used for landscaping. The parking in front of plots 13-16 has been reduced and now includes 1 visitor parking bay. One of the two parking spaces provided for plots 24-25 have now been allocated as visitor parking as agreed.

Page 8 of the Design and Access Statement refers to the Kingstone School which no longer exists.

- The enclosed Design and Access Statement has been updated and this information has been removed.

Page 5 of the Transport Statement refers to injury data between Jan 2007 and December 2012.

- Please find enclosed a revised copy of Transport Statement Rev 2 with the dates corrected.

I am concerned about the length of shared surface and that a footway is indicated.

- The footpath has been reduced from plot 16 to a hard margin as agreed. Please refer to drawing no. 4707-16-02-01A for further details.

The turning heads must be capable of accommodating the manoeuvres of the CARP vehicle of South Yorkshire Fire and Rescue.

- As discussed at the meeting the AutoTrack software have used does not have the exact details of the CARP vehicle, however we have tracked the turning heads with a DB32 large refuse vehicle which demonstrates that adequate turning facilities have been provided. Please refer to drawing 4707-16-02-08 for further details.

Speeds should be self-enforcing and vertical deflection should not be necessary.

- The centreline radius of the road in front of plot 19-22 has been reduced to enforce speeds below 20mph as agreed. Please refer to drawing no. 4707-16-02-01A for further details.

Drainage Officer Comments

Additional 3M easement required for existing watercourse

- Historic record drawings of the site note an existing watercourse on the northern boundary of the proposed development, however site inspection confirms this is a minor feature (small swale within the topography), which is likely to have been used to protect any potential run-off from the railway embankment. This feature is considered in the JBA Flood Risk Assessment and there is provision in the detailed design to construct a suitable land drain on the boundary to provide the same level of protection as the current feature. During a recent meeting Wayne Atkins (BMBC Senior Engineer, Drainage & Highways), highlighted the need for a 3m easement within our site to service this feature. This has been complied with for 80% of the boundary, however there is a restriction of width adjacent to Plot 27. This has been discussed with Wayne, agreed in principle and supplementary detail submitted for his consideration.

Urban Designer Comments

Housing Mix

Discrepancies with the housing mix schedule. The site layout plan, Design and Access Statement and planning application forms need to be consistent with one another.

- The housing mix schedule within the Design and Access Statement has been corrected and all housing schedules within the application correspond with one another.

Street Scenes

Street scenes provided need to show all the plots on both sides of the streets. Dwelling type 9, on plots 30 and 36 only, needs a window on the gable ends, facing the street.

- Drawing 4707-16-02-05A-Typical Street scenes has been revised to show the outstanding plots as requested. Plots 30 and 36 have had additional gable windows added as requested. Four copies of the planning drawing for plots 30 and 36 have been provided.

External Spacing Dimensions

The external spacing dimensions contained in SPD 'Designing New Housing Developments' are not being met with some plots:

1. *For rear garden areas the expected standard is 50m² for 2 bed dwellings and 60m² for 3/ 4 bed dwellings.*
2. *The garden depth from rear of dwelling should generally be 10m.*

As discussed at the meeting garden sizes are in proportion to the size and design of each house type. Following the pre-application meeting comments dated 03.11.2010 it was suggested that dwellings to the south boundary should be set in from the boundary to reduce height dominance on existing properties along Hawthorne Crescent 'even if it means that the garden lengths on the northern section of the proposed development are reduced in length'. This as a principal the scheme has been developed around. However the following improvements have been made to garden sizes as agreed:

- The layout has been updated to show the public footpath in front of plots 17-26 replaced with a hard margin. The removal of the footpath as enabled plots 18, 19 23-26 to move forward by 1.5m which has resulted in an increase to the garden area and depth of these plots.
- The road position has been amended following the meetings comments allowing plot 30 to be moved forward to increase the garden area and depth of this plot.
- The number of parking spaces in front of plots 13-16 has been reduced allowing these plots to move forward 1.5m. This has resulted in an increase to the garden area and depth of these plots.
- The number of parking spaces in front of plots 9-12 has been reduced allowing these plots to move which has resulted in an increase to the garden area and depth of these plots.

Internal Spacing Standards

The SPD states that 'all developments should achieve the internal spacing standards set out in the South Yorkshire Residential Design Guide (SYRDG).

1. *The minimum standard for 2 bedroom dwellings is 62m².*
 2. *The minimum standard for 3 bedroom dwellings is 77m².*
- The internal spaces provided within the proposed house types are consistent with two and three bedroom properties being approved built throughout Barnsley and the rest of the country. The designs of our homes are led by market demand and as such we have successfully built and sold our properties at other many new development sites. The sizes of our proposed house types at Higham Lane are very similar to our very successful site at Champany Fields, Dodworth which was approved by BMBC.

The SYRDG states that 'best practice suggests that the internal floor area for car spaces should be 3m by 6.5m'. Those shown on the garage plan are smaller at 2.88m by 5.81m.

- Parking provisions for all dwellings complies with Barnsley Parking Standards without the need for allocating garages as parking facilities.

Parking Standards

Parking requirements for 2 bed dwellings is 1 space per dwelling with flexibility for visitor parking.

- Please refer to the Highway response above relating to the parking standards.

Ginnels

A gate is required at the front of the ginnels to make them secure.

- Drawings 4707-16-02-02A and 4707-16-02-05A have been revised to show that access to the rear gardens through the ginnels (plots 10-11 and 14-15 only) are through lockable gates as requested.

Introduction of ginnel on the third terrace (plots 23-26).

- A secure bin store area is provided for plots 24-25 adjacent to the terrace block therefore it is not considered necessary to provide a ginnel to this block.

House Type Drawings

The house type drawings elevations are to not look like their front elevations when facing the road.

- The scheme has been developed using the principals of good urban design by providing properties which 'front' onto the public realm. The revised street scene and planning drawings clearly demonstrate, that all primary and secondary elevations which are visible from the public realm have been carefully designed and detailed to provide a consistent architectural style as they are considered the primary elevations. Tertiary elevations which do not face the public realm are less detailed with simple

heads, eills and fenestration patterns reflective of the elevations 'status' which is an historical approach to detailing dwellings. It is also worth noting the additional cost of saturating the tertiary elevations with more detailing has not at this point been allowed for within the viability appraisal for the site.

Materials Layout

Additional information regarding the Ibstock Marlborough Stock brick type required within the design and access statement.

- An image of the Ibstock Marlborough Stock brick type has been included within revised Design and Access Statement.

Further details of door finishes and timber finishes are required on the materials layout.

- Drawing 4707-16-02-03A has been revised and now includes additional information on door finishes and canopy finishes as requested.

Landscape Proposals

Discrepancy with the number of trees shown on site layout plans and landscaping plans.

- The proposed site layout plan, landscaping plan and coloured layout plan within the Design and Access Statement now all correspond with one another in relation to the trees provided on site.

Trees have a very important role in this development in reducing the dominance of front of dwelling parking, reducing the scale of the dwellings, enlivening the open space and softening the street scene views. The considerably sparser distribution of trees shown in the landscaping plan does not adequately fulfil this role.

- Following the comments above regarding the lack of trees on the development drawing SF2059-LL01C now shows that the number of trees and overall landscaping has increased considerably with tree provisions more than double the original proposal.

Additional landscaping required along the southern boundary (in particular the P.O.S area)

- Additional landscaping has been added to the southern boundary as requested. Please refer to drawing SF2059-LL01C for specific details.

The landscaping proposal plan shows different hard surfacing treatments but has no key or further details. What is the block paving specification and how are the driveways, road edgeways and pavements being treated?

- Drawing 4707-16-02-07 Surface Treatment Plan indicates the different surfacing treatments proposed.

Boundary Treatment Plan

The 1.8m brick screen walls shown on the boundary treatment plan requires a variation in the design. This needs to also be illustrated on the street scenes.

- As agreed at the consultation review meeting piers have been added to enclosure walls, the piers have been identified on both the layout plans and street scenes.

Additional rear boundary treatment to be shown to plots 38-41. The edge of the open space and car parking spaces could do with a knee rail boundary to prevent rogue car parking.

- Drawing 4707-16-02-02A has been revised to include additional boundary treatments to plots 38-41 and to the edge of the open space and car parking spaces as requested.

Applicant to confirm a cycle store will be provided for plots 38-41.

- The wording 'proposal' has been removed from drawing 4707-16-02-01A with regards to the cycle store and Redrow confirm the cycle store will be provided.

PV Panels

Further details required regarding the PV Panels mentioned within the Energy Statement.

- Please find enclosed PV Panel information from JJM Building Supplies.

I trust the enclosed information is satisfactory. If however, you require anything further please do not hesitate to contact me.

Yours faithfully

**For and on behalf of
REDROW HOMES YORKSHIRE**



**Charlotte Underwood
Designer**