

LAND SOUTH OF DEARNE VALLEY PARKWAY

GOLDTHORPE

HYBRID PLANNING APPLICATION

DESIGN & ACCESS STATEMENT
(INCLUDING DESIGN CODE PRINCIPLES AND FIRE STRATEGY)

JULY 2024

| DESIGN & ACCESS STATEMENT |

Land South of Dearne Valley Parkway, Goldthorpe Hybrid Planning Application

Document Prepared for:

newlands
developments

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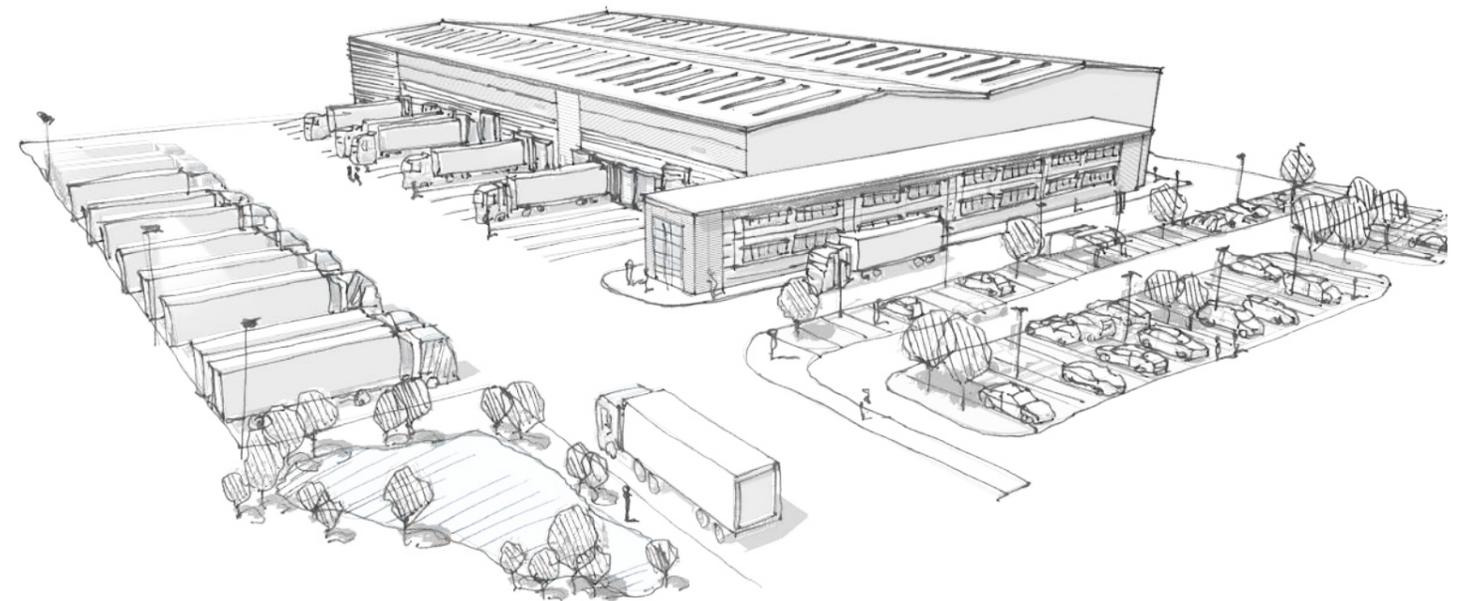
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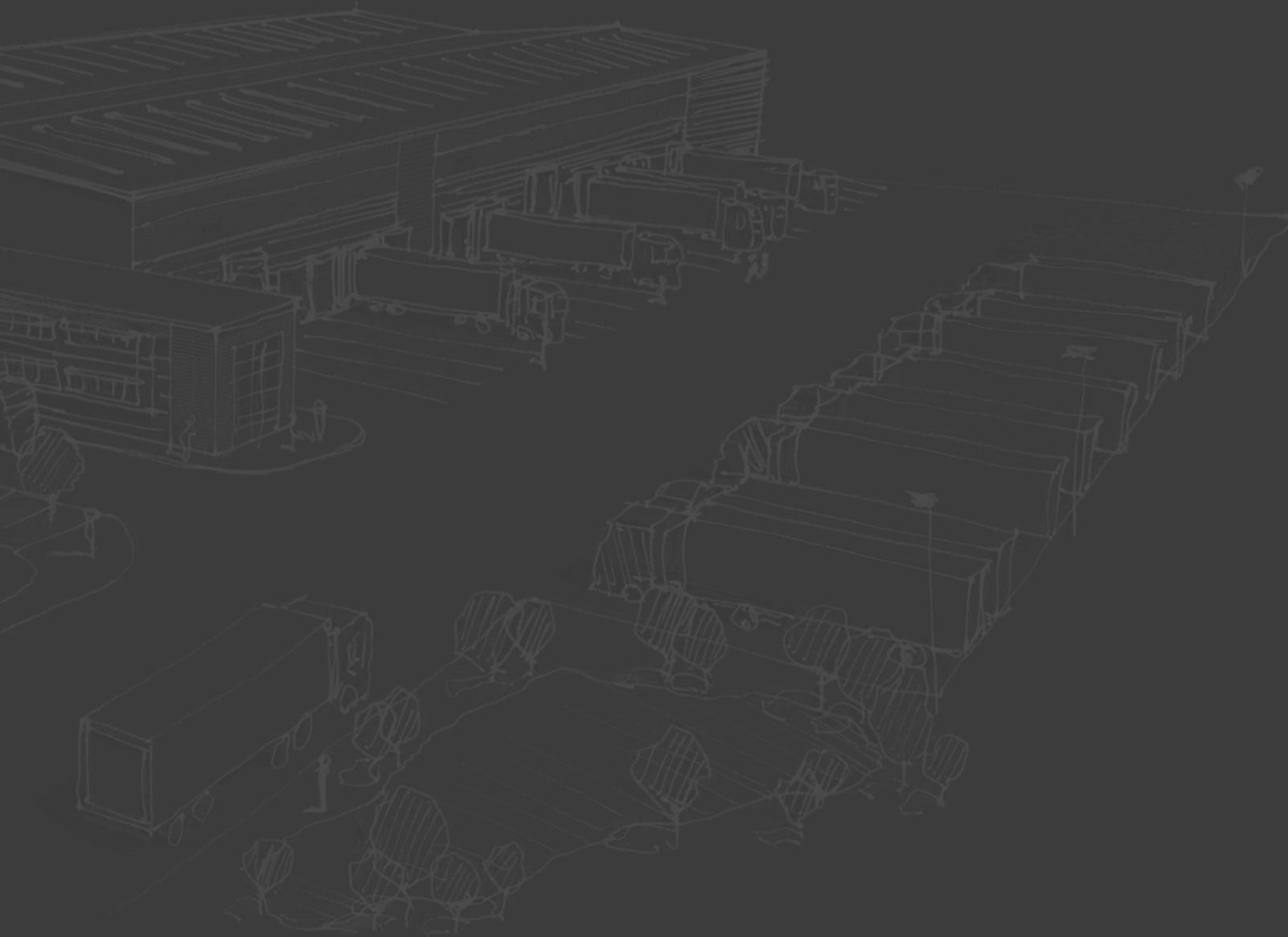
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01 INTRODUCTION

- 01.1 Statement Overview
- 01.2 The Brief
- 01.3 Report Content and Structure



01 INTRODUCTION

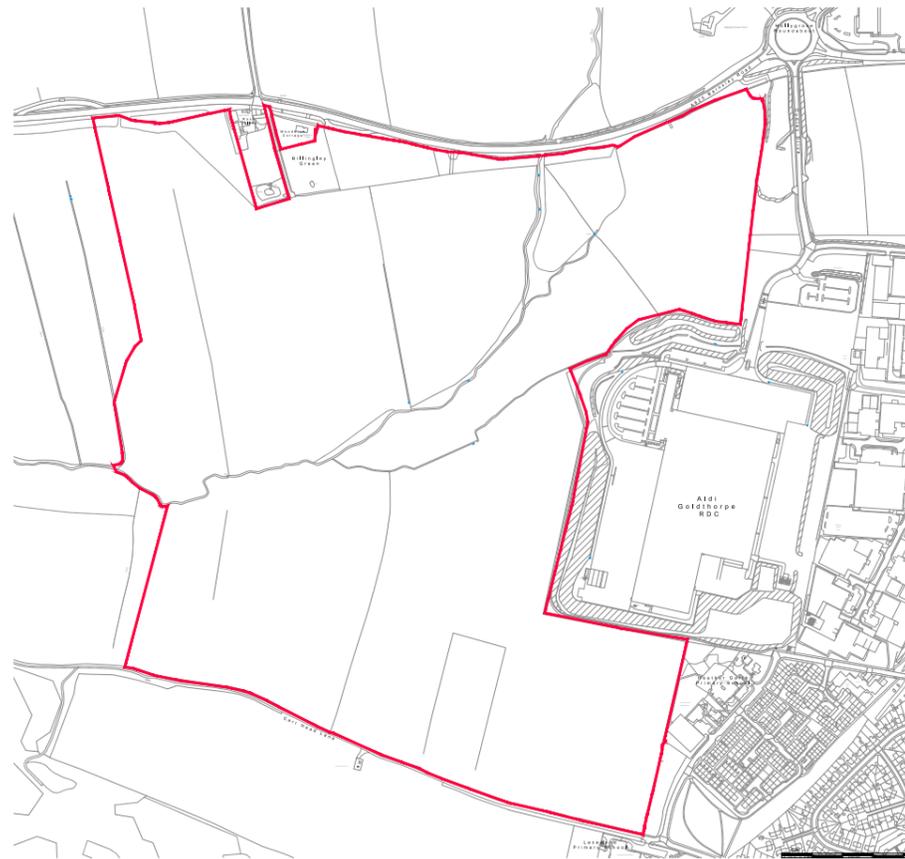
01.1 STATEMENT OVERVIEW

This Design and Access Statement has been prepared by UMC Architects on behalf of Equites Newlands (Goldthorpe) Ltd to support a Hybrid Planning Application for Class B8 (and ancillary offices) and Class B2 floorspace on land South of the Dearne Valley Parkway.

The proposed scheme is described as:
Hybrid planning application:

- Outline permission sought for the construction of Storage and Distribution (Use Class B8) and General Employment (Use Class B2) space with ancillary offices and gatehouses on four separate, self-contained and severable plots as shown on the submitted Parameters Plan. All matters reserved except for site access.
- Full permission sought for engineering infrastructure works to support the employment development comprising: the access roads; earthworks to create the development plot platforms/bunding; drainage and culvert works; a flood compensation area; and strategic landscaping areas.

Like all large development sites, this site presents a number of constraints & opportunities. This statement explains how these are addressed with the design proposals presented as part of the application.



Application Boundary Plan

Key

Application Boundary

01.2 THE BRIEF

The site is in a very strategic location and therefore creates a significant employment opportunity.

The applicant is committed to delivering a best-in-class employment park and business community which will provide for the town and the wider Dearne Valley. The proposals will deliver high quality sustainable buildings, supporting facilities and infrastructure, that will be accessible to employees, visitors, and the local community. The proposal will be set within a comprehensively masterplanned landscape which enhances biodiversity and that is accessible to employees, visitors and the local community, to the benefit of health and wellbeing.

Newlands is confident the proposals will support the vision outlined by the Barnsley 2030 strategy, by attracting significant new business given the quality of the proposals, the scale of investment, combined with the location.

The principle of high-quality design will be instigated at Goldthorpe incorporating innovative sustainable construction measures to ensure that the buildings deliver a high level of environmental performance, meeting the aspirations of potential commercial Occupiers. As such, the design elements relating to this development are high quality and 'class leading' with regard to its construction and operation.

These have been considered in tandem with visual aesthetics of the scheme.

The Energy and Sustainability Strategy for the development surpasses local and national planning policies, whilst achieving the highest viable levels of sustainable design and construction and minimising environmental impacts. The project team is also targeting a BREEAM Excellent Rating.

THE VISION

The vision for the project is to support the Barnsley 2030 objective to deliver sustainable economic growth through the delivery of a best-in-class employment location and business community, to be achieved by:

- The delivery of high quality, accessible and sustainable buildings that meet the needs of potential commercial occupiers
- A comprehensively masterplanned landscape setting that enhances biodiversity and that is accessible to employees, visitors, and the local community, to the benefit of health and wellbeing.
- The provision of safe and convenient vehicular, pedestrian and cycle access, both within the site and to connections beyond, delivering sustainable travel options and strategic connectivity.
- Establishing a sense of place and identity through maintaining a strong landscape concept that alongside a common architectural language delivers both views through the Park and legible wayfinding around it.
- Designing in sustainability and adaptability to deliver a lasting legacy.

The following pages seek to explain how the design of the masterplan has developed with input from the design team and following engagement and consultation with a number of external parties.

01 INTRODUCTION

The statement contains a summary of the site context, analysis of the surrounding areas and an explanation of the relevant design frameworks. The statement explores how the physical characteristics of the scheme have been informed by the design process and explains the steps taken in the process, culminating in the eventual design solution.

This is in accordance with the requirements of the planning application processes and The Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2013 which sets out the requirements for Design and Access Statements.



Application Boundary Aerial Plan

Key

Application Boundary

01.3 REPORT CONTENT AND STRUCTURE

The statement is structured as follows:

Section 1.0

An introduction to the site and statement overview.

Section 2.0

An explanation of the relevant local planning policy context.

Section 3.0

A site context evaluation, containing a description of the existing site and surroundings.

Section 4.0

An evaluation of the key principles considered in the design stage, including site constraints and design strategies.

Section 5.0

Provides a review of the design evolution and identifies how the pre-application consultation and engagement has informed the design.

Section 6.0

Discusses the development proposals of the proposed design, covering proposed: usage, amount, layout, scale and massing, appearance and landscaping.

Section 7.0

Outlines the vehicular and pedestrian access to and around the site.

Section 8.0

Highlights the landscaping design strategy.

Section 9.0

Covers the project's sustainability principles

Section 10.0

Discusses the envisaged crime prevention measures & lighting strategy

Section 11.0

Offers a summarising statement for the proposal.

01 INTRODUCTION



01.4 PLANNING APPLICATION BOUNDARY

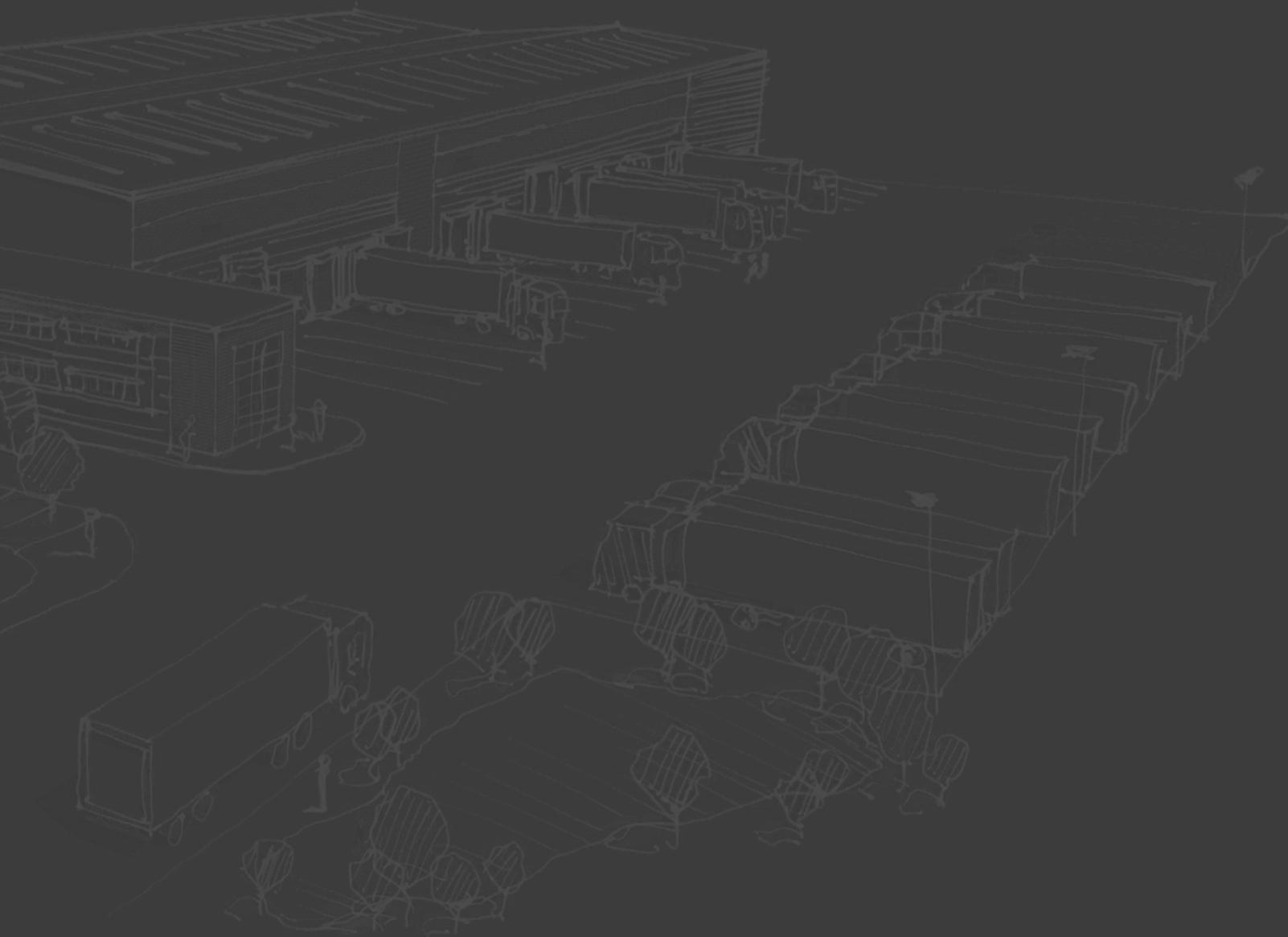
This application is a hybrid application for a total area of 85.32 Hectares (210.84 Acres). Plot 1 comprises an area of 11.35 Hectares, with Plot 2 comprising of 8.46 Hectares, Plot 3 comprising of 17.92 Hectares, and Plot 4 comprising of 6.29 Hectares totalling 44.02 Hectares.

The site is located within the administrative area of Barnsley Metropolitan Borough Council, the local authority.

Development Schedule (Outline application)						
Plot	Plot Size NDA (ha)	Maximum GIA Floor Space (m ²)	Plateau Height (in meters above ordnance datum)	Maximum Finished Floor Level (in meters above ordnance datum) [+1.000m above proposed plateau]	Maximum Building Height Measured to roof ridge / highest point (in meters above ordnance datum)	Ridge Height (above F.F.L. level)
Plot 1	11.35	204,000m² Total Area distributed across Plots 1, 2, 3 & 4	24.50	25.50	43.50	18.00
Plot 2	8.46		25.00	26.00	44.00	18.00
Plot 3	17.92		33.70	34.70	52.70	18.00
Plot 4	6.29		33.70	34.70	52.70	18.00
Total	44.02					
The use class applied for is primarily Class B8 with up to 30% of the floorspace being for Class B2 together with ancillary office space						

02 PLANNING POLICY

- 02.1 Local Planning Policy
- 02.2 National Planning Policy



02 PLANNING POLICY

02.1 LOCAL PLANNING POLICY

2.1.1 A detailed assessment of the planning policies will be provided in the planning statement.

2.1.2 The adopted Local Plan sets out how the Local Authority will manage the physical development of the borough, including the provision of sufficient land in strategic locations to attract more businesses to borough and to enhance existing business growth. The aim of this is to increase the number of jobs available, increasing the scope of opportunities available to local residents. It also aims to provide improved housing to meet existing need and the needs of future generations.

2.1.3 Barnsley's statutory development plan consists of the following documents:

- Local Plan
- Joint Waste Plan

2.1.4 This document has been prepared in accordance with section 6.12 of the adopted Local Plan, and with specific reference to the site specific policy for the allocation, ES10. The document has also been prepared in accordance with the Council approved Goldthorpe Masterplan Framework.

2.1.5 The Local Plan sets out guidance for the development of ES10 to be delivered as Employment Land. This guidance includes the following:

- Protect and enhance biodiversity value and on the nearby Old Moor RSPB reserve and ensure that the development avoids impacts or incorporates effective mitigation measures;
- Provide a contribution towards improvements to biodiversity within the Dearne Valley Green Heart Nature Improvement Area;
- Include the creation of a habitat corridor (at least 8m in width) along Carr Dike and a sustainable drainage scheme to ensure that rainwater falling on the site is still able to drain into the Dike aiming to improve water quality;
- Improve the highway network to mitigate the impact of additional traffic generated by the development on surrounding roads and in particular effects on the A635 and other strategic road links to the A1/M and M1 motorways;

- Provide appropriate access to housing site reference HS51 from Billingley View through the south east corner of the site;
- Retain the existing woodland and hedgerows on the site periphery;
- Retain the section of hedgerow remaining in the north-west corner of the site;
- Avoid locating any built development in Flood zones 2 and 3;
- Safeguard the setting of the Billingley Conservation Area; Give consideration to Carr Dike and the connecting unnamed ordinary watercourse which run through the site;
- Provide an air quality assessment to assess the impacts of traffic emissions within air quality management areas along the A635 and other strategic road links to the A1/M and M1. Any adverse impacts on air quality should be mitigated in accordance with policy AQ1.

2.1.6 Below is a summary of the Local Plan policies that are relevant to the site and to which the Masterplan Framework has had regard to and against which future planning applications will be assessed:

- Policy SD1 - Presumption in favour of Sustainable Development
- Policy GD1 - General Development
- Policy LG2 - The Location of Growth
- Policy E1 - Providing Strategic Employment Locations
- Policy E2 - The Distribution of New Employment Sites
- Policy E3 - Uses of Employment Land
- Policy ES10 - Land South of Dearne Valley Parkway
- Policy HS51 - Site to the west of Broadwater Estate - required appropriate access from Billingley View through the south east corner of site ES10
- Policy T3 - New Development and Sustainable Travel
- Policy T4 - New Development and Transport Safety
- Policy T5 - Reducing the Impact of Road Travel
- Policy D1 - High Quality Design and Place Making
- Policy LC1 - Landscape Character
- Policy HE1 - The Historic Environment

- Policy HE2 - Heritage Statements and general application procedures
- Policy HE6 - Archaeology
- Policy GI1 - Green Infrastructure
- Policy GS2 - Green Ways and Public Rights of Way
- Policy BIO1 - Biodiversity and Geodiversity
- Policy GB1 - Protection of Green Belt
- Policy CC1 - Climate Change
- Policy CC2 - Sustainable Design and Construction
- Policy CC3 - Flood Risk
- Policy CC4 - Sustainable Design and Construction
- Policy RE1 - Low Carbon and Renewable Energy
- Policy CL1 - Contaminated and Unstable Land
- Policy AQ1 Development in Air Quality Management Areas
- Policy UT2 - Utilities Safeguarding
- Policy I1 - Infrastructure and Planning Obligations

2.1.7 The Local Authority have set out a strategy for the borough achieving zero carbon by 2045 (Zero 45). Further consideration will be given to the following measure with future developments:

- Creating energy efficient well insulated buildings in order to reduce carbon emissions;
- Use of renewable energy sources (e.g. solar, wind, biofuels) for all or part of their energy needs to reduce carbon emissions;
- Sustainability standards such as BREEAM and CEEQUAL when designing developments;
- Implementation of sustainable drainage systems to safely deal with surface water run-off and minimise the risk of flooding;
- Identifying opportunities to incorporate space within new buildings to accommodate low carbon technology in the future, to ensure that new development is durable and adaptable;
- Recycling facilities;
- Identifying opportunities for green and solar roofs;
- Travel plans to encourage active and sustainable travel.

02 PLANNING POLICY

02.2 NATIONAL PLANNING POLICY FRAMEWORK

The National Planning Policy Framework (NPPF) sets out the Government's planning policies for England and explains how these should be applied.

Presumption in favour of sustainable development

At the heart of the NPPF is a presumption in favour of sustainable development (paragraph 10). For decision-taking this means:

- approving development proposals that accord with an up-to-date development plan without delay; or
- where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, granting permission unless:
 - i. the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or
 - ii. any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

Areas of particular importance referred to above are clarified within footnote 7, and relate to habitats sites (and those sites listed in paragraph 181) and/or designated as Sites of Special Scientific Interest; land designated as Green Belt, Local Green Space, an Area of Outstanding Natural Beauty, a National Park (or within the Broads Authority) or defined as Heritage Coast; irreplaceable habitats; designated heritage assets (and other heritage assets of archaeological interest referred to in footnote 68); and areas at risk of flooding or coastal change.

With respect to decision making, the NPPF makes clear that local planning authorities should approach decisions on proposed development in a positive and creative way; they should work proactively with applicants to secure developments that will improve the economic, social and environmental conditions of the area. Decision-makers at every level should seek to approve applications for sustainable development where possible (paragraph 38).

Building a strong, competitive economy

Support for employment generating development is reiterated time and time again within the NPPF. Paragraph 81 sets out that significant

weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development.

Paragraph 83 goes on to outline that planning policies and decisions should recognise and address the specific locational requirements of different sectors. This includes making provision for clusters or networks of knowledge and data-driven, creative or high technology industries; and for storage and distribution operations at a variety of scales and in suitably accessible locations. Promoting sustainable transport

The NPPF also provides clear guidance on the consideration of development proposals from a transport perspective. Paragraph 110 states that:

"In assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:

- a) appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location;
- b) safe and suitable access to the site can be achieved for all users;
- c) the design of streets, parking areas, other transport elements and the content of associated standards reflects current national guidance, including the National Design Guide and the National Model Design Code 46; and
- d) any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree."

It goes on to suggest that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Achieving well-designed places and conserving the historic environment

The NPPF is clear that the creation of high-quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities (paragraph 126). With respect to the historic environment, the NPPF states that the heritage assets, such as Conservation Areas and listed buildings,

should be conserved in a manner appropriate to their significance (paragraph 189).

Paragraph 197 of the NPPF sets out that, in determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. In determining applications, local planning authorities should take account of:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation.
- b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- c) the desirability of new development making a positive contribution to local character and distinctiveness.

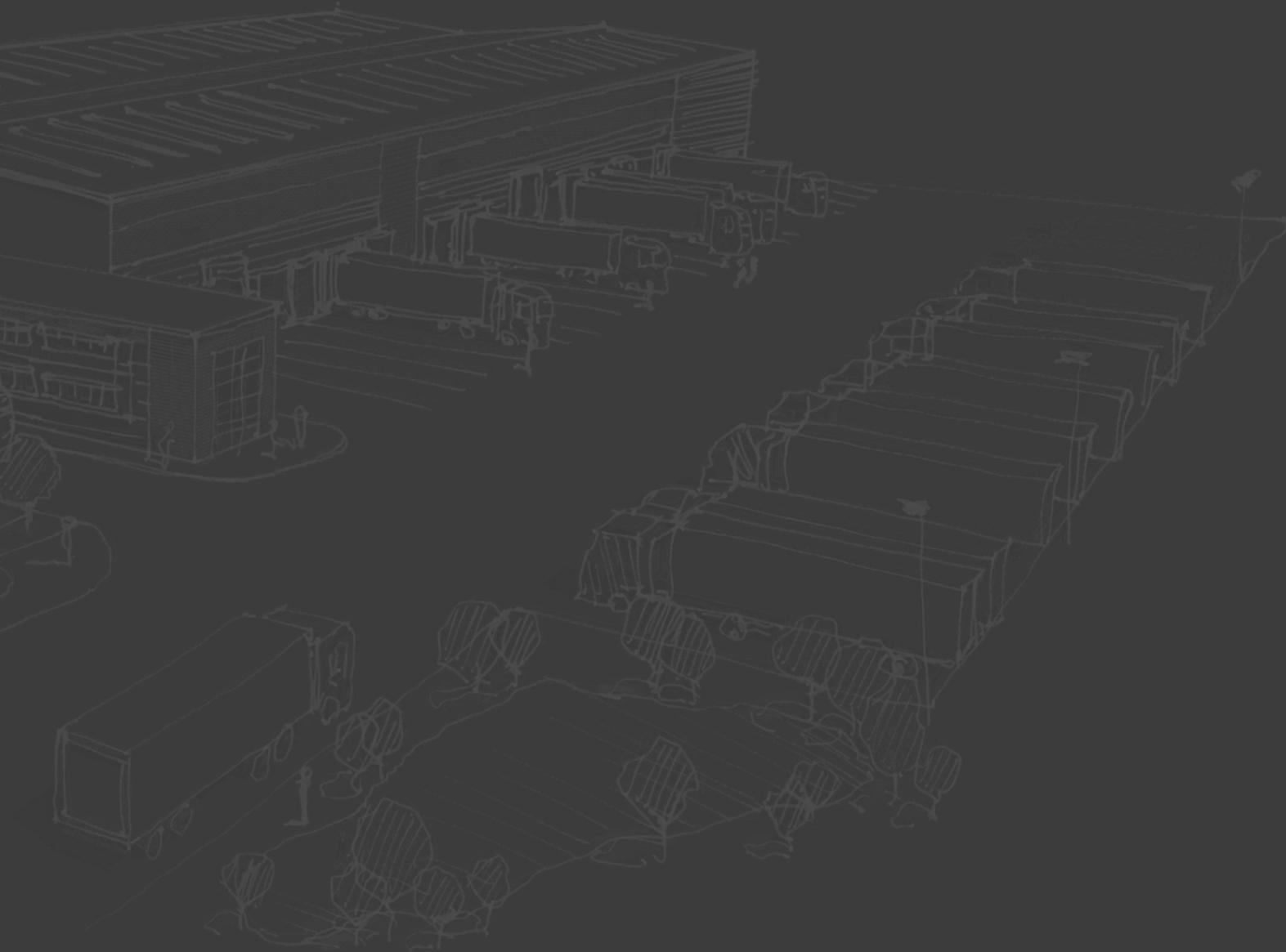
Conserving the Natural Environment

Paragraph 176 of the NPPF affords great weight to the protection and enhancement of biodiversity. It outlines the following principles for the LPA to assess development proposals against (para 180):

- a) if significant harm to biodiversity resulting from a development cannot be avoided (through locating on an alternative site with less harmful impacts), adequately mitigated, or, as a last resort, compensated for, then planning permission should be refused;
- b) development on land within or outside a Site of Special Scientific Interest, and which is likely to have an adverse effect on it (either individually or in combination with other developments), should not normally be permitted. The only exception is where the benefits of the development in the location proposed clearly outweigh both its likely impact on the features of the site that make it of special scientific interest, and any broader impacts on the national network of Sites of Special Scientific Interest;
- c) development resulting in the loss or deterioration of irreplaceable habitats (such as ancient woodland and ancient or veteran trees) should be refused, unless there are wholly exceptional reasons and a suitable compensation strategy exists; and
- d) development whose primary objective is to conserve or enhance biodiversity should be supported; while opportunities to improve biodiversity in and around developments should be integrated as part of their design, especially where this can secure measurable net gains for biodiversity or enhance public access to nature where this is appropriate.

03 SITE CONTEXT

- 03.1 Site Location
- 03.2 Existing Site Photographic Study
- 03.3 Relevant Local Planning History
- 03.4 Site Constraints
- 03.5 Site Opportunities
- 03.6 Existing Highways and Pedestrian Access
- 03.7 Conservation and Heritage
- 03.8 Ecology
- 03.9 Green Infrastructure



03 SITE CONTEXT



Site Location - Wider Context



Site Location - Immediate Context

03.1 SITE LOCATION

The site covers Local Plan reference ES10 and is located West of the settlement Bolton on Dearne and Goldthorpe. The site is situated directly adjacent to the A635 Dearne Valley Parkway providing a connection to the M1, motorway 9 miles to the West and the A1 four miles to the East.

To the East of the site sits the Goldthorpe Aldi RDC and the Goldthorpe Industrial Estate, and to the South-East a school and residential properties. To the West of the site sits greenbelt with the RSPB Dearne Valley to the South of the site.



National Site Location

03 SITE CONTEXT

03.2 EXISTING SITE PHOTOGRAPHIC STUDY

Images 1-11 offer views onto the proposed site as it exists. A general understanding of the current boundary conditions can be taken from these photographs.



Key Plan of Existing Views

- Key**
- Application Boundary
 -  Key Photographic Viewpoints



1 View South from A635



2 View South from A635



3 View South from A635



4 View South-West from Hollygrove Roundabout

03 SITE CONTEXT

03.2 EXISTING SITE PHOTOGRAPHIC STUDY



Key Plan of Existing Views

Key

-  Application Boundary
-  Key Photographic Viewpoints



5 View West from Dudley Drive



6 View West from Dudley Drive



7 View South towards Aldi Goldthorpe RDC from Dudley Drive



8 View West from Billingley View

03 SITE CONTEXT

03.2 EXISTING SITE PHOTOGRAPHIC STUDY



Key Plan of Existing Views

Key

-  Application Boundary
-  Key Photographic Viewpoints



9 View West from Carr Head Lane



10 View West from Carr Head Lane



11 View West from Carr Head Lane

03 SITE CONTEXT

03.3 RELEVANT LOCAL PLANNING HISTORY

3.3 On a site immediately to the East, planning application reference number 2007/1645 consisted of the construction of a regional office and distribution centre (class B1 and B8) and ancillary facilities including a service station and refuse area. formation of a new access and car park, lorry parking, associated groundworks and extensive landscaping.

The scheme included a warehouse, chilled food storage areas and office space totalling 560,000ft², serving Aldi's growing portfolio of stores across South and West Yorkshire, Lincolnshire and Derbyshire, employing some 400 staff.

3.3 Aldi RDC Goldthorpe is located to the immediate East of the site boundary, of which planning permission was successfully granted in October 2010. The building provides a consistent colour palette primarily consisting of RAL 9006 and RAL 9007. There is opportunity for the proposed scheme to respond positively to the immediate context, through the implementation of an aesthetically pleasing colour palette and considered zonal master planning.



Aerial Photograph - Aldi Goldthorpe RDC



Aldi Goldthorpe RDC Part Elevation

- Aluminium Parapet DB 703
- Flat metal cassettes RAL 9006
- Glazed Panel
- Glazed Canopy
- Aluminium Window Frames RAL 9006, RAL 9007
- Louvres RAL 9006



Elevation Photograph - Aldi Goldthorpe RDC



Aldi Goldthorpe RDC Part Elevation

- Aluminium Parapet DB 703
- Insulated Metal Panel System (sinuous metal panel) RAL 9007
- Precast Concrete Sandwich Elements - Light Grey
- Dock Door RAL 9006

03 SITE CONTEXT

03.3 RELEVANT LOCAL PLANNING HISTORY

3.3 To the East of the site sits the existing Goldthorpe Industrial Estate which began expanding in the 1980s, but most notably between 2000-2015 through a string of successful planning applications.

The site hosts a plethora of industrial units each providing varying examples of finish and aesthetic appeal.



Photograph 01 - TLS Recruitment Agency



Photograph 04 - Dibor E-Commerce



Photograph 07 - Mallinson Transport and Warehousing



Photograph 02 - A&S Packing



Photograph 05 - Holme Farm Produce



Photograph 08 - Multiple Occupied Units



Photograph 03 - Tekfloor Ltd.



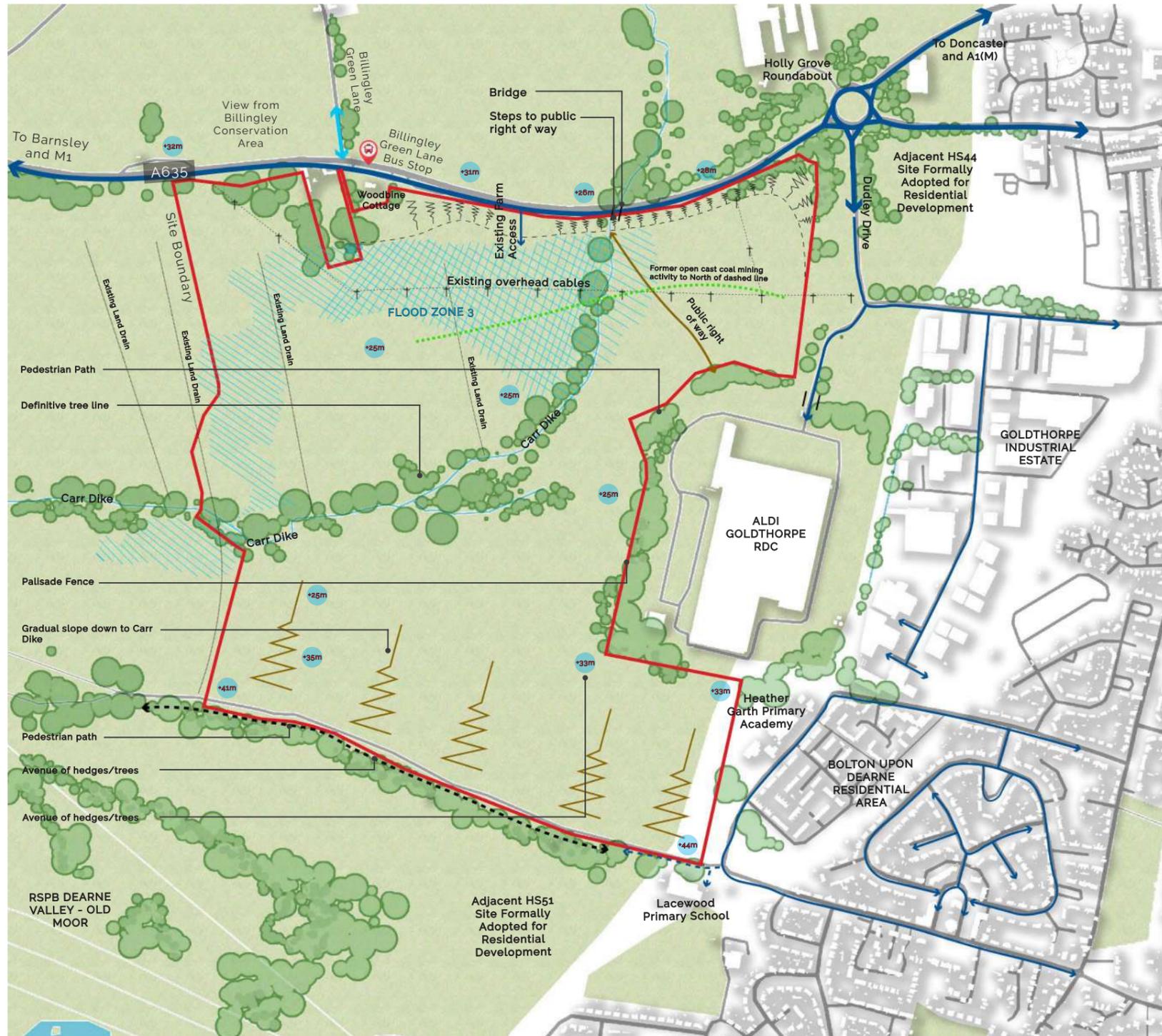
Photograph 06 - Premier Technical Ltd.



Photograph 09 - Multiple Occupied Units

03 SITE CONTEXT

03.4 SITE CONSTRAINTS



The adjacent site constraints diagram identifies the main constraining features inherent within the site.

Existing Character

The site predominantly comprises arable farmland. The central part appears fairly flat, with the North and South of the site providing more significant changes in level. Fields are divided by hedgerow field boundaries. Part of the site is affected by flood zones 2 and 3, particularly along the Carr Dike.

Topography

The landform of the central part and Eastern/Western part of the site is relatively flat at approximately 25 metres AOD. There is a change in level to the North of the site leading up to road level in line with the A635 which stands at approximately 26 - 32 metres AOD. To the South of the site the land levels change to approximately 33 - 44 metres AOD leading up to the public footpath. Detailed analysis of the site levels has been done to design plateaus to screen future development and minimise visual impact to the surrounding context.

Visual Impact

Key public viewpoints have been established to allow the modelling of the topography and building heights to generate a visual assessment for the site. This has explored the nature of the existing visual amenity of the area and sought to establish the approximate visibility of the site from surrounding locations and receptors. There are currently views of the site from Billingley Conservation Area situated 1 mile to the North of the site. There are also views from existing and proposed housing developments to the site.

Services

Three existing underground land drains have been identified crossing the site, all of which travel from the North side of the site down towards Carr Dike. There are also existing overhead cables which run on an east/west axis toward the north of the site.

Existing Buildings

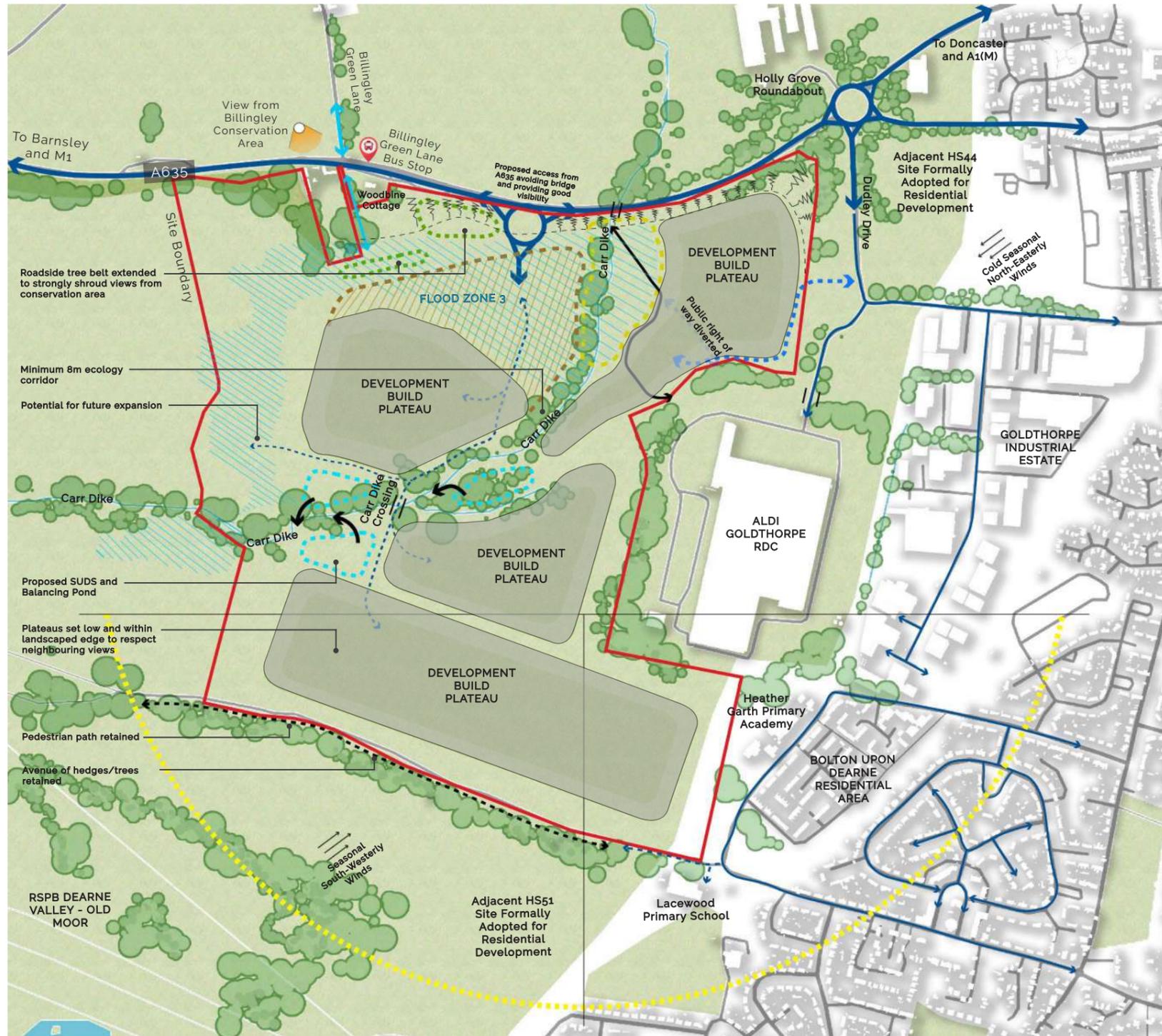
Two residential properties sit to the North of the site accessed via the A635. To the East of the site sits the Aldi Goldthorpe RDC followed by the Goldthorpe Industrial Estate, accessed via Dudley Drive. Heather Garth Primary Academy sits to the South-East of the site followed by a residential area predominantly consisting of two-storey semi-detached properties.

Ecology

There is an existing water course (Carr Dike) transecting the site from the West of the site, alongside a range of tree types and hedgerows. RSPB Dearne Valley is located to the South-West of the site, alongside an SSSI - Bolton Tip, Bolton Ings and Old Moore. Furthermore, there is an adjacent Green belt to the North, West and South of the Site. A further exploration of the ecology evident on site has been explored in section 3.7.

03 SITE CONTEXT

03.5 SITE OPPORTUNITIES



The adjacent site opportunities diagram identifies the main opportunities emphasised by the site.

Natural Dichotomy

There is an opportunity to utilise the natural split created by the Carr Dike ecology corridor which transects the site. The situation of Carr Dike can aid with the implementation of appropriate development plateaus, thus ensuring the future development responds more meaningfully to the immediate site context.

Development Plateaus

Large level development plateaus are achievable through the use of cut and fill and bunding. Alongside providing suitable areas to develop successfully, the careful consideration of plateau levels and location can aid in minimising visual impact on the surrounding context.

Access

A new roundabout is consented to the North of the site which will provide the primary access to the site.

Tree Belt

As a result of the new roundabout to access the site from the A635, there is opportunity to enhance the tree scape either side of this access point and internal estate road.

Levels Bunding and Treescapes

Through the considered implementation of design plateaus and the use of bunding and treescapes, visual impact can be minimised. The Billingley Conservation Area is located approximately 1 mile to the North of the site, by which siting treescapes to the North side of the site will minimise the visual impact from the Conservation Area.

Ecological Enhancements

There is an opportunity to ecologically enhance the site through the implementation of a SUDS (Sustainable Urban Drainage System), and a carefully considered landscape strategy.

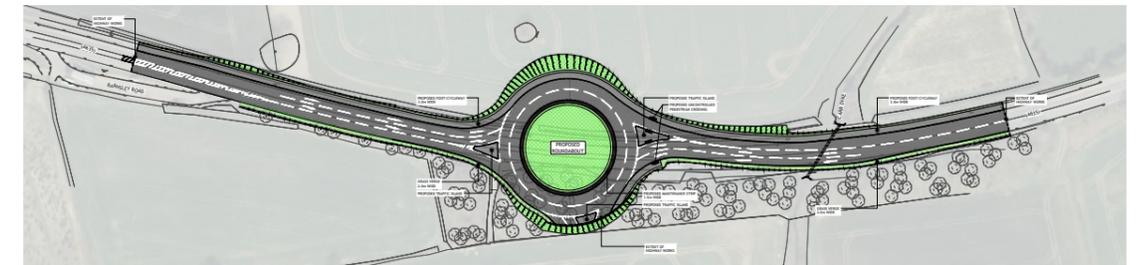
03 SITE CONTEXT



03.6 EXISTING HIGHWAYS AND PEDESTRIAN ACCESS

----- Adjacent roads and vehicular access points

The site is bound to the North by the existing A635 and to the East by Goldthorpe Industrial Estate. Access to the site from the North would aim to utilise the newly proposed roundabout on the A635 which is now under construction.



Proposed New Roundabout (Planning Reference: 2021/1511)

Public Transport

Whilst walking and cycling offer an attractive alternative to the private car for many short – and medium – distance trips, bus travel can also be an effective option. In particular, quality bus services offer the potential to replace car travel locally.

Bus services which run near the site include the X19, 208, 218 and 218a service, each utilising the Billingley Green Lane Bus Stop, Highgate Dudley Drive Bus Stop, and the Darfield Balkley Lane Bus Stop, which provide links to Barnsley, Doncaster and Rotherham.

Goldthorpe Train Station is located approximately 2km to the North-East of the site. The station operates on the Wakefield Line and provides hourly rail services between Leeds and Sheffield from Monday to Saturday, with a reduced service on Sunday.

Bus Stop	Distance from Site	Servicing
Billingley, Billingley Green Lane	0.15km	X19, 208, 218, 218a
Highgate, Dudley Drive	0.5km	X19, 208, 218, 218a
Darfield, Balkley Lane	1.53km	X19, 208, 218, 218a

Public Rights of Way

There is currently a public right of way on the site, providing a route from the Carr Dike bridge on the A635 to the West of the Aldi RDC which connects to Barnsley Road. The PROW is currently not well used, and is not suitable for a variety of users. The proposal includes to improve the PROW link to Carr Field Lane.

03 SITE CONTEXT

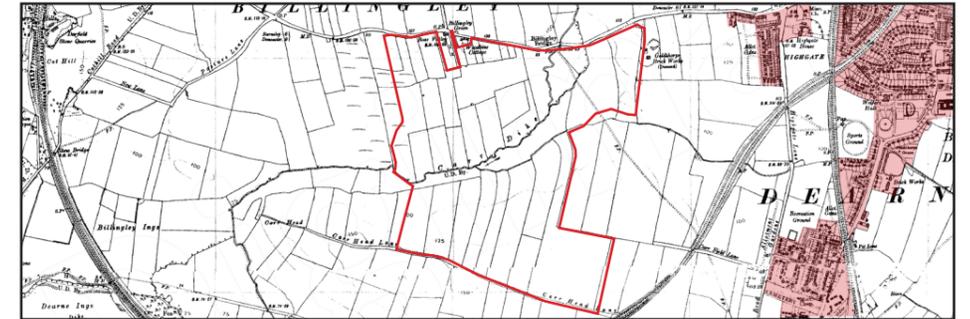
03.7 CONSERVATION AND HERITAGE

The Billingley Conservation Area sits to the North of the site which includes three Grade II listed buildings within the village: Billingley Hall, Poplar Farmhouse and Manor House. The proximity of the site to the conservation area and its impact on the character and setting of the listed buildings was assessed as part of the Local Plan process during which the Local Plan Inspector concluded that the impact was acceptable.

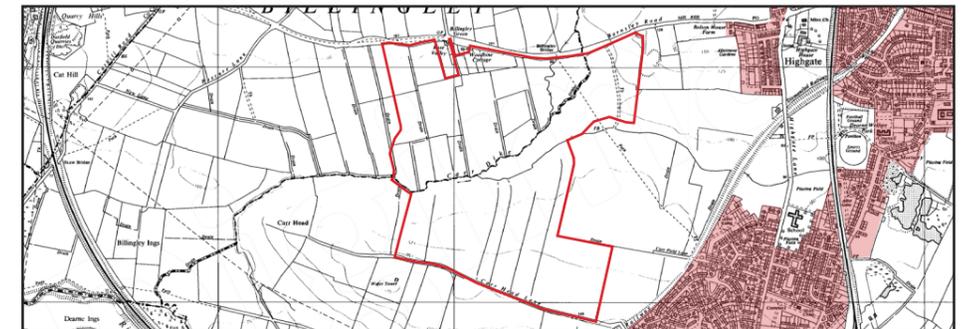
The below historical maps illustrate the urban expansion of the surrounding context, which has predominantly occurred to the East and South-East of the site. These areas include more modern buildings which buffer the site from the original settlement of Bolton upon Dearne. It is also clear that the Goldthorpe Industrial Estate has been expanding since the 1980s.



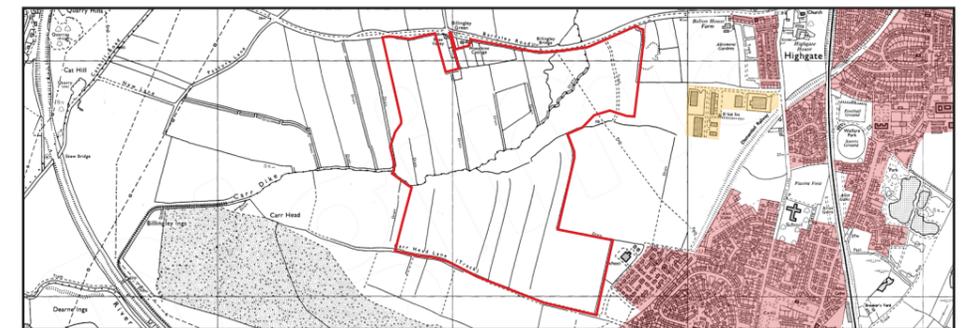
View towards site from edge of Conservation Area (LVIA Viewpoint 12)



Historical Map 1 | 1930s



Historical Map 2 | 1960s



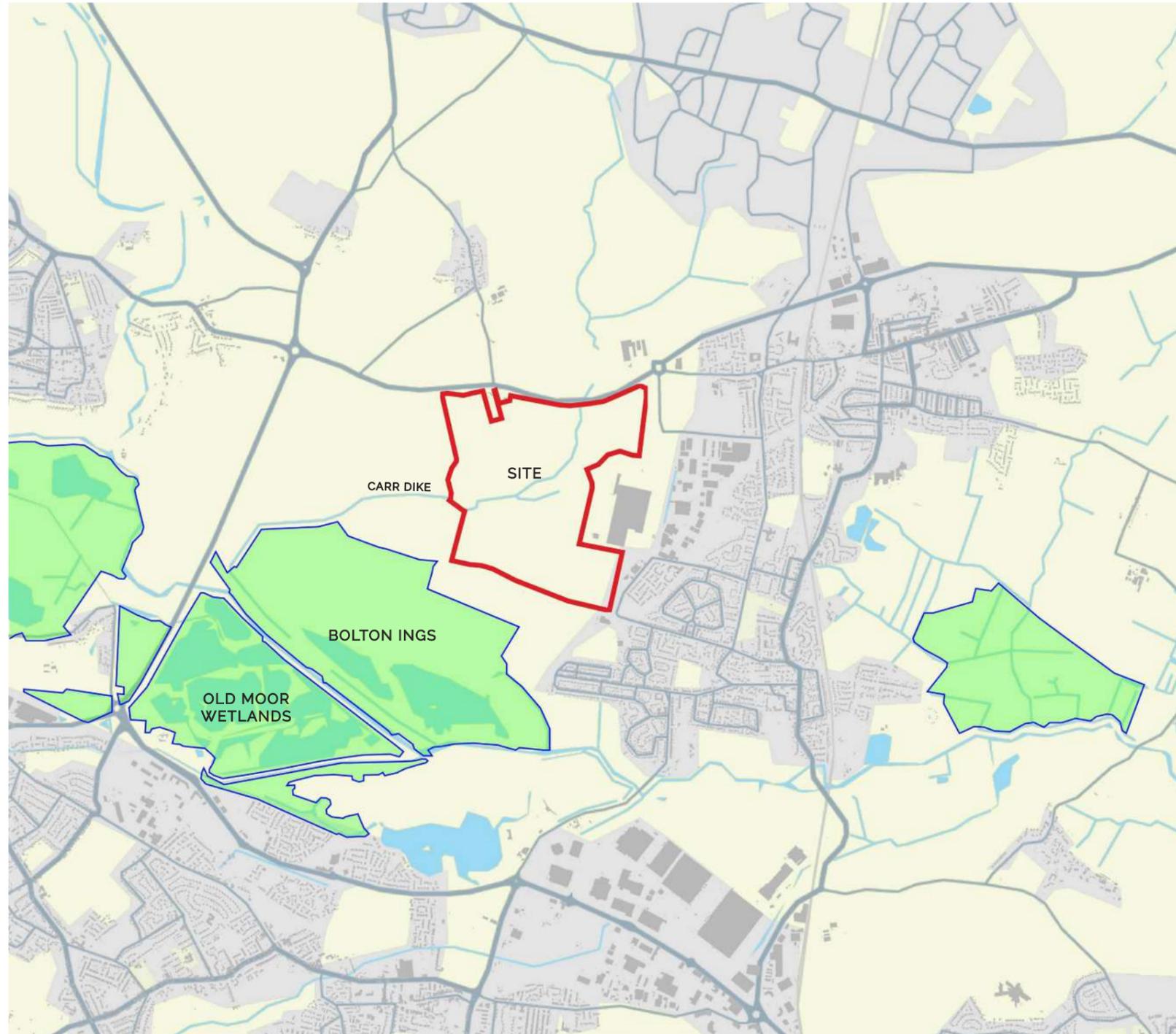
Historical Map 3 | 1980s



Historical Map 4 | 2010s

03 SITE CONTEXT

03.8 ECOLOGY



The site is located approximately 550m North of Bolton Ings and 1km North of Old Moor wetlands, both of which are included in the RSPBs Dearne Valley reserves.

The Dearne Valley supports breeding birds of lowland damp grassland, lowland open water and their margins and scrub. Carr Dike acts as a hydrologic and terrestrial habitat connectivity between the site and the wetlands.

The Barnsley Local Plan included collaboration from the RSPB, Natural England, and the Environmental Agency to understand the sensitivities of the site, of which the Council was able to satisfy that the site is suitable for development. In 2021, Natural England notified that the land to the South of the site including Bolton Ings, Old Moor, and Bolton Tip as a proposed Site of Special Scientific Interest (SSSI). Natural England has confirmed that the development is not prohibited subject to satisfactory mitigation proposals.

The site falls within the Dearne Valley Green Heart Nature Improvement Area. A partnership was formed for the Nature Improvement Area (NIA) with the overall aim of restoring and enhancing the ecological networks of the river, its floodplain, and its link to the link to habitats within its surrounding context.

Within the site sits Carr Dike which enters the site from the Northern boundary and exits on the Western boundary. Bordering Carr Dike is broadened semi-natural woodland, plantation woodland and sections of species poor grassland.

Barnsley Council's Biodiversity and Geodiversity Supplementary Planning Document specifies within the NIA, specific biodiversity enhancements over and above the minimum mitigation/compensation measures are required. The proposed scheme is expected to incorporate full site biodiversity measures including comprehensive sustainable drainage systems and landscape scheme.

The proposal will look to retain as many areas of significant ecological value as possible, but in some instances this may not be possible, thus additional ecological mitigation will be required. The mitigation measures are to achieve a biodiversity net gain of at least 10%.



Satellite Image of Immediate Context

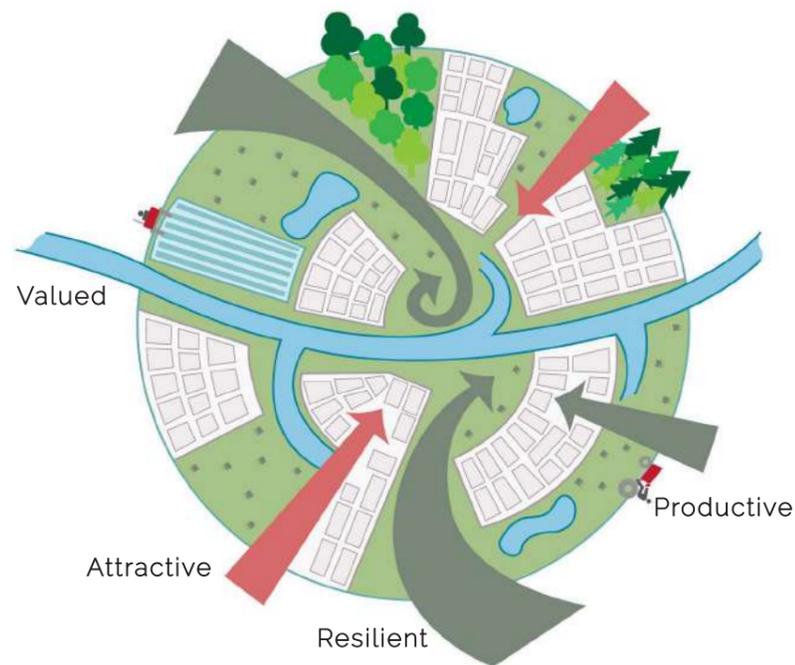
03 SITE CONTEXT

03.9 GREEN INFRASTRUCTURE

The Council recognises the major contribution that the natural environment will make in realising economic ambitions. With the impacts of climate change combined with the decline of traditional industries and the associated economic, social and environmental consequences, there has been stronger focus directed towards how we protect and preserve our natural environment.

Green infrastructure is seen as a critical component to new development; new assets that create economic, social and environmental value together with helping to reduce the long-term costs to the borough of responding to climate change.

Particular emphasis is placed on green infrastructure which radiates in to towns and cities. This vision envisages cities and towns with green infrastructure links in the form of green lungs/wedges such as transport corridors (for example tree lined avenues and arterial routes) across South Yorkshire and beyond. This could involve improving transport routes and green infrastructure investments to key gateways. Green infrastructure nodes could be developed, linking different assets. Public realm improvements could be sought to improve linkages to outer ring roads.



Strategy Vision: A vision of a multi-functional green network for South Yorkshire

The Local Authority set out four strategic objectives:

Objective 1: To accelerate sustainable economic growth

- By increasing the attractiveness of brownfield and employment sites for commercial investment either as new build or estate refurbishment
- Increasing and sustaining a high quality employment offer with a series of on-site open spaces, water bodies, footpaths and landscaping as appropriate
- Creating new parks, open spaces and landscaping to increase the attractiveness of new housing and employment land for investment
- Enhancing the appearance of the public transport hubs and services to promote walking and cycling as journeys to work
- Stimulating investment by creating attractive environments and improving image
- Promoting nature and activity-based tourism
- Encouraging better use of the River Dearne, Don and Dove

Objective 2: To adapt to and mitigate climate change by:

- Using woodlands and peat to increase the Borough's natural carbon storage capacity
- Increasing tree canopy cover on streets and in the public realm-to provide more shade, moderate urban temperatures and reduce surface water run-off
- Maintaining, and where possible increasing, the amount of vegetation cover in urban areas to reduce surface water run-off and increase the cooling effect-e.g. favouring green roofs and green walls in new and refurbished buildings, minimising the use of non-porous surfacing in the public realm and in garden materials and when existing buildings are refurbished
- Creating more areas of open water and water features to increase cooling.
- Increasing the use of Sustainable Drainage Systems (SUDS) to provide storm water attenuation and reduce flood risk.
- Identifying opportunities to undertake river restoration projects, reinstating natural flood plains to create wetlands and flood storage areas.

- Reducing carbon emissions by encouraging people to make local journeys by cycle and or on foot.

- Adopting measures which enable wildlife and habitats to adapt to climate change and maintain biodiversity.

Objective 3: To improve access, movement and connectivity with sustainable travel and secure healthy communities and well-being by:

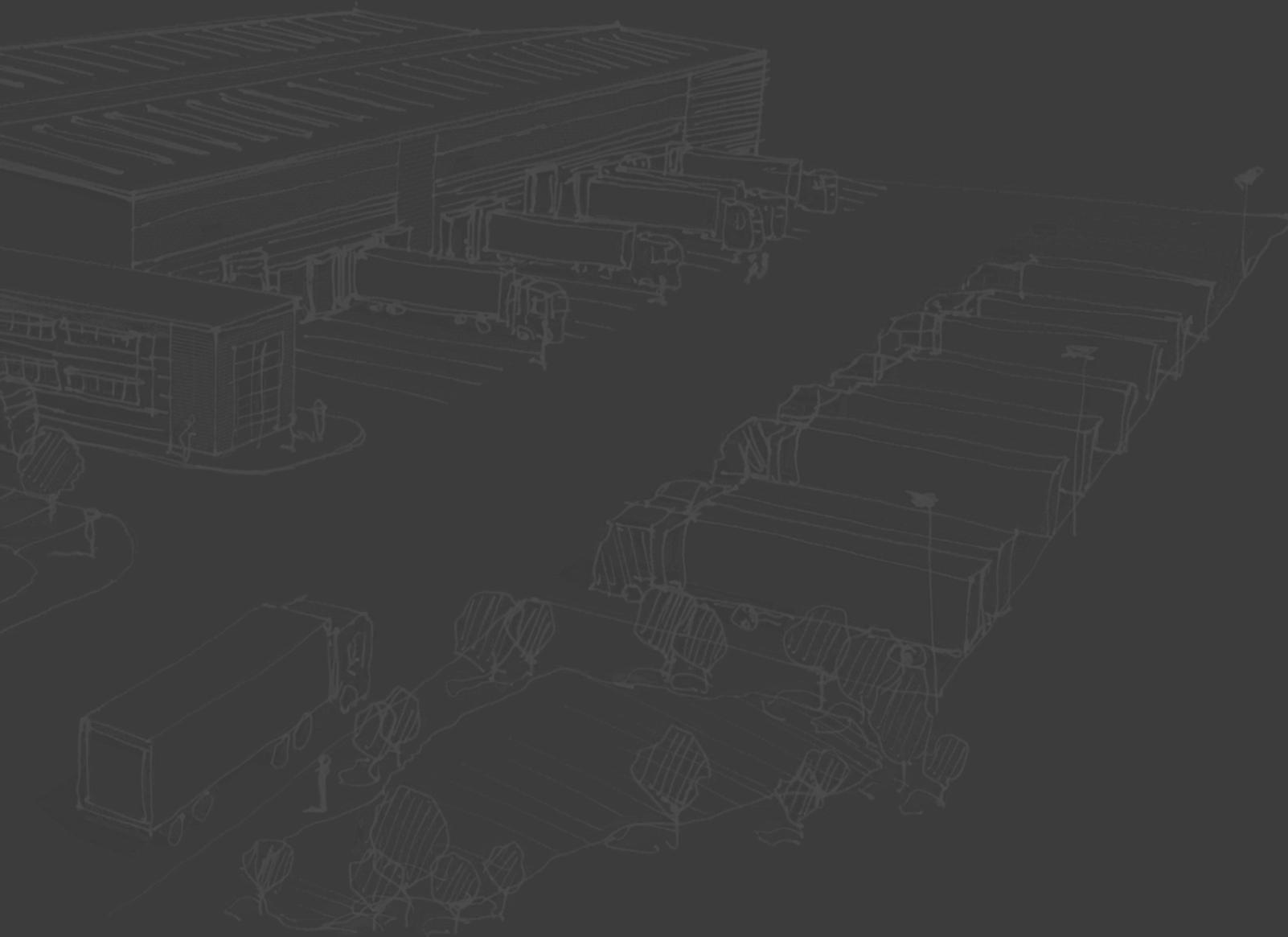
- Increasing the quality and accessibility of natural green space, walkways and cycleways
- Increasing the use of natural green space, walkways, and cycleways
- Providing spaces for play, sport and relaxation – promoting physical and mental health and well-being.
- Fostering links between sites such as parks to create continuous green routes within and between communities
- Making use of natural features such as the rivers to create visual gateways on the strategic transport network

Objective 4: To protect and improve countryside and natural environment by:

- Increasing the areas of high biodiversity value
- Developing a network of multi-functional green spaces
- Conserving and enhancing the priority species and habitats identified in the Biodiversity Action Plan.
- Reducing habitat fragmentation through the creation, extension and restoration of priority wildlife habitats.
- Further enhancing the Borough's designated wildlife and geodiversity sites and ensure all 'Local Wildlife Sites' are in positive management.
- Maintaining and improving the condition of water bodies across the Borough.
- Increasing the use of SUDS drainage techniques to enhance water policy.

04 CONCEPT DESIGN

- 04.1 Key Design Principles
- 04.2 Masterplan Layout Principles
- 04.3 Schematic Design Code Principles Diagram
- 04.4 Design Code



04 CONCEPT DESIGN



Deliver a high quality development that is responsive to the sites constraints, setting and function.



Create a park that engages with the workforce, as well as the local population, to promote a community atmosphere.



Consider the built form, landscape and communal spaces to ensure a high quality, holistic approach.



Utilise modern design methods and materials, to ensure the best possible outcomes for energy use, carbon footprint and wellness.

04.1 KEY DESIGN PRINCIPLES

Taking into account the requirements of the brief and combining with an understanding of the site constraints and opportunities, allows a number of key principles to be established, as follows:

Design & Character

To create an attractive, self-contained and functional development with clear identity, which relates well within its context. Buildings should be well-designed, with attention to detail and provide clear legibility in the choice of façade material specifications.

Functionality

To provide a development that will meet the long-term needs of occupiers for running an efficient and successful business. Clear thought must be given to optimise functionality and avoid unnecessary routes of travel.

Protect Key Viewpoints

To design the building form and elevation treatment taking into account key viewpoints and context of the development. Views may be mitigated with appropriate use of screen bunding and landscaping and where this can't be achieved the architecture of the buildings should address best practice to reduce visual impact.

Orientation & Movement

To ensure that the development provides a sense of arrival for visitors arriving by vehicle or on foot. Routes for HGVs, cars, cyclists and pedestrians should be clearly segregated to avoid potential conflicts. Clarity of design and layout should be at the forefront, with signage being a fallback.

Quality of Public Realm

To create a development which enhances the quality of public realm. Amenity should be provided for the use of all users to create a positive work environment for the area and within public areas of the development. New footpaths should link into the wider existing network, increasing amenity and connectivity.

04.2 MASTERPLAN LAYOUT PRINCIPLES

In order to develop the masterplan and parameters layout, it was necessary to use the briefing requirements alongside the information gathered from analysis of the site. A number of key aspects have been identified in order to lay out the development:

New Access Roundabout

The principal highway access into the development needs to be from the A635. A new roundabout is under construction which will provide suitable access to the site.

Access for Pedestrians & Cyclists

Access for pedestrians and cycles needs to be established and connectivity enabled from the existing offsite infrastructure. Ideally this would be central to the site and provide connectivity between the site and access roads via the new access roundabout.

Configuration of Development Plots

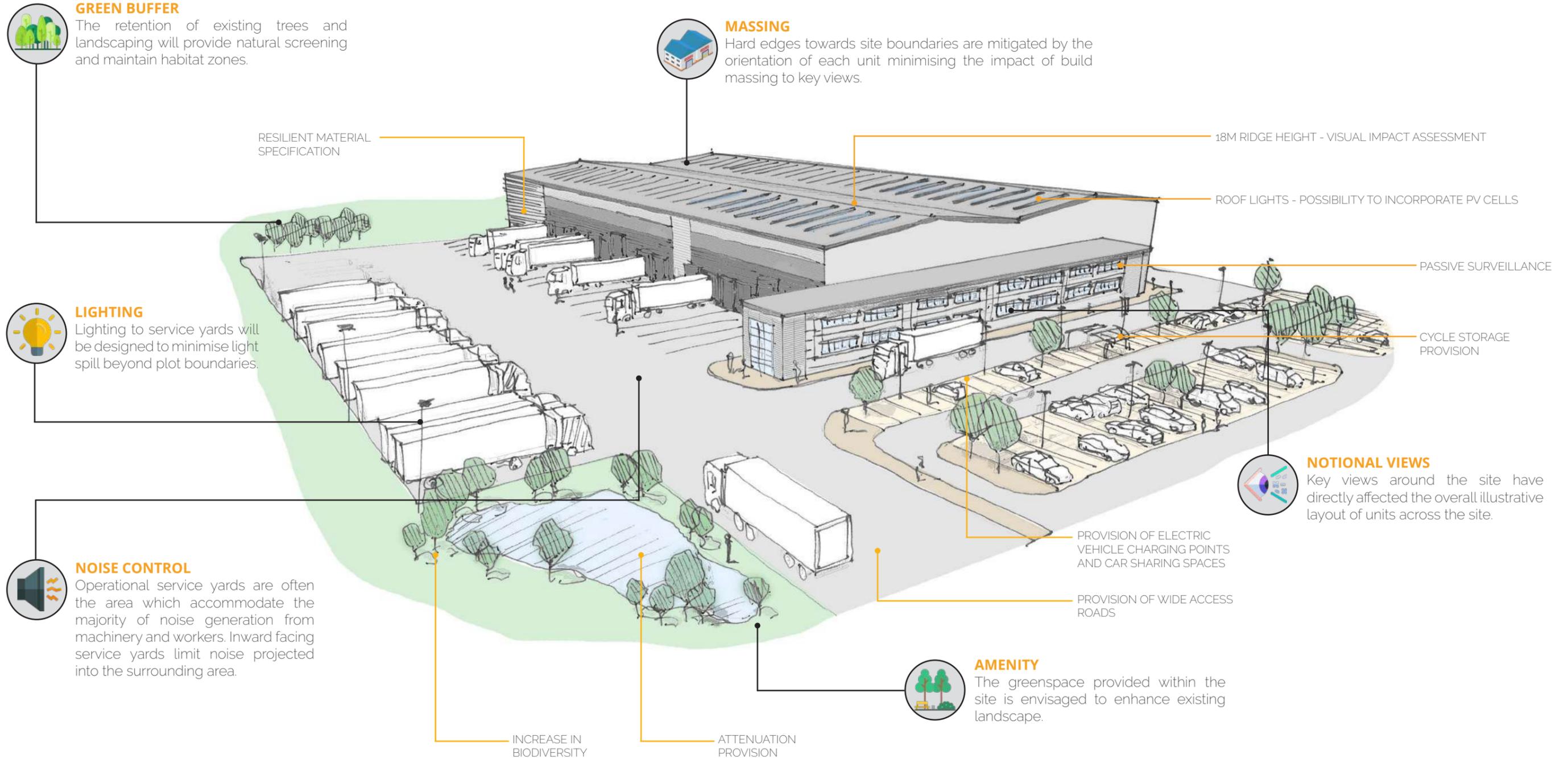
A configuration of 4 development plots each having the ability to cater for varying unit footprints was considered the optimum arrangement in order to offer the required flexibility of floorspace to potential end users and future reserved matters applications. The plots should be configured to provide a centralised spine road allowing access to each of the development plots.

Peripheral mounding and soft landscaping

New earthworks mounding will be formed along peripheral boundaries, which will assist in mitigating both visual and noise impacts. Significant new landscape will be planted where required, which will enhance and soften the bunding, offering screening with varied heights and densities as well as create ecological habitats and deliver biodiversity net gain on site.

04 CONCEPT DESIGN

04.3 SCHEMATIC DESIGN CODE PRINCIPLES DIAGRAM



Key Principles Diagram

04 CONCEPT DESIGN

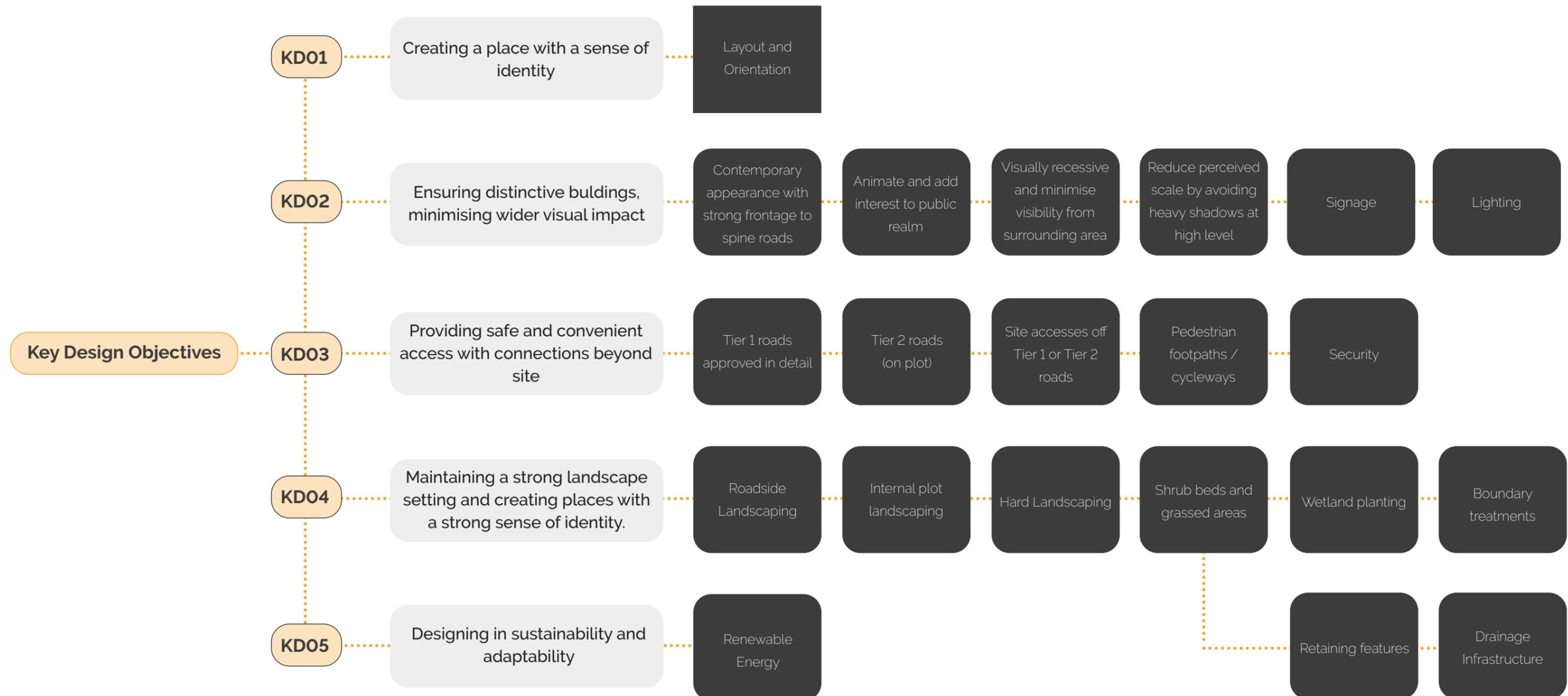
04.4 DESIGN CODE

These key design objectives form the Design Code which will apply to the proposed development. The purpose of the Design Code is to set a framework for the design of all future on-plot developments across the site and for consideration of that design at reserved matters stage by the local planning authority. It will:

- Set out the long term commitment to high quality design from the outset – for buildings, infrastructure and landscaping on plot.

- Enable a cohesive approach to design across the site, building on the detailed design of the initial infrastructure and strategic landscaping, and the approved outline parameters for the development plots.

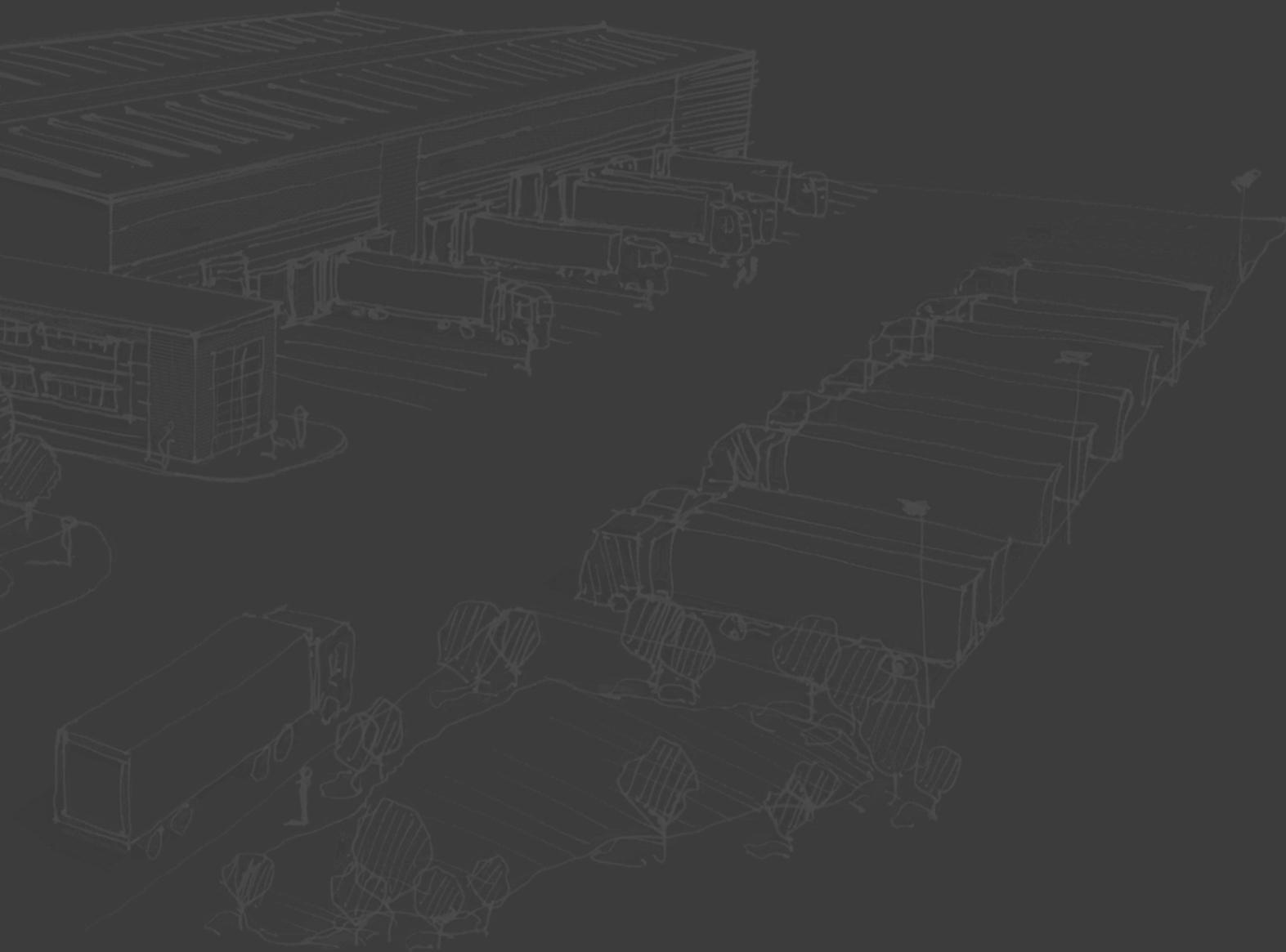
- Allow for a more streamlined process at the reserved matters approval stage.



Design Guide Hierarchy of Elements

05 DESIGN EVOLUTION

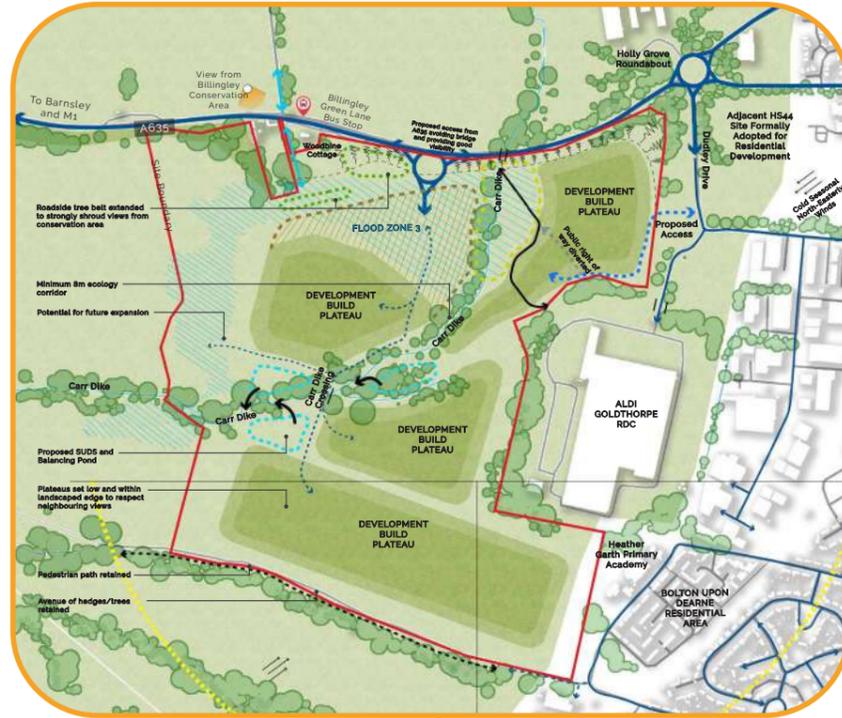
- 05.1 Timeline
- 05.2 Phasing Strategy
- 05.3 Development Plots / Parameters Plan
- 05.4 Design Analysis Summary
- 05.5 Masterplan Concept Development
- 05.6 Elevation / Roof Concept Development
- 05.7 Case Study Concept Development
- 05.8 Elevational Configuration
- 05.9 Masterplan Compliance Assessment
- 05.10 Design Panel Review
- 05.11 Ecological Design Evolution
- 05.12 Fire Statement



05 DESIGN EVOLUTION

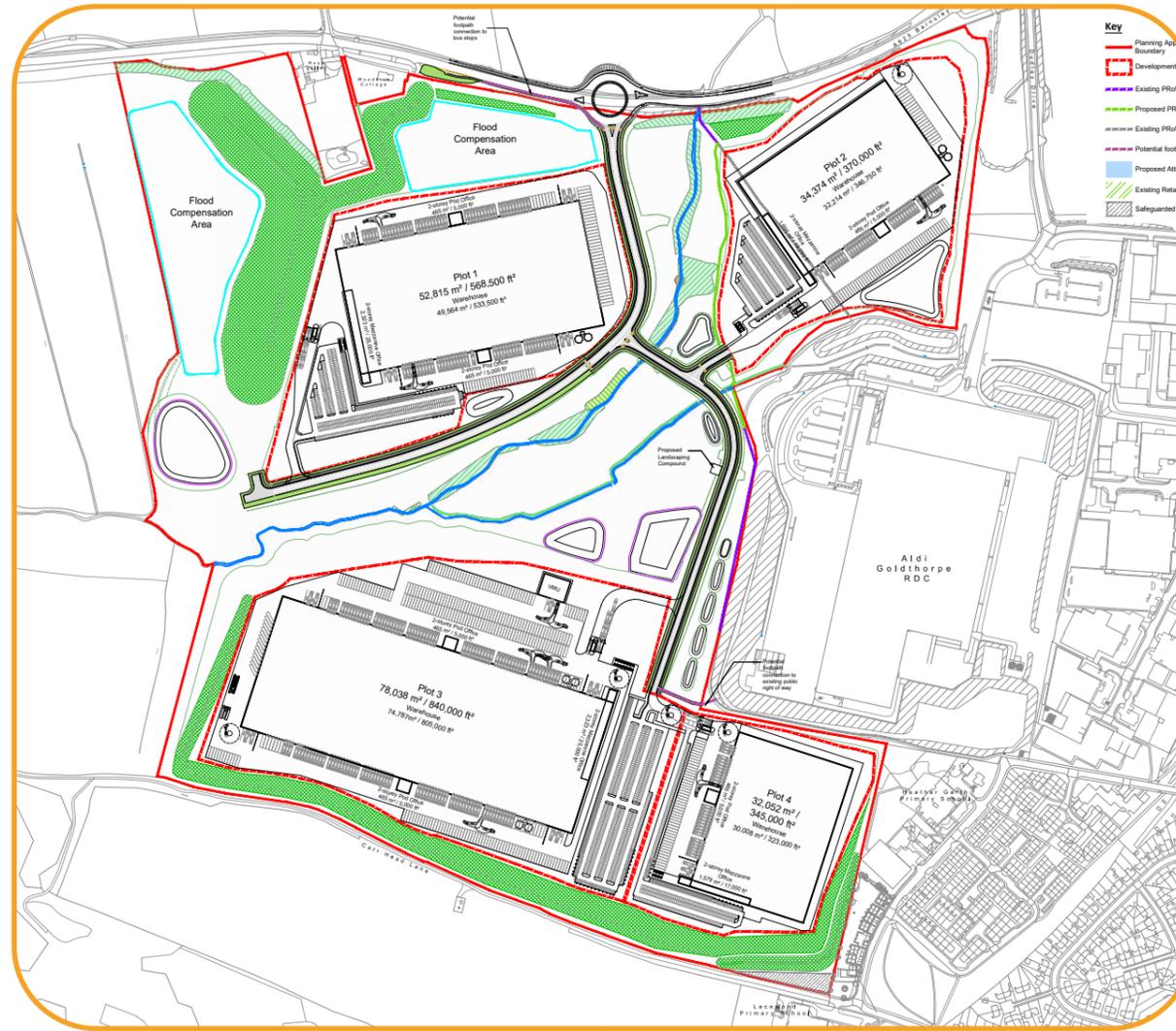
05.1 TIMELINE

CONCEPT DEVELOPMENT COMMENCES 2022



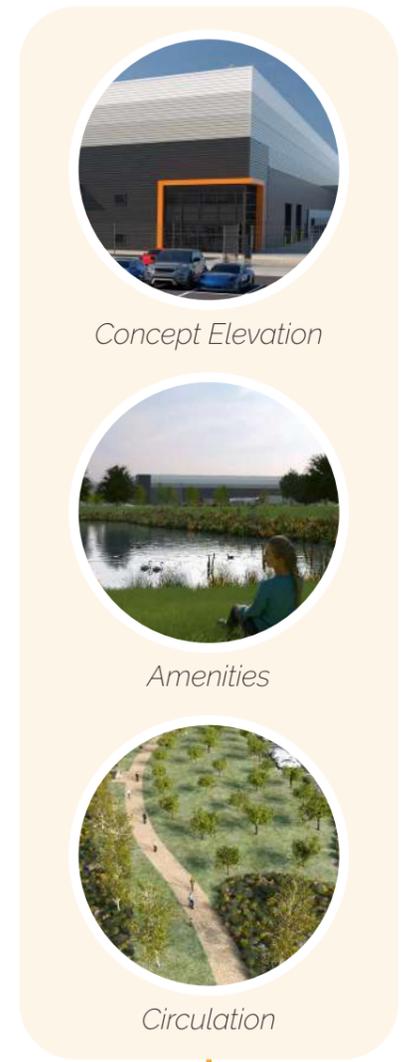
SITE ANALYSIS & STRATEGIC MASTERPLAN

Establishing site constraints and opportunities. Determining principle transport corridors, development areas & landscape zones. Designing in accordance with the Council's approved Masterplan Framework.



SKETCH MASTERPLAN

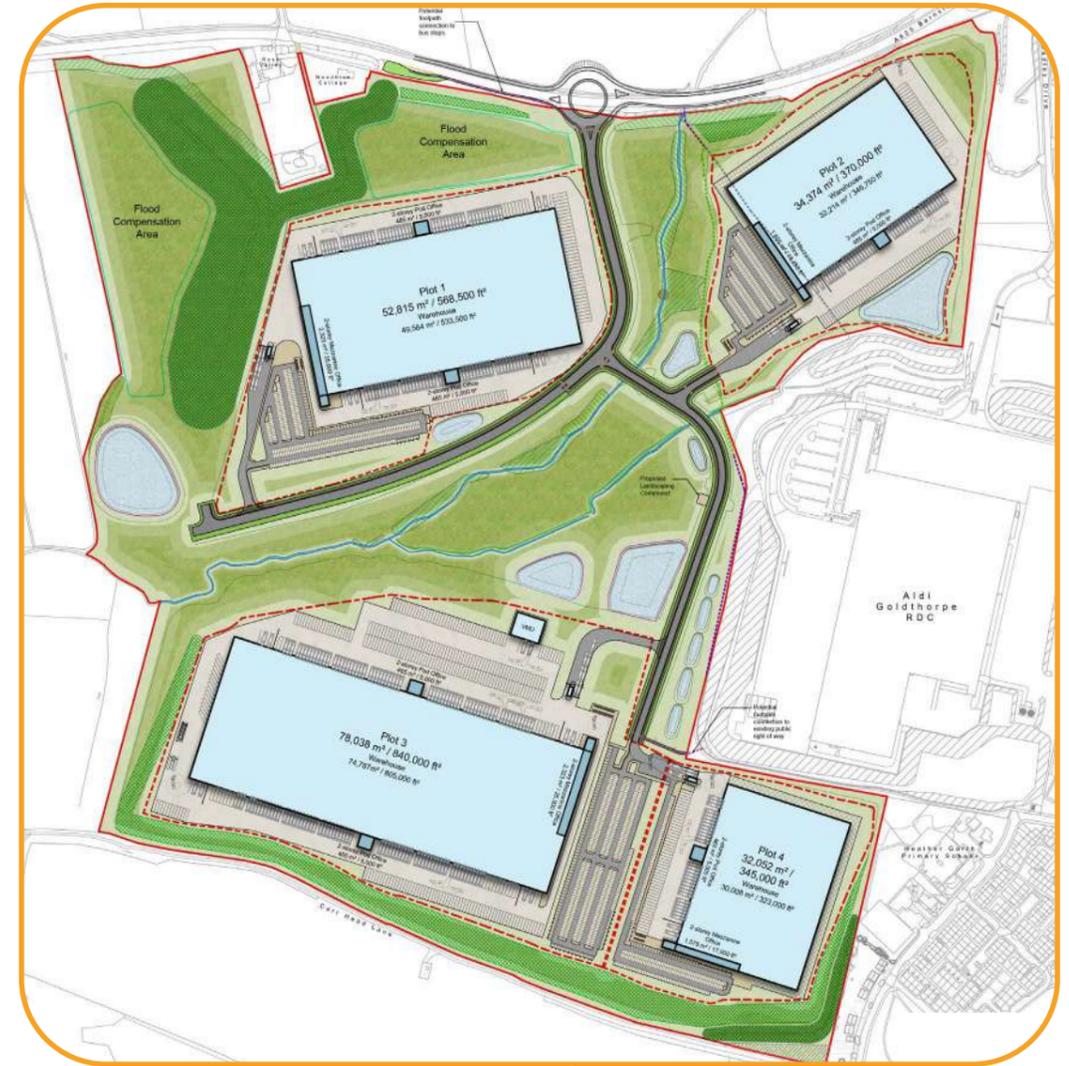
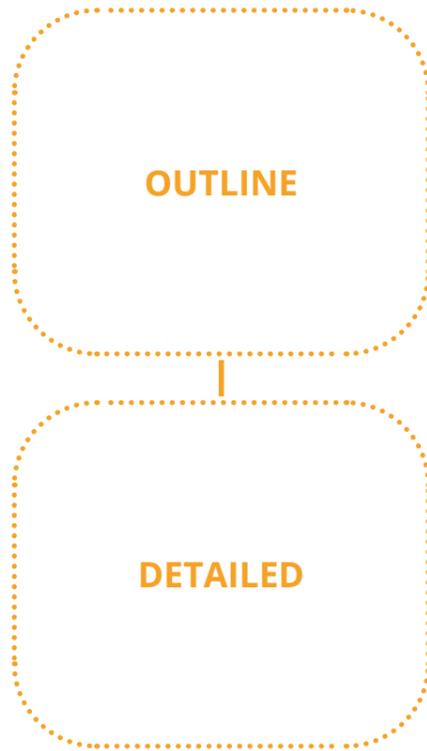
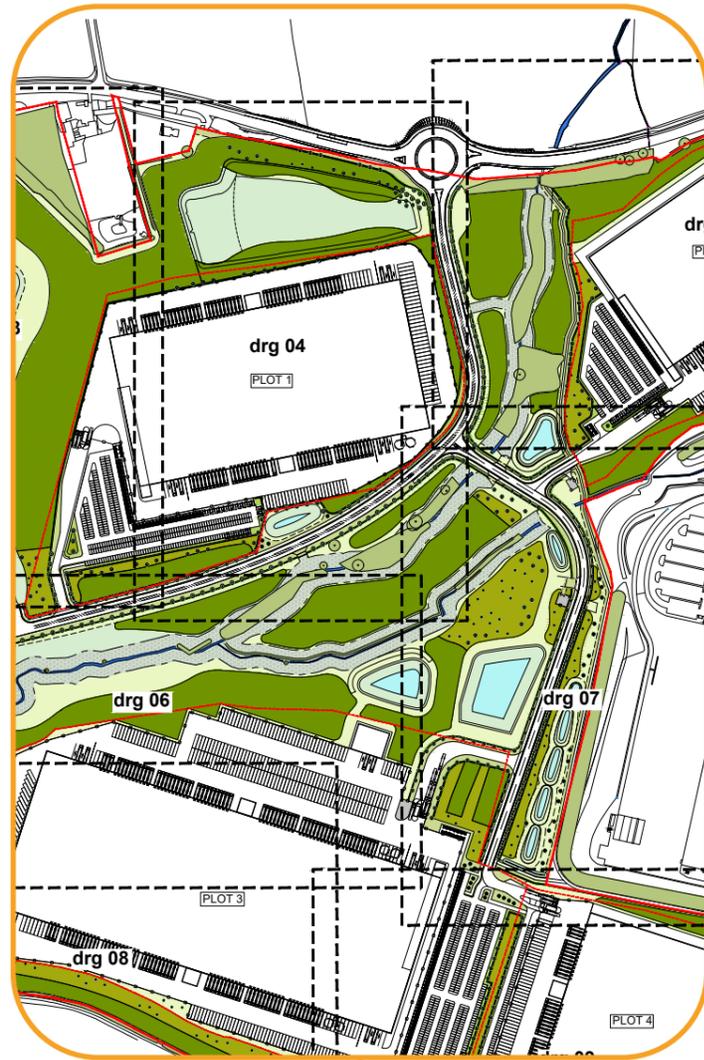
Development of plateaus established within principle road and footpath network corridors identified for blue and green infrastructure. Plot options are explored conceptually.



CONCEPT DESIGN DEVELOPMENT

Develop conceptual design in collaboration between architectural engineering and landscape disciplines.

05 DESIGN EVOLUTION



LANDSCAPING STRATEGY

Green & blue infrastructure developed with defined visual impact analysis, noise mitigation, movement & amenity. Planting strategies are confirmed.

HYBRID APPLICATION

Application material is developed in detail with technical considerations concluded on road design footpath arrangements & infrastructure attenuation strategy.

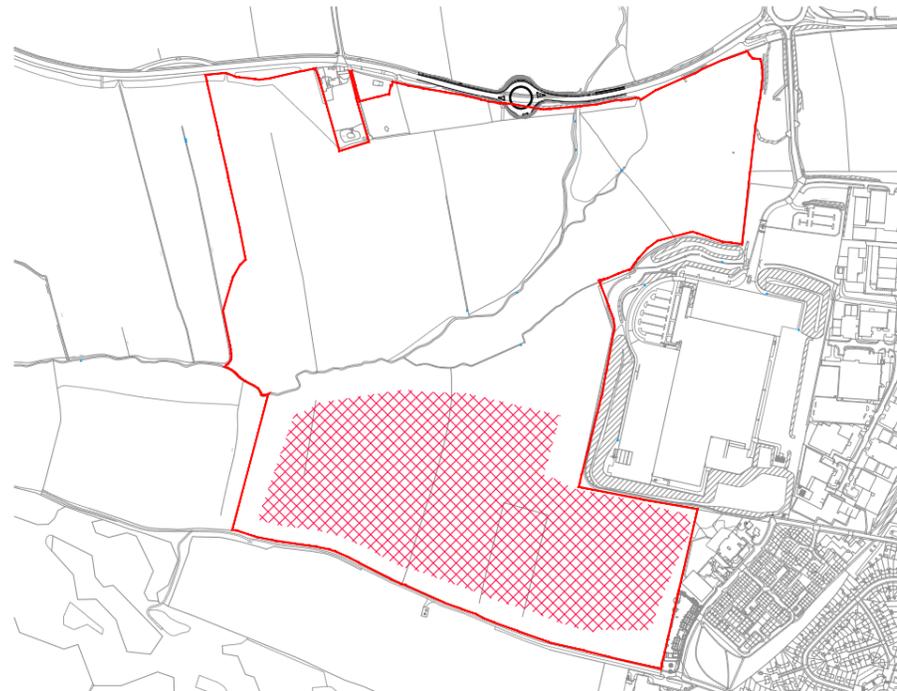
MASTERPLAN OPTIONS

Within committed designs for the infrastructure, a proposed illustrative masterplan is developed for the on-plot arrangement of buildings service yards and car parks which is used as the basis for the public consultation engagement.

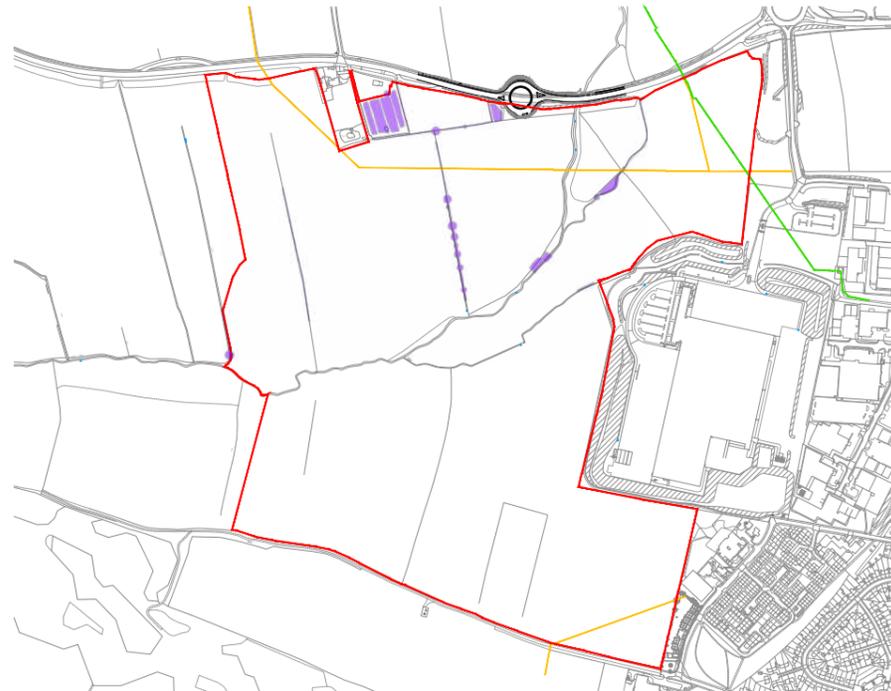
PRESENT

05 DESIGN EVOLUTION

05.2 PHASING STRATEGY



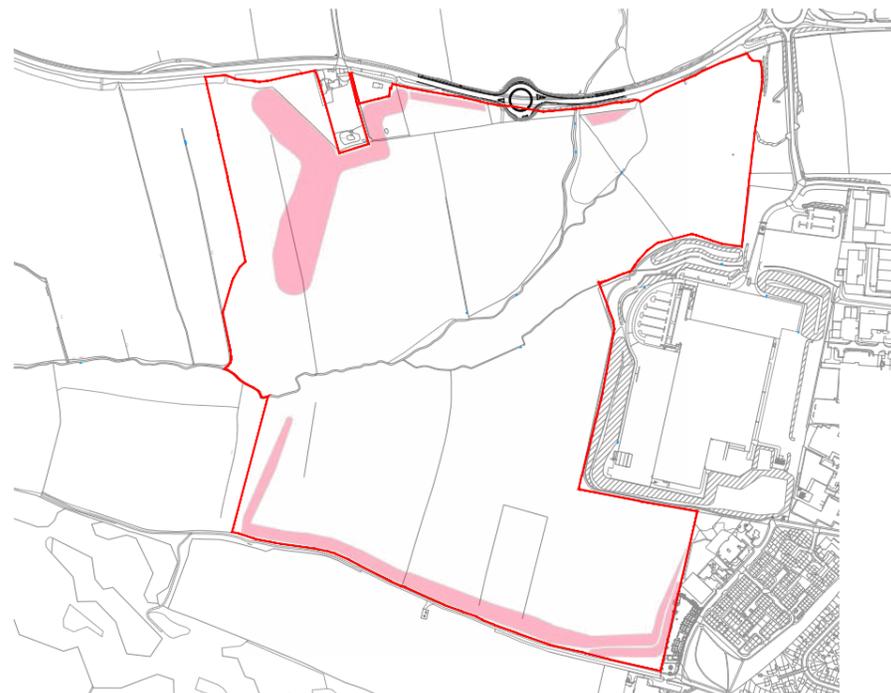
PHASE 1 - Archaeology (Extent to be agreed)



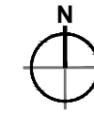
PHASE 2 - Site Clearance & Utilities Diversion



PHASE 3 - Earthworks (Plateaus to be Created)



PHASE 4 - Screen Bunding

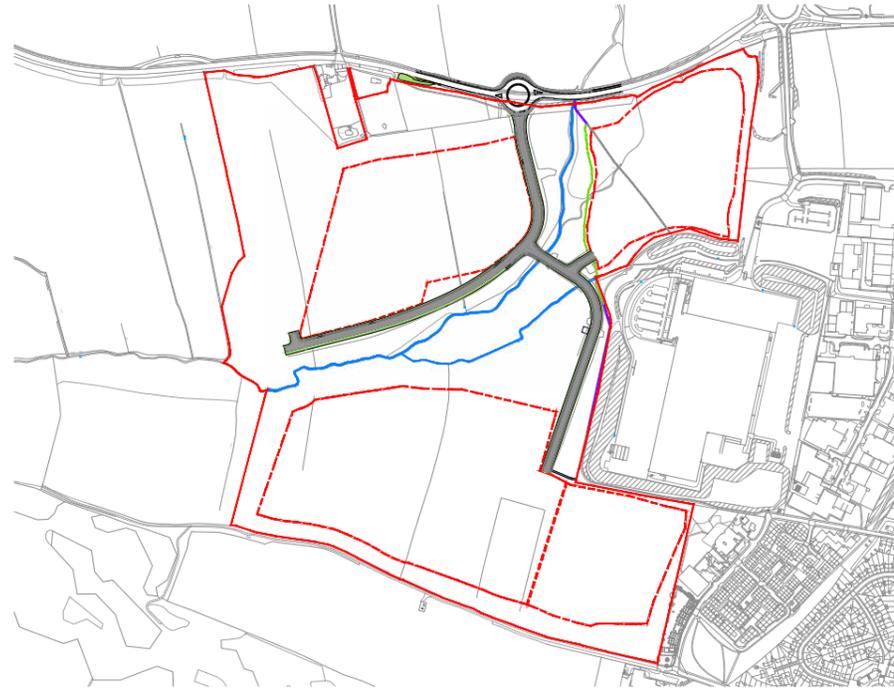


Key

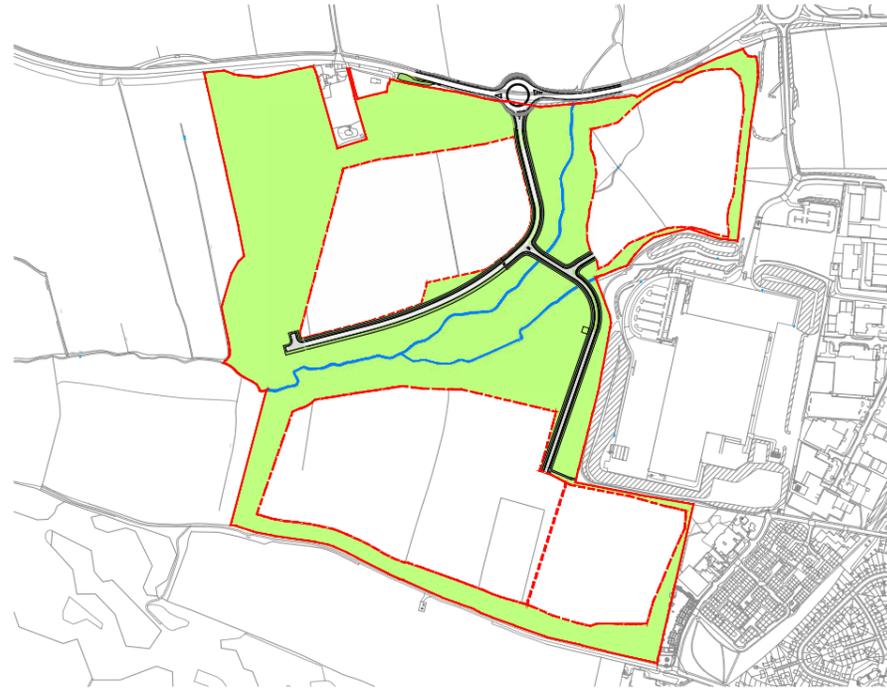
- Planning Application Boundary 210.81 ac 85.31 ha
- Phase 1: Areas of Archaeological Works
- Phase 2: Site Clearance
- Phase 2: Existing YW Public Sewer Removed
- Phase 2: Existing Overhead Power-lines Diverted
- Phase 3: Earthworks to Form Plateaus
- Phase 3: Attenuation
- Phase 4: Screening Bund
- Phase 5: Site Roads with Combined Cycle & Footway
- Phase 5: Site Drainage
- Phase 5: Existing PRoW Retrained
- Phase 5: Existing PRoW Diverted
- Phase 5: Existing PRoW Removed
- Phase 5: Potential footpath link created
- Phase 6: Strategic Landscaping
- Phase 7: Future Reserved Matters

05 DESIGN EVOLUTION

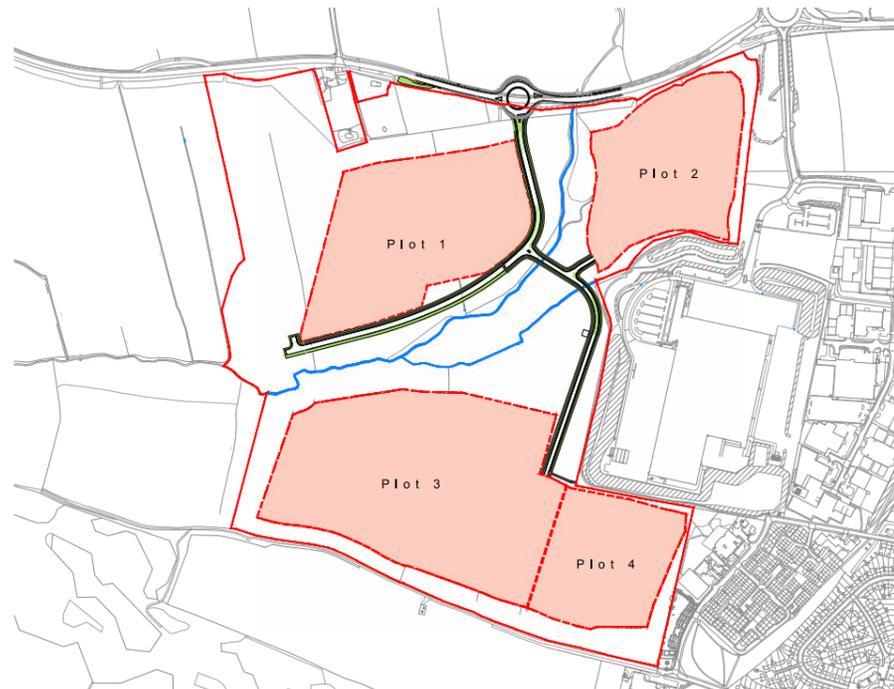
05.2 PHASING STRATEGY



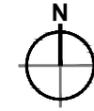
PHASE 5 - Onsite Roads & PRoW Diversion



PHASE 6 - Landscaping (To Perimeter of Site)



PHASE 7 - Build-Out Plots



Key

- Planning Application Boundary 210.81 ac 85.31 ha
- Phase 1: Areas of Archaeological Works
- Phase 2: Site Clearance
- Phase 2: Existing YW Public Sewer Removed
- Phase 2: Existing Overhead Power-lines Diverted
- Phase 3: Earthworks to Form Plateaus
- Phase 3: Attenuation
- Phase 4: Screening Bund
- Phase 5: Site Roads with Combined Cycle & Footway
- Phase 5: Site Drainage
- Phase 5: Existing PRoW Retrained
- Phase 5: Existing PRoW Diverted
- Phase 5: Existing PRoW Removed
- Phase 5: Potential footpath link created
- Phase 6: Strategic Landscaping
- Phase 7: Future Reserved Matters

05 DESIGN EVOLUTION



05.3 DEVELOPMENT PLOTS / PARAMETERS PLAN

As a result of the design evolution described in the previous sections a parameters plan has been generated for submission under this application. The plan proposes 4 development plots / plateaus which could accommodate a wide range of potential future Occupier requirements.

The parameters plan indicates the proposed finished floor levels and maximum building heights for the various parts of the application site. Whilst the detailed design of the site layout is not yet fixed, a total footprint of up to 204,000sqm is proposed across the 34 development plots.

The plan demonstrates how the periphery of the site is used to accommodate landscape and planting to soften the edges of the development and provide an element of screening to key viewpoints most notably to the northern boundary adjacent to plot 1.

Development Schedule (Outline application)						
Plot	Plot Size NDA (ha)	Maximum GIA Floor Space (m ²)	Plateau Height (in meters above ordnance datum)	Maximum Finished Floor Level (in meters above ordnance datum) [+1.000m above proposed plateau]	Maximum Building Height Measured to roof ridge / highest point (in meters above ordnance datum)	Ridge Height (above F.F.L. level)
Plot 1	11.35	204,000m ² Total Area distributed across Plots 1, 2, 3 & 4	24.50	25.50	43.50	18.00
Plot 2	8.46		25.00	26.00	44.00	18.00
Plot 3	17.92		33.70	34.70	52.70	18.00
Plot 4	6.29		33.70	34.70	52.70	18.00
Total	44.02					
plot						
The use class applied for is primarily Class B8 with up to 30% of the floorspace being for Class B2 together with ancillary office space						

05.4 DESIGN ANALYSIS SUMMARY

Establish a high quality development in a prominent gateway location.

The proposal aims to achieve a high quality development by the successful integration of design, sustainability and connectivity. The site is superbly connected to the strategic road network being immediately adjacent to the A635. The entrance and internal estate roads will be seen from each of the ancillary offices and will be positioned to provide a positive aspect within the development, in addition to providing clear orientation of the building entrances.

Contribute directly to creating safe, attractive spaces with high quality buildings and landscape.

The proposed development is a landscape led design, analysing and enhancing the existing environment in terms of green and blue infrastructure and utilising these natural assets in terms of natural security and amenity across the site.

The proposed building appearance will be defined through future reserved matters applications however the design intention is that all units will be sleek and simple in design, with a contemporary colour palette and large glazed areas providing a high quality office environment. Individual plots allow for retaining dense landscaped areas, providing a softer environment for site users

Design buildings which respect existing context and comply with height parameters.

The proposed development will have an acceptable and respectful relationship with surrounding land uses and the open countryside whilst mitigating visual impact on the surrounding context.

Create sustainable, well designed buildings, which are good places to work.

The design details for each unit will be determined via future reserved matters applications however the design intention will be to create a contemporary simplistic aesthetic with high quality materials. A modern colour palette will be proposed with feature offices facing the internal road where possible, to create an attractive façade and a consistent architectural language throughout the site.

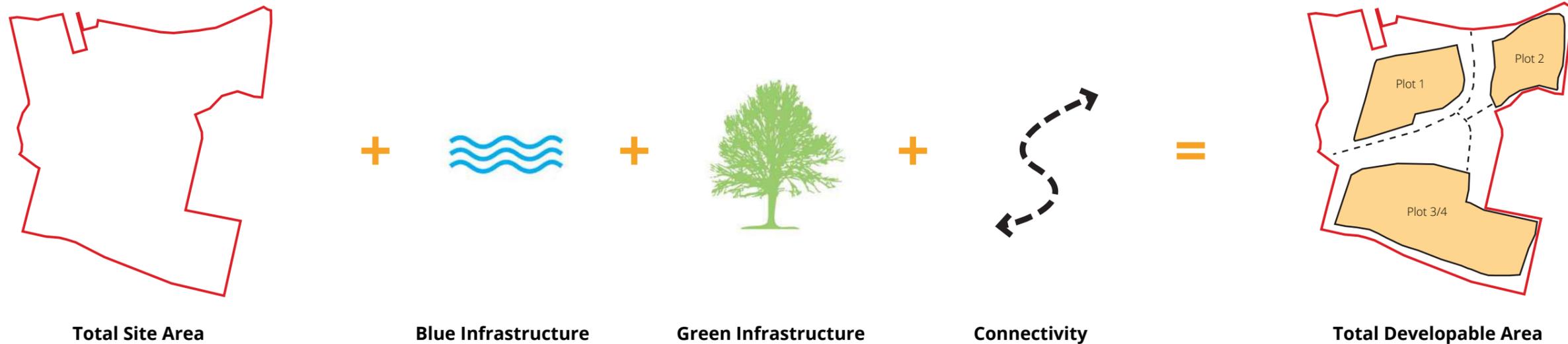
Each unit will also be designed so that where applicable natural lighting will provide the primary source of light. Good lighting levels throughout the development mitigate health and safety hazards within the workplace whilst promoting a healthy and efficient work force. Indoor air pollution coupled with bad ventilation can lead to a number of health problems. Therefore, all units and offices across the site will be ventilated to provide good amounts of fresh air.

The buildings and site layout should fulfil a required level of security and operational functionality.

The yards are to be securely contained and shielded with fencing and offer secure car parking away from HGV Vehicles where appropriate. Car parks are overlooked by the office area and separate entrances for cars and HGVs have been established where appropriate. This strategy is to be reviewed and determined as part of future reserved matters applications.

Ensure inclusive design to allow for a good level of accessibility within and between buildings.

The entire illustrative masterplan is suitable for both able-bodied and accessible use, not restricting users anywhere. The appropriate amount of accessible parking has been allocated and is located close to the level building entrance where possible.



Masterplanning Concept Diagram

05 DESIGN EVOLUTION

05.5 MASTERPLAN CONCEPT DEVELOPMENT

KEY DESIGN FEATURES

- 5 Units
- Green Buffer Zones
- Central Spine Connectivity to Existing Vehicular Infrastructure
- Active Frontage
- Design Plateau Utilisation



KEY DESIGN FEATURES

- 3 Units
- Green Buffer Zones
- Central Spine Connectivity to Existing Vehicular Infrastructure
- Active Frontage
- Design Plateau Utilisation



KEY DESIGN PRINCIPLES ACHIEVED

- Design & Character
- Functionality
- Orientation & Movement
- Quality of Public Realm

MASTERPLAN LAYOUT PRINCIPLES ACHIEVED

- New Access Roundabout
- Access for Pedestrians & Cyclists
- Configuration of Development Plots
- Peripheral mounding and soft landscaping

KEY DESIGN CODE

- KD01** Creating a place with a sense of identity
- KD02** Ensuring distinctive buildings minimise wider visual impact
- KD03** Providing safe and convenient access with connections beyond site
- KD04** Maintaining a strong landscape setting and creating places with a strong sense of identity
- KD05** Designing in sustainability and adaptability



KEY DESIGN PRINCIPLES ACHIEVED

- Design & Character
- Functionality
- Orientation & Movement
- Quality of Public Realm

MASTERPLAN LAYOUT PRINCIPLES ACHIEVED

- New Access Roundabout
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- KD05** Designing in sustainability and adaptability



05 DESIGN EVOLUTION

05.5 MASTERPLAN CONCEPT DEVELOPMENT

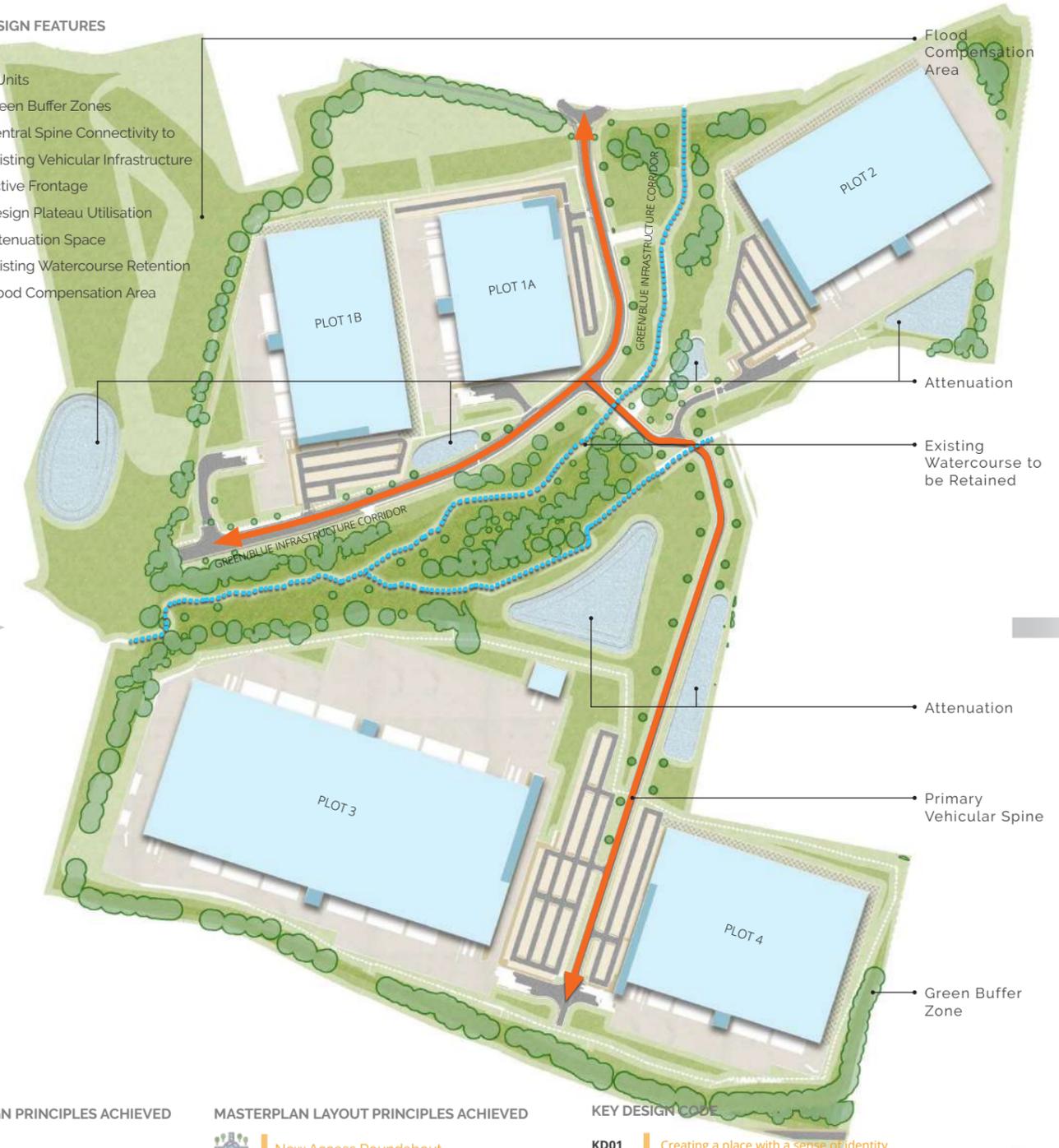
KEY DESIGN FEATURES

- 5 Units
- Green Buffer Zones
- Central Spine Connectivity to Existing Vehicular Infrastructure
- Active Frontage
- Design Plateau Utilisation
- Attenuation Space



KEY DESIGN FEATURES

- 5 Units
- Green Buffer Zones
- Central Spine Connectivity to Existing Vehicular Infrastructure
- Active Frontage
- Design Plateau Utilisation
- Attenuation Space
- Existing Watercourse Retention
- Flood Compensation Area



KEY DESIGN PRINCIPLES ACHIEVED

- Design & Character
- Functionality
- Orientation & Movement
- Quality of Public Realm

MASTERPLAN LAYOUT PRINCIPLES ACHIEVED

- New Access Roundabout
- Access for Pedestrians & Cyclists
- Configuration of Development Plots
- Peripheral mounding and soft landscaping

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- KD01** Creating a place with a sense of identity
- KD02** Ensuring distinctive buildings minimise wider visual impact
- KD03** Providing safe and convenient access with connections beyond site
- KD04** Maintaining a strong landscape setting and creating places with a strong sense of identity
- KD05** Designing in sustainability and adaptability



KEY DESIGN PRINCIPLES ACHIEVED

- Design & Character
- Functionality
- Protect Key Viewpoints
- Orientation & Movement
- Quality of Public Realm

MASTERPLAN LAYOUT PRINCIPLES ACHIEVED

- New Access Roundabout
- Access for Pedestrians & Cyclists
- Configuration of Development Plots
- Peripheral mounding and soft landscaping

KEY DESIGN CODE

- KD01** Creating a place with a sense of identity
- KD02** Ensuring distinctive buildings minimise wider visual impact
- KD03** Providing safe and convenient access with connections beyond site
- KD04** Maintaining a strong landscape setting and creating places with a strong sense of identity
- KD05** Designing in sustainability and adaptability



05 DESIGN EVOLUTION

05.5 MASTERPLAN CONCEPT DEVELOPMENT

KEY DESIGN FEATURES

- 4 Units
- Green Buffer Zones
- Central Spine Connectivity to Existing Vehicular Infrastructure
- Active Frontage
- Design Plateau Utilisation
- Attenuation Space
- Existing Watercourse Retention
- Flood Compensation Area
- Retained Vegetation



KEY DESIGN PRINCIPLES ACHIEVED

- Design & Character
- Functionality
- Protect Key Viewpoints
- Orientation & Movement
- Quality of Public Realm

KEY DESIGN CODE

- KD01** Creating a place with a sense of identity
- KD02** Ensuring distinctive buildings minimise wider visual impact
- KD03** Providing safe and convenient access with connections beyond site
- KD04** Maintaining a strong landscape setting and creating places with a strong sense of identity
- KD05** Designing in sustainability and adaptability

MASTERPLAN LAYOUT PRINCIPLES ACHIEVED

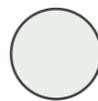
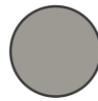
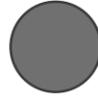
- New Access Roundabout
- Access for Pedestrians & Cyclists
- Configuration of Development Plots
- Peripheral mounding and soft landscaping

05 DESIGN EVOLUTION

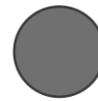
05.6 ELEVATION / ROOF CONCEPT DEVELOPMENT



Gradient Horizontal Cladding:

-  Horizontal Cladding
Colour: White
-  Horizontal Cladding
Colour: Light Grey
-  Horizontal Cladding
Colour: Mid Grey

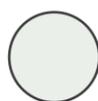
Main Wall Vertical Cladding:

-  Vertical Cladding
Colour: Dark Grey
-  Vertical Cladding
Colour: Mid Grey

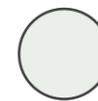
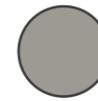
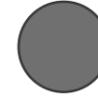
Feature Banding and Panelling:

-  Feature Banding
Colour: Blue Grey

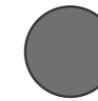
Office Composite Panels:

-  Composite Panels
Colour: White

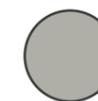
Gradient Horizontal Cladding:

-  Horizontal Cladding
Colour: White
-  Horizontal Cladding
Colour: Light Grey
-  Horizontal Cladding
Colour: Mid Grey

Main Wall Vertical Cladding:

-  Vertical Cladding
Colour: Dark Grey
-  Vertical Cladding
Colour: Mid Grey

Feature Banding and Panelling:

-  Feature Banding
Colour: Light Grey

Office Composite Panels:

-  Composite Panels
Colour: Dark Grey

Timber Panels:

-  Timber Panels
Colour: Timber

05 DESIGN EVOLUTION

05.6 ELEVATION / ROOF CONCEPT DEVELOPMENT



Pixelated Horizontal Cladding:

-  Horizontal Cladding
Colour: White
-  Horizontal Cladding
Colour: Light Grey
-  Horizontal Cladding
Colour: Mid Grey
-  Feature Banding
Colour: Blue Grey

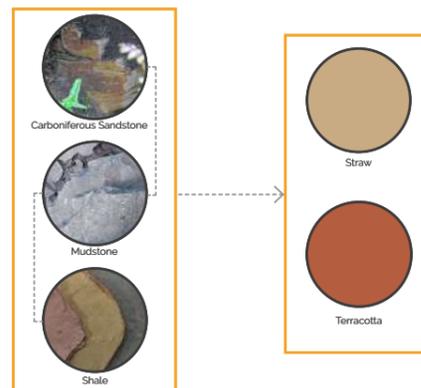
Main Wall Vertical Cladding:

-  Vertical Cladding
Colour: Dark Grey
-  Vertical Cladding
Colour: Mid Grey
-  Feature Banding
Colour: Blue Grey
-  Feature Flashing
Colour: Terracotta

Office Composite Panels:

-  Composite Panels
Colour: Straw

Colour Selection:



Pixelated Horizontal Cladding:

-  Horizontal Cladding
Colour: White
-  Horizontal Cladding
Colour: Light Grey
-  Horizontal Cladding
Colour: Mid Grey
-  Feature Banding
Colour: Blue Grey

Main Wall Vertical Cladding:

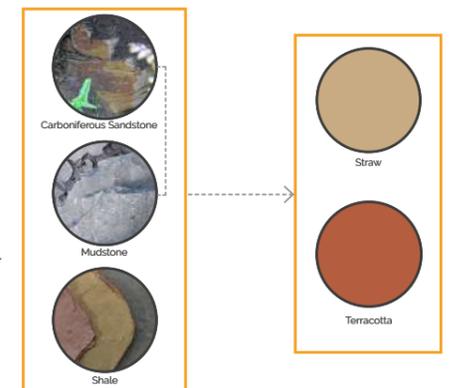
-  Vertical Cladding
Colour: Dark Grey
-  Vertical Cladding
Colour: Mid Grey
-  Feature Banding
Colour: Terracotta

Feature Banding and Panelling:

Office Composite Panels:

-  Composite Panels
Colour: Straw

Colour Selection:



05 DESIGN EVOLUTION

05.6 ELEVATION / ROOF CONCEPT DEVELOPMENT

External View



External View



Aerial View



Aerial View



Pixelated Horizontal Cladding:

-  **Horizontal Cladding**
Colour: White
-  **Horizontal Cladding**
Colour: Light Grey
-  **Horizontal Cladding**
Colour: Mid Grey
-  **Feature Banding**
Colour: Blue Grey

Feature Flashing:

-  **Feature Flashing**
Colour: Terracotta

Main Wall Vertical Cladding:

-  **Vertical Cladding**
Colour: Dark Grey
-  **Vertical Cladding**
Colour: Mid Grey

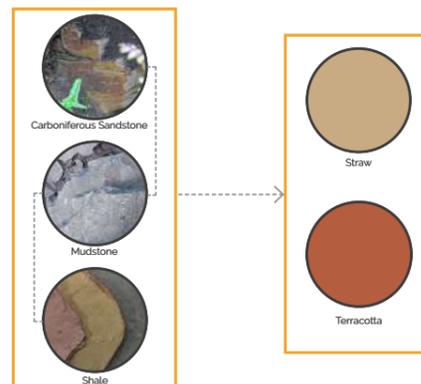
Office Composite Panels:

-  **Composite Panels**
Colour: Straw
-  **Feature Flashing**
Colour: Terracotta

Feature Banding and Panelling:

-  **Feature Banding**
Colour: Blue Grey

Colour Selection:



Gradient Horizontal Cladding:

-  **Horizontal Cladding**
Colour: White
-  **Horizontal Cladding**
Colour: Light Grey
-  **Horizontal Cladding**
Colour: Mid Grey
-  **Feature Banding**
Colour: Blue Grey

Main Wall Vertical Cladding:

-  **Vertical Cladding**
Colour: Dark Grey
-  **Vertical Cladding**
Colour: Mid Grey

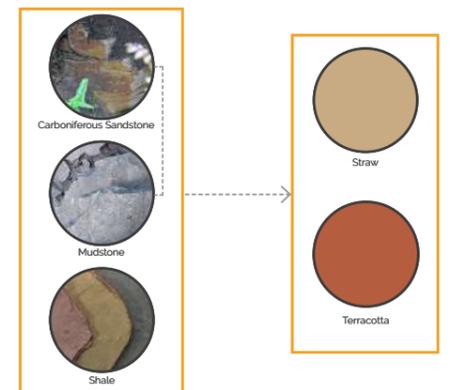
Feature Banding and Panelling:

-  **Feature Banding**
Colour: Terracotta

Office Composite Panels:

-  **Composite Panels**
Colour: Straw

Colour Selection:



05 DESIGN EVOLUTION

05.6 ELEVATION / ROOF CONCEPT DEVELOPMENT



Gradient Horizontal Cladding:

-  **Horizontal Cladding**
Colour: White
-  **Horizontal Cladding**
Colour: Light Grey
-  **Horizontal Cladding**
Colour: Mid Grey

Main Wall Vertical Cladding:

-  **Vertical Cladding**
Colour: Dark Grey
-  **Vertical Cladding**
Colour: Mid Grey

Feature Banding and Panelling:

-  **Feature Banding**
Colour: Light Grey

Office Composite Panels:

-  **Composite Panels**
Colour: Dark Grey

Timber Panels:

-  **Timber Panels**
Colour: Timber

Gradient Horizontal Cladding:

-  **Horizontal Cladding**
Colour: White
-  **Horizontal Cladding**
Colour: Light Grey
-  **Horizontal Cladding**
Colour: Mid Grey

Main Wall Vertical Cladding:

-  **Vertical Cladding**
Colour: Dark Grey
-  **Vertical Cladding**
Colour: Mid Grey

Feature Banding and Panelling:

-  **Feature Banding**
Colour: Light Grey

Office Composite Panels:

-  **Composite Panels**
Colour: Dark Grey

Timber Panels:

-  **Timber Panels**
Colour: Timber

05 DESIGN EVOLUTION

05.6 ELEVATION / ROOF CONCEPT DEVELOPMENT



Gradient Horizontal Cladding:

-  Horizontal Cladding
Colour: White
-  Horizontal Cladding
Colour: Light Grey
-  Horizontal Cladding
Colour: Mid Grey

Main Wall Vertical Cladding:

-  Vertical Cladding
Colour: Dark Grey
-  Vertical Cladding
Colour: Mid Grey

Feature Banding and Panelling:

-  Feature Banding
Colour: Light Grey

Office Composite Panels:

-  Composite Panels
Colour: Dark Grey

Timber Panels:

-  Timber Panels
Colour: Timber

Roof Cladding:

-  Cladding Panels
Colour: Green

Gradient Horizontal Cladding:

-  Horizontal Cladding
Colour: White
-  Horizontal Cladding
Colour: Light Grey
-  Horizontal Cladding
Colour: Mid Grey

Main Wall Vertical Cladding:

-  Vertical Cladding
Colour: Dark Grey
-  Vertical Cladding
Colour: Mid Grey

Feature Banding and Panelling:

-  Feature Banding
Colour: Light Grey

Office Composite Panels:

-  Composite Panels
Colour: Dark Grey

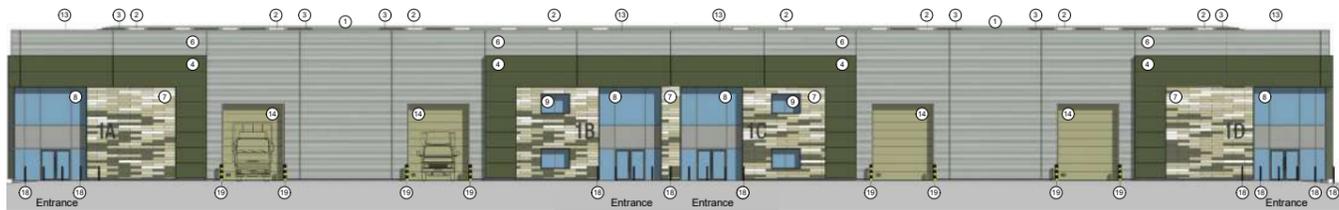
Timber Panels:

-  Timber Panels
Colour: Timber

05 DESIGN EVOLUTION

05.7 CASE STUDY CONCEPT DEVELOPMENT

GATEWAY 36 | BARNSELEY



Planning External Materials Specification		
Ref	Specification	Colour
1	Profiled metal cladding	Olive Green RAL 100 30 20
2	'Mansafy' triple skin glass reinforced polyester (GRP) Roof lights 15% target maximum	
3	Photovoltaic Panels 15% target Note - Rainwater disposal shall be by means a syphonic drainage system of galvanised gutters with downpipe and discharge locations agreed. All gutters shall be installed before assembly and have a factory applied protective finish to external surfaces.	
4	Composite Wall Panel, horizontally laid with preformed corners. Flashings to be pressed metal Plastisol HPS200 coated with preformed corners. Panel thickness to suit M&E U-Value calculations.	Olive Green RAL 100 30 20
5	Composite Wall Panel, horizontally laid with preformed corners. Flashings to be pressed metal Plastisol HPS200 coated with preformed corners. Panel thickness to suit M&E U-Value calculations.	Khaki Green RAL 100 60 20
6	Built up profiled metal cladding (laid horizontally). Flashings to be pressed metal Plastisol HPS200 coated with preformed corners.	Goosewing Grey RAL 050 70 05
7	Feature flat panel rain-screen metal cladding (laid horizontally)	4 no. Colours
	Colour 1	Olive Green RAL 100 30 20
	Colour 2	Sand Brown RAL 100 70 20
	Colour 3	Pale Beige RAL 100 60 10
	Colour 4	Traffic White
8	P.P.C. Aluminum thermally broken anti sun double-glazed doors & screens. Spandrel glazing panels shown hatched.	Goosewing Grey RAL 050 70 05
9	P.P.C. Aluminum thermally broken anti sun double-glazed Windows. Opening windows to be provided where necessary. Spandrel glazing panels shown hatched.	Goosewing Grey RAL 050 70 05
10	Fire exit doors to be High Security doors, vandal-resistant heavy duty steel door sets Plastisol HPS200 coated	Goosewing Grey RAL 050 70 05
13	Parapet flashing to be pressed metal Plastisol HPS200 coated with preformed corners	Goosewing Grey RAL 050 70 05
14	Level access door to be plastisol-coated, insulated, sectional overhead shutters, Size - 3500 x 4,500mm. Panel thickness to suit M&E U-Value calculations.	Khaki Green RAL 100 60 20
15	Dock leveller doors to be plastisol-coated, insulated, sectional overhead shutters, 2,800 x 2,750 mm. Panel thickness to suit M&E U-Value calculations.	Khaki Green RAL 100 60 20
16	Dock Shelter	
18	Stainless steel bollards	
19	Heavy Duty Bollard	



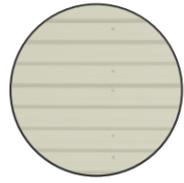
Cladding Panels
Colour: Olive Green



Cladding Panels
Colour: Khaki Green



Cladding Panels
Colour: Sand Green



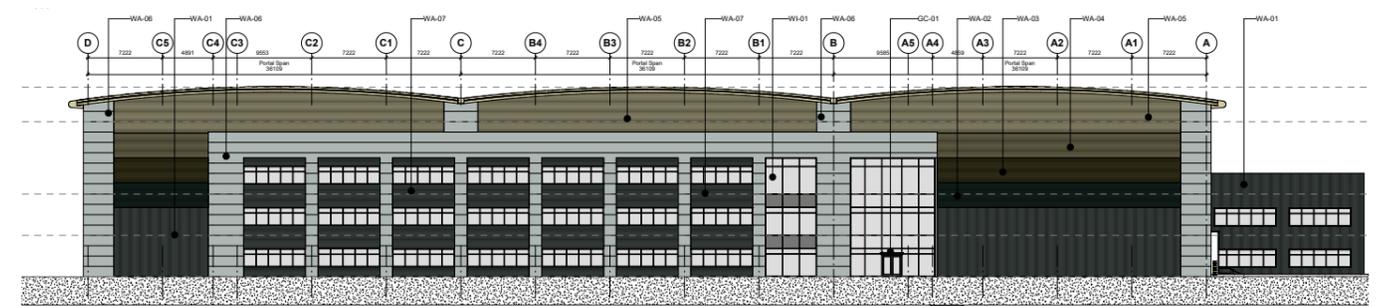
Cladding Panels
Colour: Pale Beige

Gateway 36 is a high-quality industrial and logistics scheme located in Barnsley - immediately adjacent to J36 M1 - where Phase 2 of the development is currently under construction.

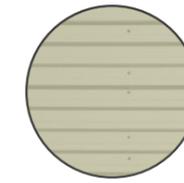
The first three units, comprising 110,000 sq. ft of Grade-A trade, industrial and logistics space, marketed as 'R-Evolution 36' are now complete and available for immediate occupation.

Aesthetically a range of cladding configurations have been combined as illustrated on the above CGI and elevation. The use of green cladding aids in blending the building into the immediate surrounding green landscaping. The combination of pixelated cladding and solid horizontal cladding adds visual interest and creates a more impactful elevation.

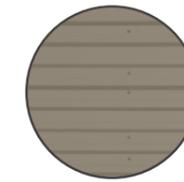
BARNSELEY 340 | BARNSELEY



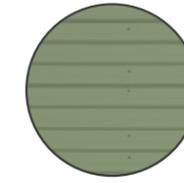
EX-01	Personnel access door Colour: To match adjacent cladding
GC-01	Glazed Canopy
RF-01	Profiled roof cladding Colour: Meadowland RAL 100 80 20
WA-01	Vertically laid Built up profiled wall cladding Colour: Merlin Grey RAL 180 40 05 BS18B25
WA-02	Horizontally laid Built up profiled wall cladding Colour: Ivy RAL 170 20 10
WA-03	Horizontally laid Built up profiled wall cladding Colour: Olive Green RAL 100 30 20
WA-04	Horizontally laid Built up profiled wall cladding Colour: Moorland Green RAL 100 60 20
WA-05	Horizontally laid Built up profiled wall cladding Colour: Meadowland RAL 100 80 20
WA-06	Horizontally laid 'flat' composite cladding system Colour: Pure Grey RAL 000 55 00
WA-07	Horizontally laid Built up profiled wall cladding Colour: Merlin Grey RAL 180 40 05 BS18B25
WA-08	Prowall Concrete
WA-09	Translucent Panel Colour: Grey Tinted
WI-01	Glazing system & framing



Cladding Panels
Colour: Meadowland



Cladding Panels
Colour: Moorland Green



Cladding Panels
Colour: Olive Green

Barnsley 340 sits within the Gateway 36 development, strategically located near to J36 M1.

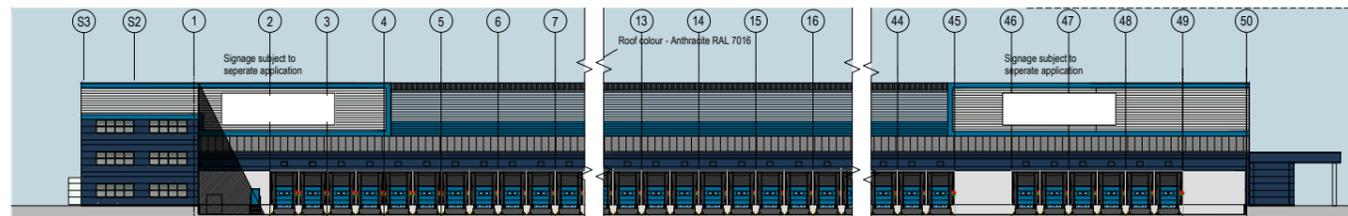
The unit will provide 340,000 sq. ft of Grade-A logistics space in a building targeting BREEAM rating 'Excellent'.

Aesthetically the cladding utilises long stretches of horizontal cladding with breaks instigated by subtle vertical sections on each elevation. The chosen colours are visually effective in terms of blending into the surrounding context, yet also creating a visually emphatic facade.

05 DESIGN EVOLUTION

05.7 CASE STUDY CONCEPT DEVELOPMENT

EQUITES PARK | BARNSELEY



SCHEDULE OF MATERIALS

Office Elevations;
Office cladding, horizontally laid plastic coated metal microrib MR300 cladding (or similar approved):
External finish and colour:

- Solent Blue: RAL 240 40 40
- Sargasso Blue: RAL 5003
- White: RAL 9003

 (Colour options from standard HPS200 Ultra range)

PPC frame glazed entrance doors and windows with grey tinted glazing.

- External colour: Anthracite: RAL 7016

 Lookalike glazing panels where required, indicated darker grey (anthracite film or spandrel panels)

Main Warehouse;
Graded horizontally fitted trapezoidal profiled built up metal cladding system: external finish and colour: HPS 200 ultra:

- Ice Blue (top) RAL 230 80 10
- Solent Blue: RAL 240 40 40
- Clear polycarbonate translucent 2m strip
- Sargasso Blue: RAL 5003
- Roof - Anthracite RAL 7016

 500mm wide x 100mm deep Aluminium feature flashings / framing to wall cladding.

- Solent Blue: RAL 240 40 40

 Precast concrete panel walling to loading dock surround

Insulated composite panel Dock and Level Access doors

- Solent Blue: RAL 240 40 40

 Steel faced fire escape / personnel doors,

- Solent Blue: RAL 240 40 40

- Occupier signage, design and size to be confirmed. Subject to separate Signage Application.

Cladding Panels
Colour: Solent Blue

Cladding Panels
Colour: Sargasso Blue

Cladding Panels
Colour: White

Cladding Panels
Colour: Anthracite

Equites Park is located immediately adjacent to junction 36 of the M1 motorway.

The building is Evri's largest European parcel sorting distribution centre providing 340,000 sq ft of space.

The cladding primarily consists of horizontally laid cladding panels providing stretches of blue and white along the facade of the building.

Aesthetically, the chosen colour palette provides a gradient effect with the sky limiting visual impact.

ARROW POINT | BARNSELEY



SCHEDULE OF MATERIALS

Office Elevations;
Office cladding, horizontally laid plastic coated metal microrib MR300 cladding (or similar approved):
External finish and colour:

- Wedgewood Blue: RAL 220 50 15
- White: RAL 9003
- Feature entrance surround Orange: RAL 2003

 (Colour options from standard HPS200 Ultra range)

PPC frame glazed entrance doors and windows with grey tinted glazing.

- External colour: Anthracite: RAL 7016

 Lookalike glazing panels where required, to be anthracite film or spandrel panels.

Main Warehouse;
Graded horizontally fitted trapezoidal profiled built up metal cladding system: external finish and colour: HPS 200 ultra:

- White: RAL 9003
- Ice Blue: RAL 230 80 10
- Wedgewood Blue: RAL 220 50 15
- Ocean Blue: RAL 230 30 25

 500mm wide x 100mm deep Aluminium feature flashings / framing to wall cladding.

- White: RAL 9003

Roof

- Anthracite RAL 7016

 Precast concrete panel walling to loading dock surround

Insulated composite panel Dock and Level Access doors

- Anthracite RAL 7016

 Steel faced fire escape / personnel doors,

- Anthracite RAL 7016

- Occupier signage, design and size to be confirmed. Subject to separate Signage Application.

Cladding Panels
Colour: Ocean Blue

Cladding Panels
Colour: Wedgewood Blue

Cladding Panels
Colour: Ice Blue

Cladding Panels
Colour: White

Arrow Point is located immediately adjacent to Junction 36 of the M1 Motorway. Arrow Point Barnsley offers direct access into Barnsley, Wakefield and Leeds to the North; and access to Sheffield, and the wider East Midlands in the South.

The cladding configuration consists of horizontally laid panels providing a gradient aesthetic with the sky and surrounding context. The use of shades of blue minimises aesthetic impact by blending the façade of the building in with the surrounding context.

The cladding around the office is characterised by smaller cladding panels and the use of flashing, to enhance visual interest and provide visitors direction when arriving at the building.

05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION

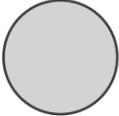
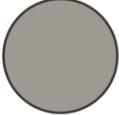


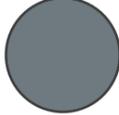
Option A | Elevation A



Option A | Elevation B

Pixelated Horizontal Cladding:

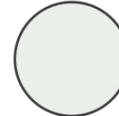
-  **Horizontal Cladding**
Colour: Light Grey
-  **Horizontal Cladding**
Colour: Mid Grey

-  **Horizontal Cladding**
Colour: Dark Grey
-  **Horizontal Cladding**
Colour: Blue Grey

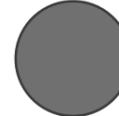
Main Wall Vertical Cladding:

-  **Vertical Cladding**
Colour: Night Grey
-  **Vertical Cladding**
Colour: Dark Grey

Office Composite Panels:

-  **Composite Panels**
Colour: White

Feature Banding and Panelling:

-  **Feature Banding**
Colour: Dark Grey

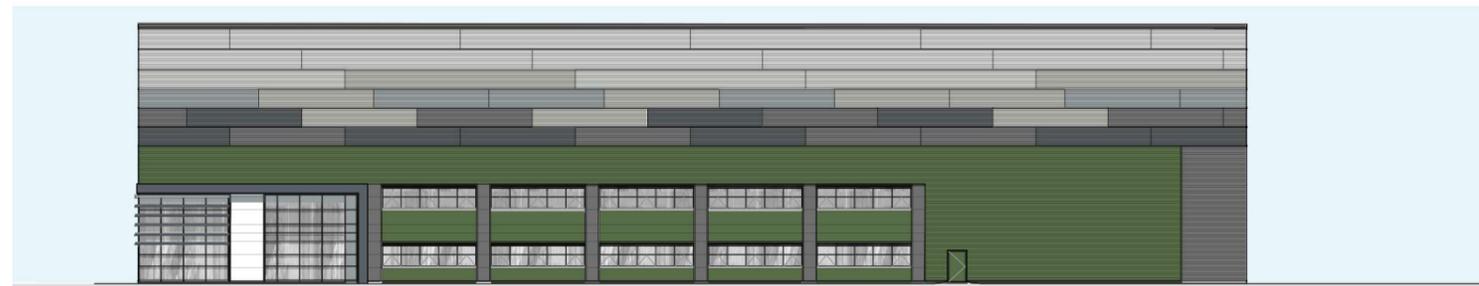


05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION

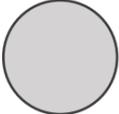


Option B | Elevation A



Option B | Elevation B

Pixelated Horizontal Cladding:

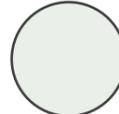
-  **Horizontal Cladding**
Colour: Light Grey
-  **Horizontal Cladding**
Colour: Mid Grey

-  **Horizontal Cladding**
Colour: Dark Grey
-  **Horizontal Cladding**
Colour: Blue Grey

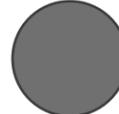
Main Wall Vertical Cladding:

-  **Vertical Cladding**
Colour: Green
-  **Vertical Cladding**
Colour: Dark Grey

Office Composite Panels:

-  **Composite Panels**
Colour: White

Feature Banding and Panelling:

-  **Feature Banding**
Colour: Dark Grey

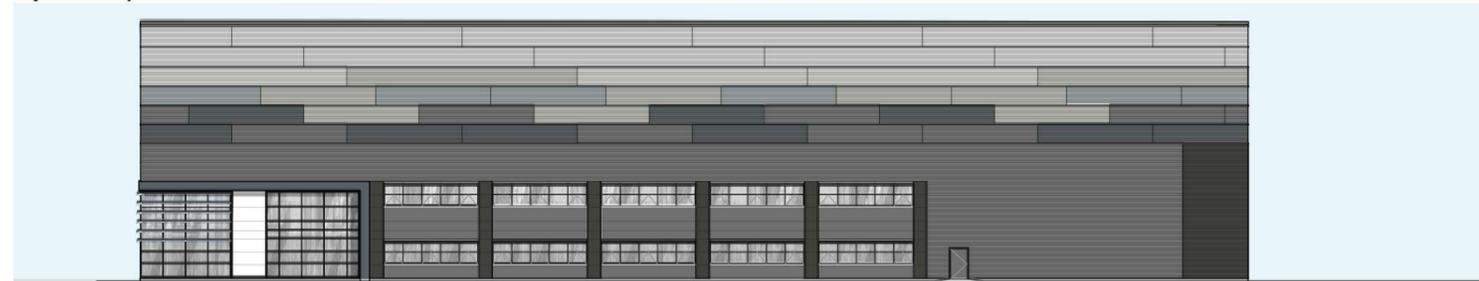


05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION

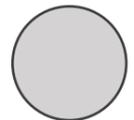


Option C | Elevation A



Option C | Elevation B

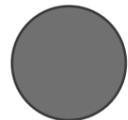
Pixelated Horizontal Cladding:



Horizontal Cladding
Colour: Light Grey



Horizontal Cladding
Colour: Mid Grey



Horizontal Cladding
Colour: Dark Grey

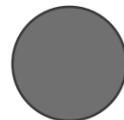


Horizontal Cladding
Colour: Blue Grey

Main Wall Vertical Cladding:

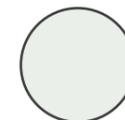


Vertical Cladding
Colour: Night Grey



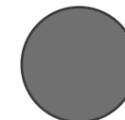
Vertical Cladding
Colour: Dark Grey

Office Composite Panels:



Composite Panels
Colour: White

Feature Banding and Panelling:



Feature Banding
Colour: Dark Grey



05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION



Option B | Elevation A



Option B | Elevation B

Pixelated Horizontal Cladding:

Main Wall Vertical Cladding:

Feature Banding and Panelling:



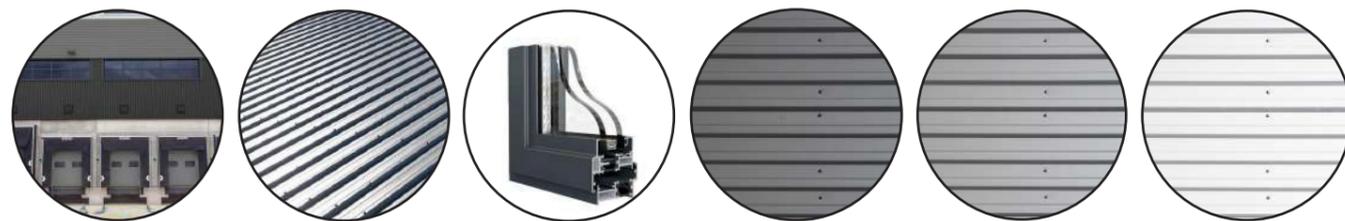
05 DESIGN EVOLUTION

05.8 ELEVATIONAL CONFIGURATION

PLOT 112 | PETERBOROUGH

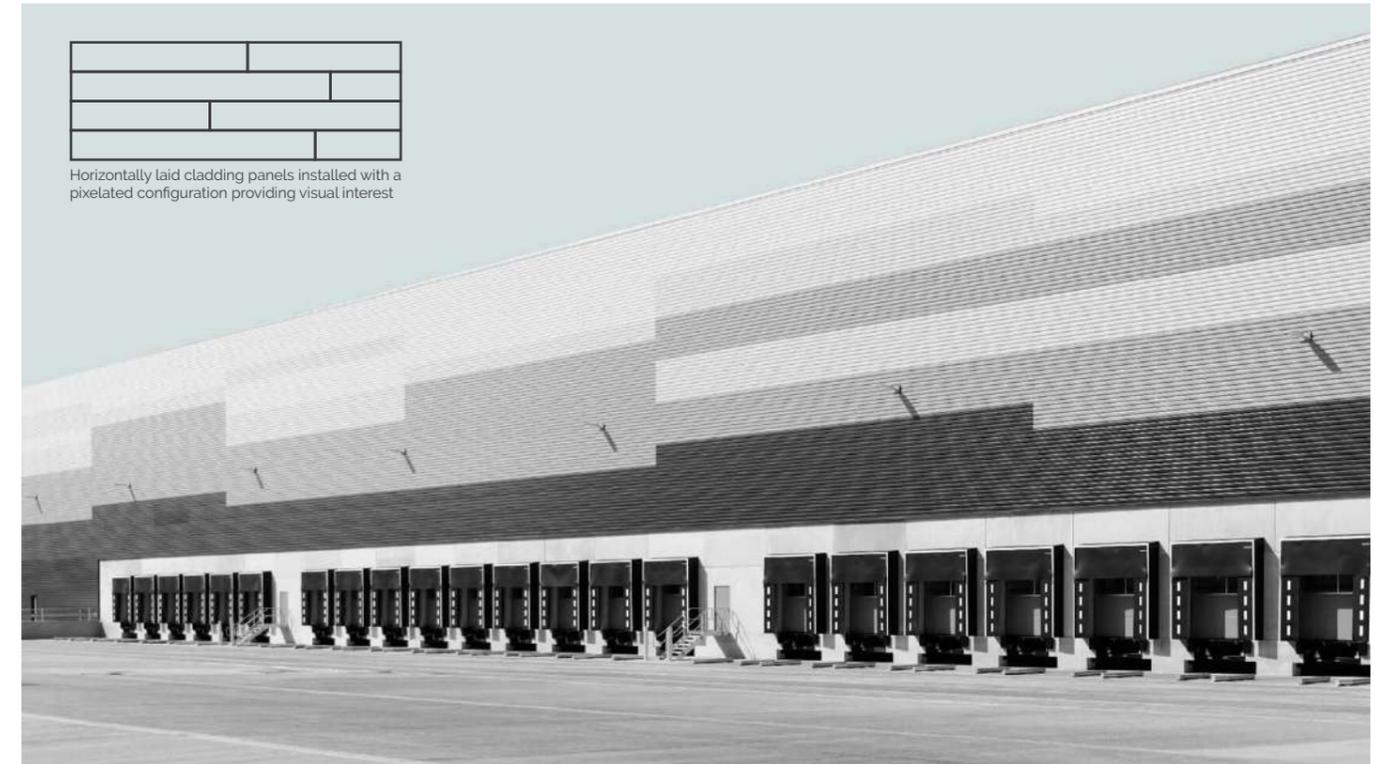


- 1) Vertically laid profiled wall cladding system
- 2) Trapezoidal roof panels
- 3) Anthracite curtain walling, windows & doors
- 4) Horizontally laid micro rib cladding system (Dark Grey)
- 5) Horizontally laid micro rib cladding system (Grey)
- 6) Horizontally laid micro rib cladding system (White)

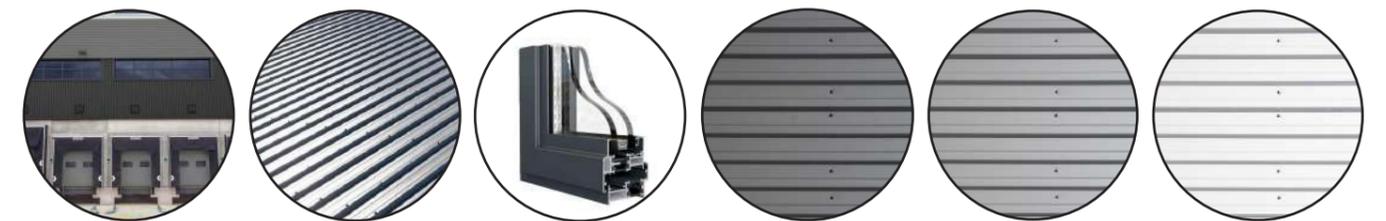


- 1) Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.
- 2) Differing cladding materials to add visual interest and break up the visual building massing.
- 3) Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.
- 4) The use of darker grey tones help anchor the facades back into the landscape whilst delivering visual interest and break up the overall mass of the building.
- 5) Differing cladding materials to add visual interest and break up the visual building massing.
- 6) Upper levels of the façade clad in white horizontal cladding to provide a gradient with the sky

DSV LOGISTICS | MERCIA PARK



- 1) Vertically laid profiled wall cladding system
- 2) Trapezoidal roof panels
- 3) Anthracite curtain walling, windows & doors
- 4) Horizontally laid micro rib cladding system (Dark Grey)
- 5) Horizontally laid micro rib cladding system (Grey)
- 6) Horizontally laid micro rib cladding system (White)



- 1) Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.
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- 5) Differing cladding materials to add visual interest and break up the visual building massing.
- 6) Upper levels of the façade clad in white horizontal cladding to provide a gradient with the sky

05 DESIGN EVOLUTION

05.9 MASTERPLAN COMPLIANCE ASSESSMENT

Pre-application advice was provided by Barnsley Metropolitan Borough Council outlining key points to consider prior to the application.

Comments Provided	Proposed Response
Urban Design, Height and LVIA	
<p>When the Goldthorpe Masterplan was adopted, the Council nonetheless recognised that the site would likely appeal to occupiers seeking larger units. A pragmatic decision was therefore taken to tolerate buildings with heights of up to 18m. However, the location, scale and massing of these buildings will invariably need careful consideration and mitigation in the form of substantial screening will be necessary to ensure that landscape and visual impact is minimised. This is because, for the reasons articulated above, we are not prepared to tolerate the same level of visual and landscape harm that we allowed at Hoyland West.</p> <p>Urban design comments have been received and our officer is keen to know what the development will look like from the A635 Barnsley Road, which given the volume of traffic will clearly be an 'everyday' view of the development for a lot of people.</p> <p>It will also be important to see what the development will look like from the edges of existing housing and the new housing development sites (HS44 to the east and HS51 to the south).</p>	<p>Each of the units will be designed in accordance with the Goldthorpe Masterplan Framework Document and in line with the comments received from the Local Planning Authority. The Goldthorpe Masterplan Framework Document states that 'buildings should not exceed 15m to the highest point where the footprint is 20,000sqm or less and should not exceed 18m to the highest point where footprints are over 20,000sqm.' Each of the proposed buildings within the development will be designed to provide a maximum ridge height of 18m.</p> <p>A photographic study will be undertaken which will investigate the key views of the site and determine the overall impact to the surrounding context.</p>
Economic Considerations	
<p>Enterprising Barnsley actively support this application and they considered that the development will help to create much needed, new appropriately designed modern industrial units that the market requires (re height), in a strategically important area of the borough, that will help to both attract future inward investors and retain growing indigenous local companies. It is considered that this development is essential in helping BMBC and SYMCA to create future employment opportunities (jobs) as per the existing strategic economic and corporate plans and helping to provide a future delivery of jobs at all levels for local residents to access.</p> <p>It is argued that without such appropriately sized (height) buildings, there is a real possibility that BMBC could approve a scheme that is unfit for occupier market and could result in economic malaise, as opposed to being a clear economic stimulant.</p>	<p>The proposed units will provide suitable space for the occupiers providing employment opportunities for the local residents to access.</p>

Comments Provided	Proposed Response
Highways	
<p>Access to the proposed housing allocation HS51 requires access through the south-east section of this site. This is to ensure that Lacewood Primary School is not detrimentally affected by any future development in terms of highway safety. This access should be provided as part of an early phase of the development and the layout should ensure no ransom strip is retained.</p> <p>What impact the additional traffic will have through the village of Hickleton in Doncaster given that a substantial proportion of the anticipated HGV movements will be to and from junction 37 of the A1M.</p>	<p>Access to the proposed housing allocation HS51 has been provided to the South-East side of the site.</p> <p>The cover letter submitted by JEH Planning states <i>'from a highway perspective, it is our view that the A635 and the local highway network in general is suitable for both existing and future traffic generation of the scale anticipated from the development without any off-site highway mitigation measures being required.'</i></p> <p>The main access to the site will utilise the newly proposed roundabout access point from the A635.</p>
Air Quality	
<p>The AQMA was declared because existing nitrogen dioxide levels through Hickleton are already substantially elevated. It is therefore expected that mitigation will be necessary in order to fully offset the air quality impact from the traffic associated with the ES10 site.</p>	<p>Options for possible mitigation have already been discussed with Doncaster and these are limited due to the close proximity of a number of dwellings in Hickleton to the carriageway. Conversely, a mitigation scheme will be agreed.</p>
Conservation	
<p>ES10 includes the eastern edge of the (former) RSV1 and the southern two thirds of the (former) D1. Those sites were assessed for their likely heritage impact in terms of setting and direct impacts (archaeology).</p>	<p>The northern most part of D1 is over 800m to the southeast of the Conservation Area and this distance allied with only limited long distance views results in a limited contribution to the setting and low potential for harm.</p>

05 DESIGN EVOLUTION

05.9 MASTERPLAN COMPLIANCE ASSESSMENT

Comments Provided	Proposed Response
Biodiversity	
<p>Due to the proximity of the site to the SSSI there is potential for adverse impacts on the SSSI, in the absence of mitigation.</p> <p>Due to the proximity of the proposal to the Dearne Valley Wetlands SSSI, development of the site may result in hydrological impacts which may also indirectly affect Bolton upon Dearne Wetlands LWS.</p> <p>The development of the site may negatively impact species associated with the Dearne Valley Wetlands SSSI and other notable species, including marsh harrier.</p> <p>As the proposal site is a masterplan framework site (ES10 – Land South of Dearne Valley Parkway) a minimum 10% Biodiversity Net Gain is required.</p>	<p>The proposed scheme will incorporate a landscape framework comprising of the conservation of existing hedgerows and trees where possible, reinforced by significant new trees, hedgerow and shrub planting.</p> <p>The proposal will include the formation of new earthworks and mounding to the main site's perimeter which will include native planting species.</p> <p>Largely native trees and shrubs would be used to reflect those in existing locality. A mix of planting sizes and densities would be adopted to create different habitats.</p> <p>The proposal will look to retain the site's ecological value and enhance the site providing a biodiversity net gain of at least 10% as required within the Goldthorpe Masterplan Framework Document.</p>
Trees	
<p>The need for a tree survey to BS59837:2012, and an Arboricultural Impact Assessment.</p>	<p>The scheme will look to protect as many of the existing trees as reasonably possible.</p> <p>The Arboricultural Impact Assessment will identify tree protection measures necessary and any relevant construction techniques which may be required to facilitate the retention of trees within the site.</p>
South Yorkshire Mining Advisory Service	
<p>The site encompasses Coal Authority 'High Risk' referral areas due to the presence of former opencast coal operations, shallow coal (Shafton seam – where not removed via the opencasts) and two old mine shafts/bell pits (very likely to have been removed however by the opencast workings).</p>	<p>A Coal Mining Risk Assessment will be undertaken which considers the full implications of the mining legacy position.</p>

Comments Provided	Proposed Response
Drainage Strategy and Flood Compensation	
<p>The Masterplan document states that built development should be avoided within Flood Zones 2 and 3. The pre-app contradicts this in that Plot 1 proposes an extended development platform in the Flood Zone 3 to the north.</p> <p>There should be no increase in surface water runoff from the new development.</p> <p>Any balancing facility should be designed to accommodate a 1 in 30-year flow from the site below ground and a 1 in 100-year flow retained within the site (including an allowance of 30% for climate change), without causing any flooding to buildings.</p>	<p>The proposed scheme will aim to minimise the effect it has on the flood zone area. Balancing facilities will be designed to accommodate a 1 in 30-year flow from the site below ground and a 1 in a 100-year flow retained within the site (including an allowance of 30% for climate change), without causing any flooding to buildings.</p> <p>The scheme will utilise Sustainable Urban Drainage (SUDS) techniques to help tackle surface water run-off problems at source using features such as soakaways, permeable pavements, grassed swales, infiltration trenches, ponds and wetlands to attenuate flood peak flows, produce water quality improvements and environmental enhancements.</p> <p>A flood risk assessment in accordance with NPPF will be produced.</p>
Pollution Control	
<p>Noise from road traffic through Hickleton is an issue as increased HGVs, particularly throughout the night, will be a further cause for concern.</p>	<p>A Noise Impact Assessment will be undertaken.</p> <p>A Framework CEMP will undertaken.</p>
Public Rights of Way	
<p>Concerns over the diversion to PROW.</p>	<p>The existing PROW footpath is in poor condition and not suitable for a variety of users. The proposed scheme would upgrade the PROW to provide 2 metre wide access for pedestrians on a safer surface. The additional proposed pedestrian routes to site would encourage sustainable transport.</p>

05 DESIGN EVOLUTION

5.10 DESIGN PANEL REVIEW

The submission of the application follows extensive engagement which emphasises the Applicant's commitment to developing the most suitable scheme for the site. Below are comments provided by Barnsley's Design Review Panel (DRP), and the proposed response.

Comments Provided	Proposed Response
Height	
<p>The Panel could see both points of view in terms of the Council holding their stance on the 18m maximum height set out in the Masterplan Framework document and Newlands seeking an increased height parameter in response to market demand for larger buildings.</p> <p>The Chair, Tom Lonsdale, criticised the 18m restriction as being 'unscientific' and not predicated on any market rationale. It was highlighted that deviations from the Masterplan could be justified through a planning application and this would be achieved through the submission of market evidence.</p>	<p>The building will be designed to provide a 15m clear internal height and a ridge height of 18m in line with the Goldthorpe Masterplan Framework Document.</p> <p>Photomontage analyses will provide evidence of key view points and the impact the proposed height will have on the surrounding context.</p> <p>A market analysis is being undertaken considering the rationale of the proposed building height.</p>
Colour	
<p>The Masterplan Framework refers to grey and blue cladding on Page 38.</p> <p>Elizabeth Motley queried whether grey is the right colour for the buildings as 'there isn't a lot of grey in nature'. She suggested that other neutral colours should be considered and went on to suggest looking at cream.</p> <p>Paul Testa understood the rationale behind the proposed cladding treatment in reducing the perceived massing but noted that the gradient may only be effective where the light banding at the top is seen against the sky rather than other viewpoints where it's seen against the landscape.</p>	<p>The proposed colour palette will be thoroughly considered to minimise the visual impact on the surrounding context. A range of external materials and colour palettes will enhance building elevations, and to soften the appearance and break up the visual proportions of the larger building elevations.</p>
Distinctiveness	
<p>Graham Roberts suggested the buildings require more distinctiveness rather than being 'the same as all the rest'. Reference was made to the trend of Mill buildings getting larger through time and some good examples of architecture that came out of this (Temple Mill (see attached) was referenced by Elizabeth Motley as well as reference to developing a factory shop for returns). A further exploration of architecture was called for by Tom Lonsdale away from the current rectilinear form.</p>	<p>This would be considered in a future Reserved Matters Application. The use of an amended cladding configuration provides distinctiveness to the facades and increases the overall aesthetic appeal of each of the proposed buildings.</p>
Roof Treatment	
<p>Elizabeth Motley suggested that the warehouses could have green roofs or some sort of pattern to break it up. The Panel queried whether the roof could be seen from any viewpoints and FPCR confirmed that it wouldn't.</p>	<p>This would be considered in a future Reserved Matters Application.</p>

Comments Provided	Proposed Response
Renewables	
<p>Graham Roberts wanted to see solar and wind incorporated into the development. He noted that it was currently 'missing a trick' by not including a decentralised energy network and also alluded to the use of ground source heat pumps.</p>	<p>This will be addressed through the Sustainability Statement submitted as part of the Hybrid application with PV included as part of Newlands' basebuild spec. Stantec to advise on feasibility of ground source heat pumps.</p>
Connectivity & Public Accessibility	
<p>Graham Roberts queried whether public connectivity would extend through the Green Blue Corridor down to the Old Moor RSPB area. Allen Parks queried whether the Flood Compensation areas would be fenced off or publicly accessible.</p> <p>There were also comments about user conflict between HGVs and pedestrians so crossing points to be considered.</p>	<p>The proposed site access strategy aims to provide safe access across the site for both vehicles and pedestrians. HGV yard access to each of the units has been segregated from the car park access to avoid congestion. Across the site, pedestrian crossing points have been provided alongside 2m wide and well-lit footpaths to ensure safe pedestrian access.</p>
Earthworks/Bunding/Cut & Fill	
<p>Tom Lonsdale noted that without further detail on the three-dimensional representation of the site, topography and earthworks strategy, it was difficult for the panel to pass more detailed comment as the section where considered to be too conceptual/diagrammatic.</p>	<p>Please refer to Structural Engineer and Civil Engineer drawings.</p>
Air Quality	
<p>Elizabeth Motley made reference to the air quality impacts of having the parking area in the south-east corner adjacent to the School.</p>	<p>The proposed building to the South-East corner of the site has been orientated to provide parking away from the South-East boundary. Between the school and the rear of the building there is a proposed green buffer zone which will mitigate some of the concerns raised regarding air quality.</p>
Flood Compensation	
<p>There appeared to be a lack of knowledge on flood compensation storage and how this is a perfectly acceptable solution to developing within the flood zone.</p>	<p>Please refer to FRA Statement.</p>

05 DESIGN EVOLUTION

5.11 ECOLOGICAL DESIGN EVOLUTION

Local Plan (2019)

LPA Requirements

- Include the creation of a habitat corridor (at least 8m in width) along Carr Dyke and a sustainable drainage scheme to ensure that rainwater falling on the site is still able to drain into the Dyke aiming to improve water quality.
- New roundabout on to A635.
- Safeguarded Southern access to the housing site.
- Protect and enhance biodiversity value on the nearby Old Moor RSPB reserve and ensure the development avoids impacts or incorporates effective mitigation measures
- Safeguard the setting of the Bllingley Conservation Area; Give consideration to Carr Dyke and the connecting unnamed ordinary watercourse.
- Retain the existing woodland and hedgerows in the north-west corner.
- Prevent development within the flood zone.

Our Response

- Maximise developed area.
- Implementation of green buffer zones
- Utilisation of new roundabout on to A635 as site access point.
- Realignment of Carr Dyke to maximise development.
- Inclusion of level plateaus.
- Careful consideration of the flood zone including the implementation of appropriate flood mitigation measures.

Parameters



- ① New Roundabout on to A635
- ② Retained Hedgerows
- ③ Minimum 8m Ecology Corridor
- ④ Retained Site Vegetation
- ⑤ No Development within the Flood Zone
- ⑥ Safeguarded Southern Access to Housing Site

Concept Development Plan



05 DESIGN EVOLUTION

5.11 ECOLOGICAL DESIGN EVOLUTION

Masterplan Framework (2021)

LPA Requirements

- Sensitive re-profiling and diversions may be allowable.
- Provision of 8m ecology corridor along Carr Dyke.
- 10% Biodiversity Net Gain to be delivered on or off-site.
- Ensure the development maintains ecological connectivity.
- Retention of ecological value may not be feasible in certain instances.

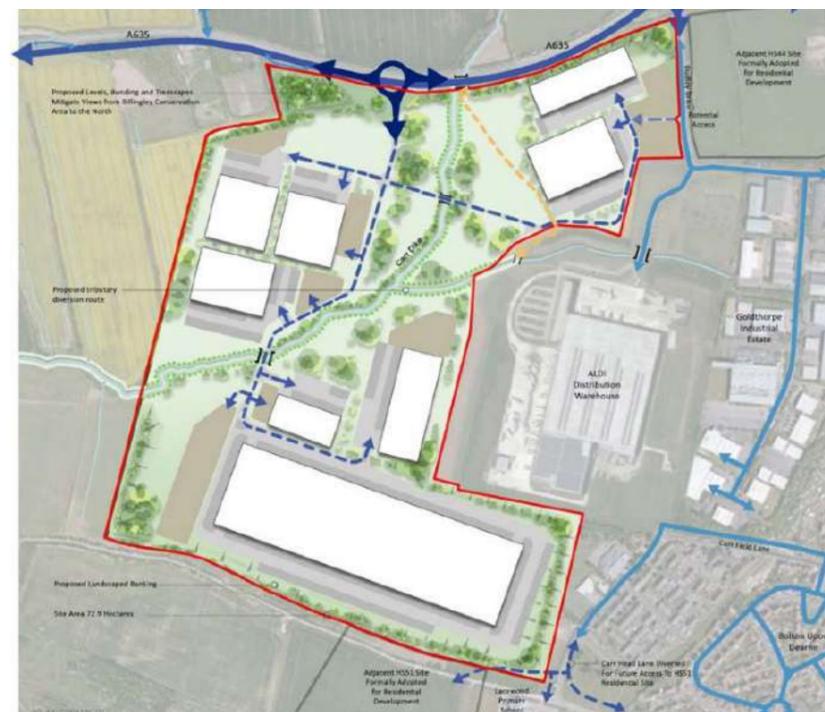
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- Maximise developed area.
- Implementation of green buffer zones
- Utilisation of new roundabout on to A635 as site access point.
- Inclusion of level plateaus.
- Careful consideration of the flood zone including the implementation of appropriate flood mitigation measures.

Goldthorpe Masterplan Framework - Site Opportunities



Goldthorpe Masterplan Framework - Illustrative Masterplan



05 DESIGN EVOLUTION

5.11 ECOLOGICAL DESIGN EVOLUTION

Pre-application (2022/23)

LPA Requirements

Masterplan Compliance Assessment (Pre-application)

Biodiversity

- Potential for adverse impacts on the SSSI, in the absence of mitigation.
- Development of the site may result in hydrological impacts which may also indirectly affect Bolton upon Dearne Wetlands LWS.
- A minimum 10% Biodiversity Net Gain is required.

Trees

- The need for a tree survey and an Arboricultural Impact Assessment.

Drainage Strategy and Flood Compensation

- Built development should be avoided within Flood Zones 2 and 3.
- There should be no increase in surface water runoff from the new development.
- Balancing facilities should be designed to accommodate a 1 in 30-year flow from the site below ground and a 1 in 100-year flow retained within the site without causing any flooding to buildings.

Our Response

Biodiversity

- The incorporation of a landscape framework comprising of the conservation of existing hedgerows and trees.
- Formation of new earthworks and mounding to the main site's perimeter.
- Native trees and shrubs used to reflect those in existing locality.
- Enhance the site providing a BNG of at least 10%.
- Development density reduced

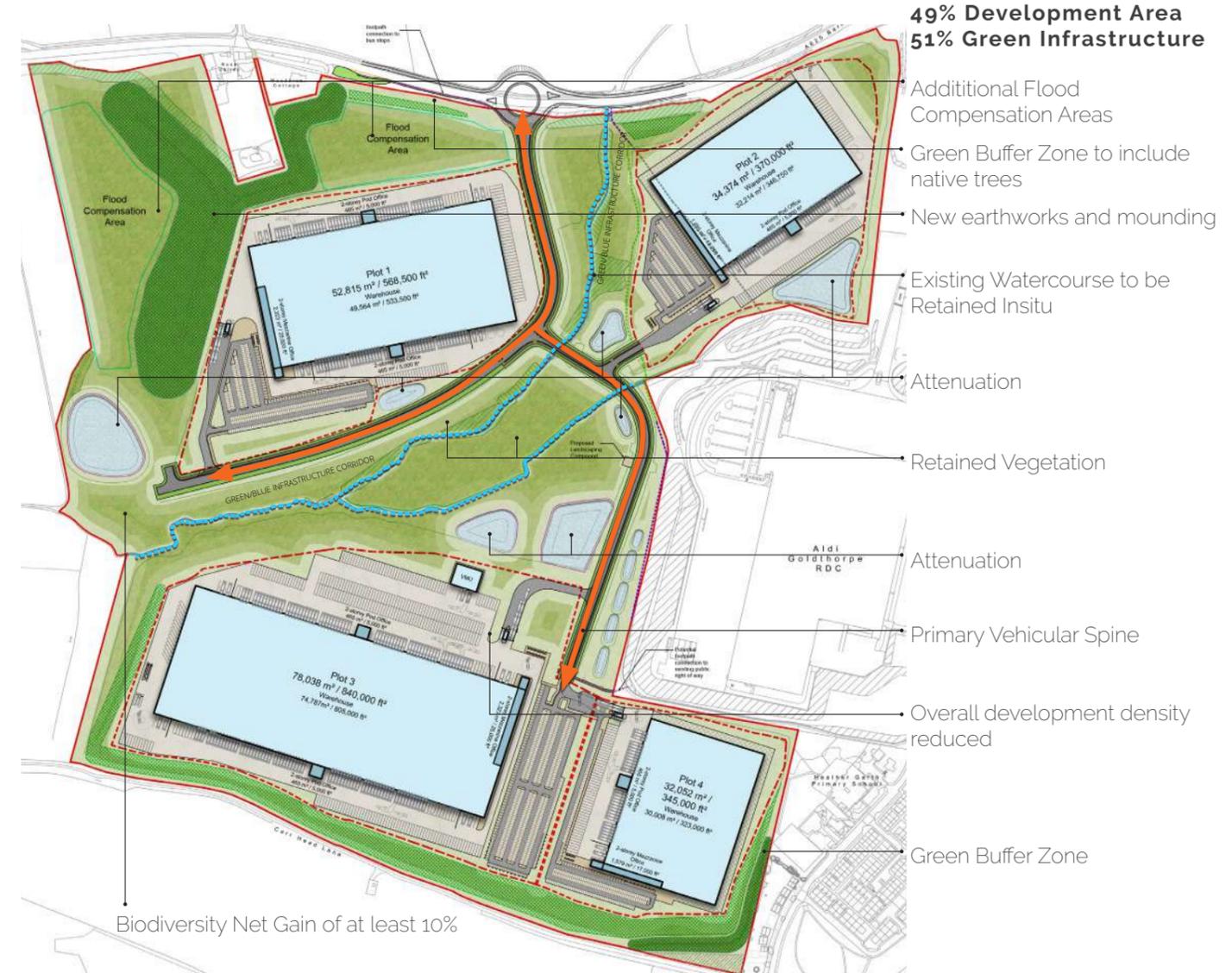
Trees

- Protection of existing trees
- Arboricultural Impact Assessment to identify tree protection measures.

Drainage Strategy and Flood Compensation

- Incorporation of balancing facilities designed to accommodate a 1 in 30-year flow from the site below ground and a 1 in a 100-year flow retained within the site.
- Additional flood compensation areas introduced to the West and North of the site to provide sufficient capacities.
- Carr Dyke retained and not diverted.

2022/23 Masterplan



05 DESIGN EVOLUTION

5.11 ECOLOGICAL DESIGN EVOLUTION

Submission (Masterplan Compliance) (2023)

LPA Requirements

Goldthorpe Masterplan Framework

- Siting | 4 development platforms.
- Landscape | Retain existing woodland and hedgerows on site periphery and hedgrow in the north-west corner of the site.
- Green Infrastructure | Retention of Carr Dyke and creation of habitat corridor (at least 8m in width).
- Biodiversity Net Gain | Achieve at least 10% BNG.

National Design Guide (Key Considerations)

- Context
- Built Form
- Nature
- Public Spaces

Our Response

Goldthorpe Masterplan Framework

- 4 development plots created on 3 plateau areas.
- Existing vegetation to be protected where possible.
- Carr Dyke retained and deliver a corridor width of between 50-80m.
- Achieve a minimum 10% BNG through on and off-site provision.

National Design Guide

- Context | Consideration of levels, building form, materiality, and ecology to minimise the impact on both the local and wider context.
- Built Form | 18m ridge height. Suitable building type for proposed location, being in close proximity to the existing Goldthorpe Industrial Estate.
- Nature | Existing watercourse to be managed appropriately. Implementation of SUDS, balancing ponds, attenuation, and additional tree belts.
- Public Spaces | Newly proposed entrance from A635 to provide safer access for pedestrians including lighting, crossing points, wide roads and paths.

2023 Masterplan



49% Development Area
51% Green Infrastructure

05 DESIGN EVOLUTION

5.11 ECOLOGICAL DESIGN EVOLUTION

Post-Submission (2024)

LPA Requirements

Yorkshire Wildlife Trust:

- Provision of mitigation for ground nesting birds, willow tits,

BMBC Forestry Officer:

- Retain T15

BMBC Ecology Officer:

- Provide mammal ledges
- Avoid artificial bank enhancements
- Utilise stone headwalls
- Provision of mitigation for marsh harriers and breeding/wintering birds

RSPB:

- Provision of mitigation for willow tit, marsh harrier and wintering birds

EA:

- Compensation of flood within Site

BMBC PROW Officer:

- Amend levels adjacent to plot 2
- Provide level access cycle route

Our Response

- Reduction in proposed woodland and scrub providing more open grassland habitat in west promoting use by birds (inc. Marsh Harrier)
- Reduction in proposed woodland and scrub along the south-west Carr Dike corridor – opening up known areas of marsh harrier activity.
- Provide scrapes within flood attenuation area promoting a diverse wet grassland structure suitable for a range of birds (inc. marsh harrier), small mammals, and amphibians.
- Flood attenuation basins managed as damp tussocky grassland; diverse structure, high quality habitat for a range of bird species including farmland birds and ground nesting species.
- Manage woodland areas to promote willow tit, creating diverse woodland structure with introduced dead wood for foraging and nesting.
- Manage edges of the flood attenuation basins providing small seed bearing plants for additional foraging for farmland and overwintering birds.
- Provide supplementary feeding for wintering birds in the west and north flood basins using seed and feeders where appropriate.
- Provide fruit bearing trees through the development for winter bird foraging (inc. some farmland birds e.g. redwing, fieldfare, starling)
- Provision of a range of bird nest boxes across the development appropriate for farmland birds located appropriately to avoid willow tit habitats.

- Provision of mammal ledges or mammal underpasses on watercourse crossings ensuring connectivity for otters, badgers and small mammals.
- Production of a development wide Management Plan to provide details of management actions to ensure habitats are established and meet requirements.
- Ongoing management and monitoring of habitats will be secured through an appropriate legal agreement and will have a minimum term of 30 years as per BNG guidance.

Landscaping Plan



05 DESIGN EVOLUTION

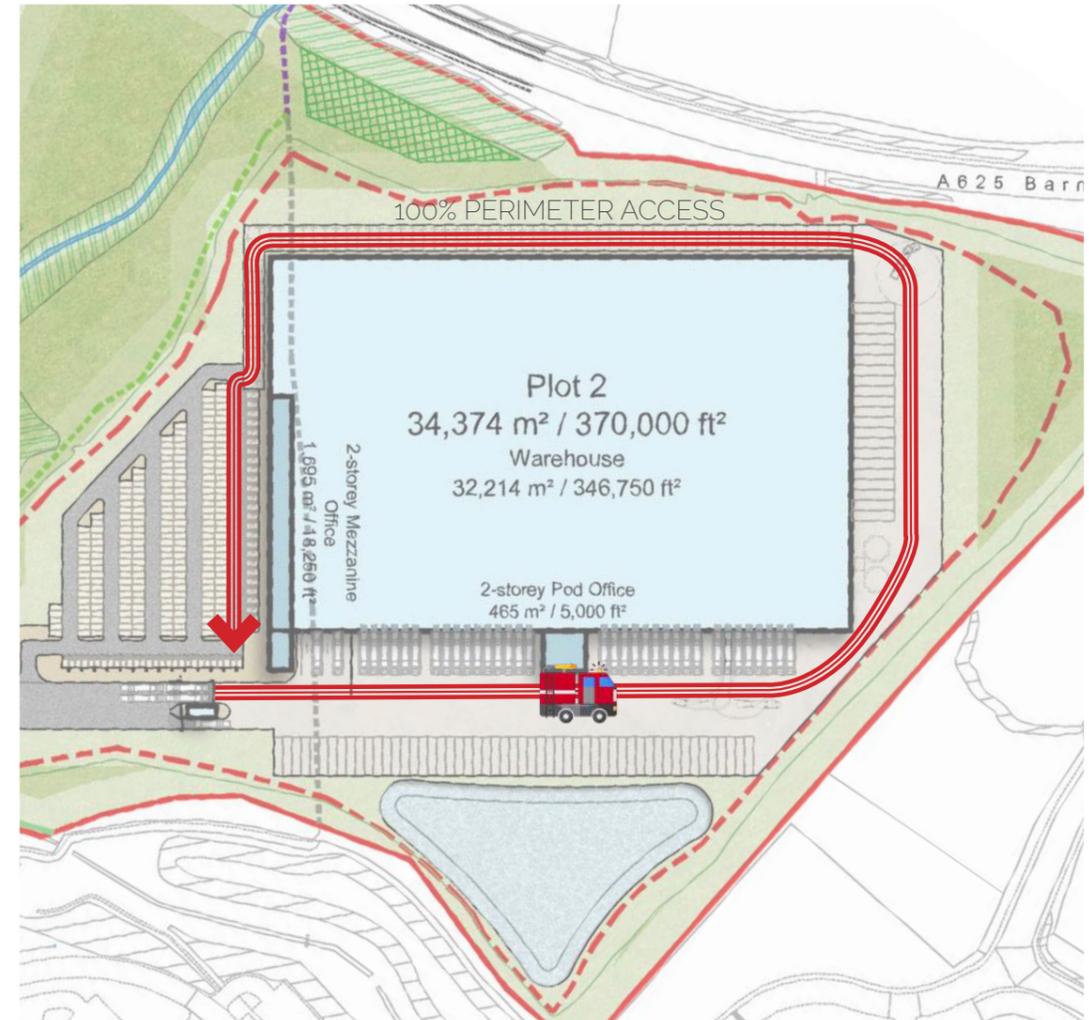
05.12 FIRE STATEMENT

The proposed buildings within the development will be designed in accordance with Approved Document Part B. Table 15.1 of Approved Document Part B makes reference to fire and rescue vehicle access to buildings. Based on the buildings indicated on the Illustrative Masterplan exceeding 24,000m², each building will be required to provide access to fire vehicles around 100% of the perimeter of the building. This will be subject to further development within a Reserved Matters application.

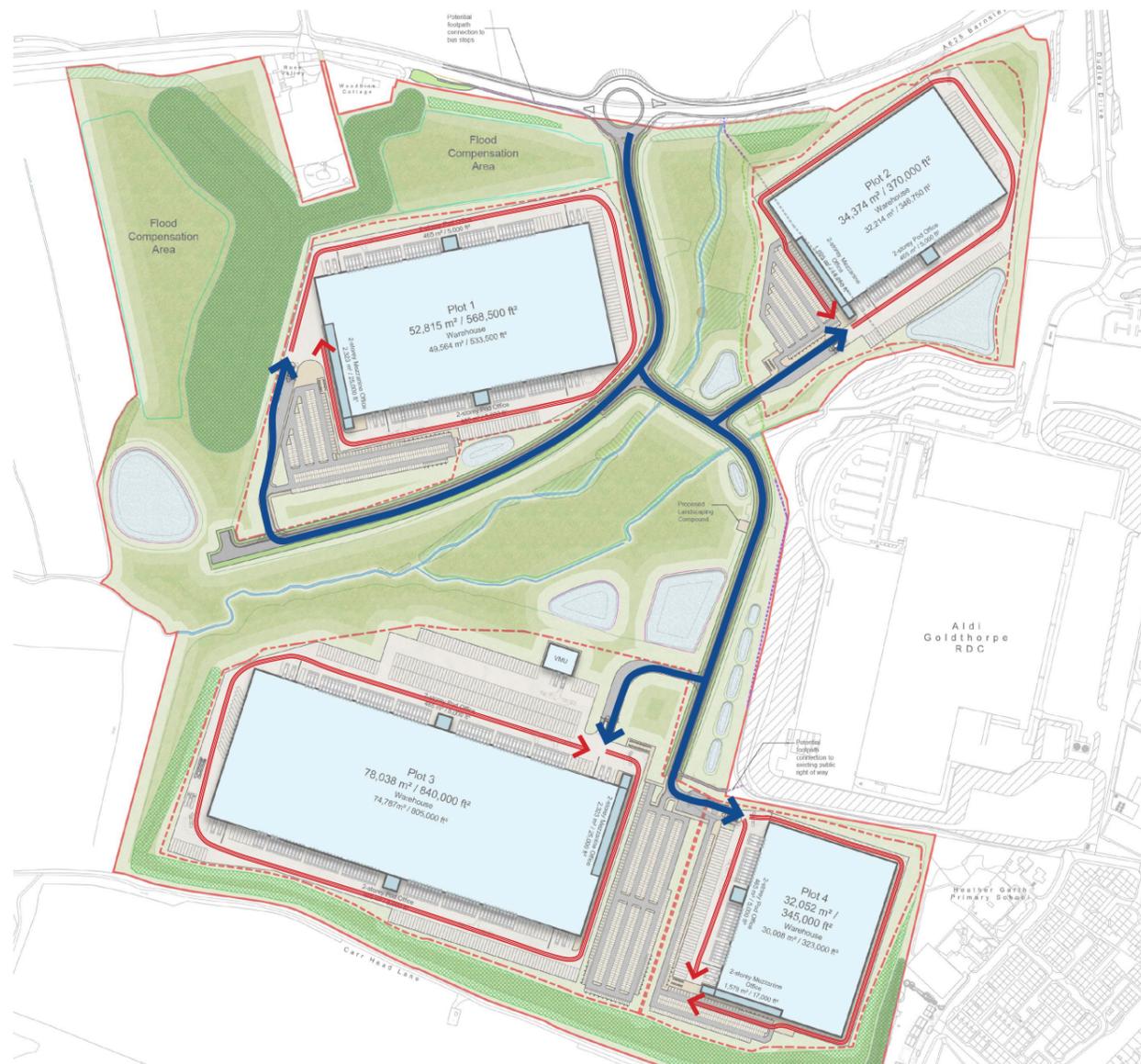
Each of the buildings have been provided with 100% perimeter access, alongside the provision of adequate road widths and turning circle distances in accordance with Table 15.2 of Approved Document Part B.

Future Reserved Matters Applications will provide evidence of the scheme's compliance with Approved Document Part B.

The site wide fire strategy will also provide adequate access to fire hydrants. As stated in Approved Document Part B, buildings not fitted with fire mains, hydrants should be within 90m of an entrance to the building.



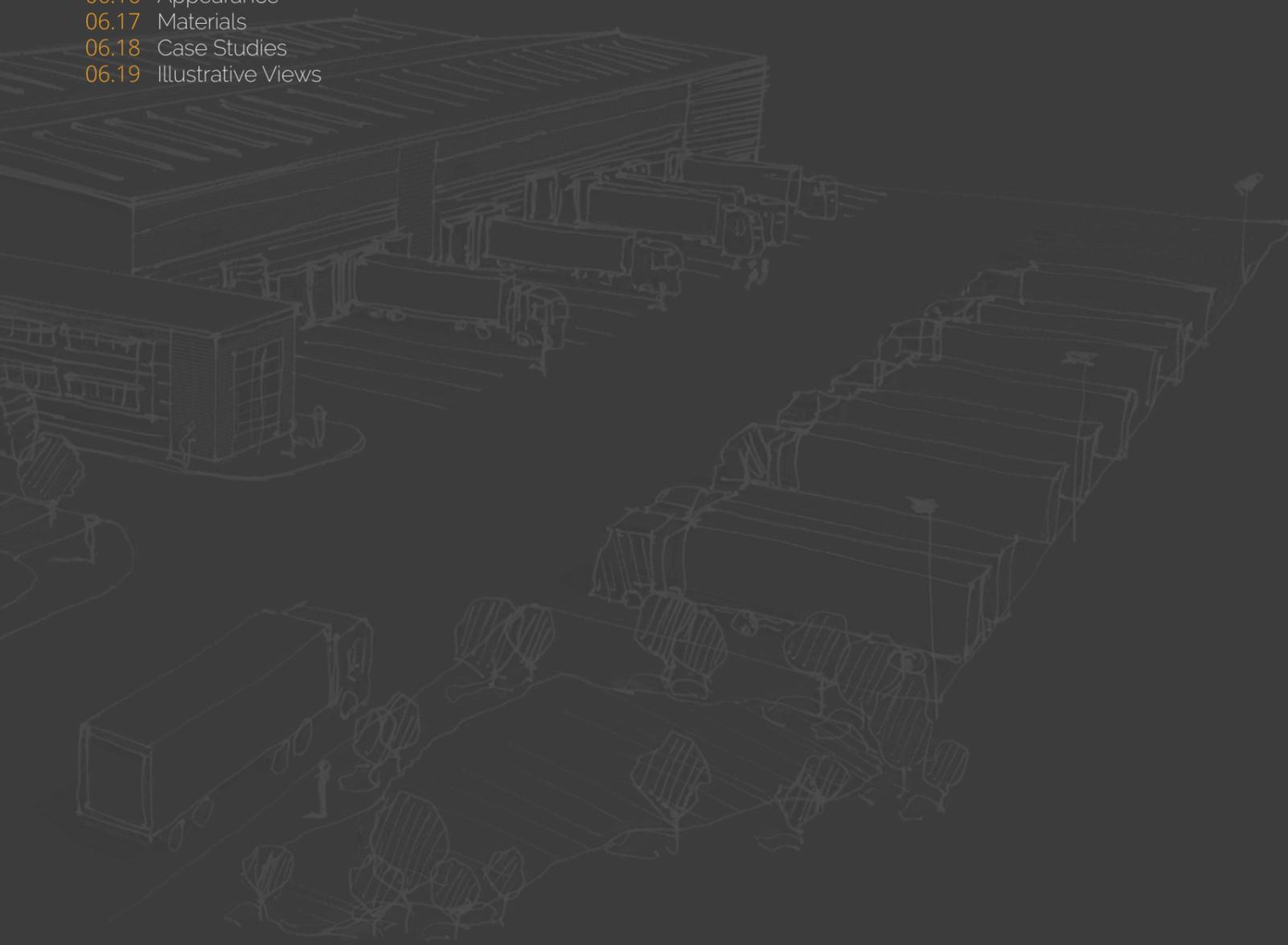
100% Perimeter Fire Vehicle Access



Site Fire Vehicle Access Strategy

06 DEVELOPMENT PROPOSALS

- 06.1 Green Infrastructure
- 06.2 Green Infrastructure Proposal
- 06.3 Blue Infrastructure
- 06.4 Blue Infrastructure Proposal
- 06.5 Connectivity
- 06.6 Connectivity Proposal
- 06.7 Illustrative Masterplan
- 06.8 Goldthorpe Masterplan Framework Compliance
- 06.9 National Design Guide Compliance
- 06.10 Proposed Plateau Levels
- 06.11 Plot Design
- 06.12 Plot Hard Landscaping
- 06.13 Proposed Culverts
- 06.14 Scale and Massing
- 06.15 Site Sections
- 06.16 Appearance
- 06.17 Materials
- 06.18 Case Studies
- 06.19 Illustrative Views



06 DEVELOPMENT PROPOSALS

06.1 GREEN INFRASTRUCTURE

The site presents opportunities to connect into the wider Green Infrastructure network beyond the site boundaries. A specialist will undertake strategic landscaping work to provide a more developed outcome to the overall landscaping design.



Existing Trees and Hedges

The application site lies in an area of low lying existing agricultural land consisting of fields with perimeter hedgerows.



Green Infrastructure Strategy

Appropriate green buffer zones will be provided along the Northern, Southern, and Eastern boundary in order to reduce visual impact as well as noise and light impacts to the surrounding context.

The introduction of attenuation ponds located within the site provide additional wildlife habitats increasing the biodiversity value of the area. These areas seek to link into the existing green infrastructure that can currently be found within and around the site.



Sketch Proposal

The sketch proposal shows a layout which responds to the site's topography and provides a structural landscape strategy.

The peripheral landscape treatment aims to define the site boundary whilst also integrating the site into the wider countryside.

 Illustrative Existing Trees

06 DEVELOPMENT PROPOSALS



Green Infrastructure Proposal

06.2 GREEN INFRASTRUCTURE PROPOSAL

The green infrastructure of the proposed masterplan offers a high quality landscaped setting to the employment site.

- Provides a sense of place through a structured and planned landscaping strategy.
- The proposals seek to establish the landscape and visual baseline in terms of its existing character and its sensitivity to the development proposals, whilst mitigating visual impact to wider surroundings
- Naturally mitigates noise and light pollution by providing appropriate natural buffer zones.
- The proposals increase the biodiversity value of the site.

Overall the proposed green infrastructure will be an important feature of the development, creating a sense of place throughout the development and integrating the proposal into the existing surrounding landscape whilst providing benefits for visitors and employees of the site including landscape and wildlife value, sustainable drainage, cycling and walking routes.

06 DEVELOPMENT PROPOSALS

06.3 BLUE INFRASTRUCTURE

The blue Infrastructure network refers to the network of such features and bodies of water across the proposed site. These consist of the existing Carr Dike which runs across the site.



Existing Blue Infrastructure

There is an existing watercourse (Carr Dike) which enters the site from the Northern boundary and exits on the Western boundary. The scheme will aim to respond to the existing watercourse through the implementation of a considered zoning strategy.



Zonal Strategy

From a hydraulic and spatial perspective, the site presents opportunities to create sustainable urban drainage solutions and therefore additional habitats and natural buffers to the surrounding context.



Sketch Proposal

Infrastructure attenuation ponds are located in seven locations around the site in order to mitigate flood risk. Additional on plot attenuation can be afforded where appropriate via balancing ponds or attenuation tanks situated beneath service yards.

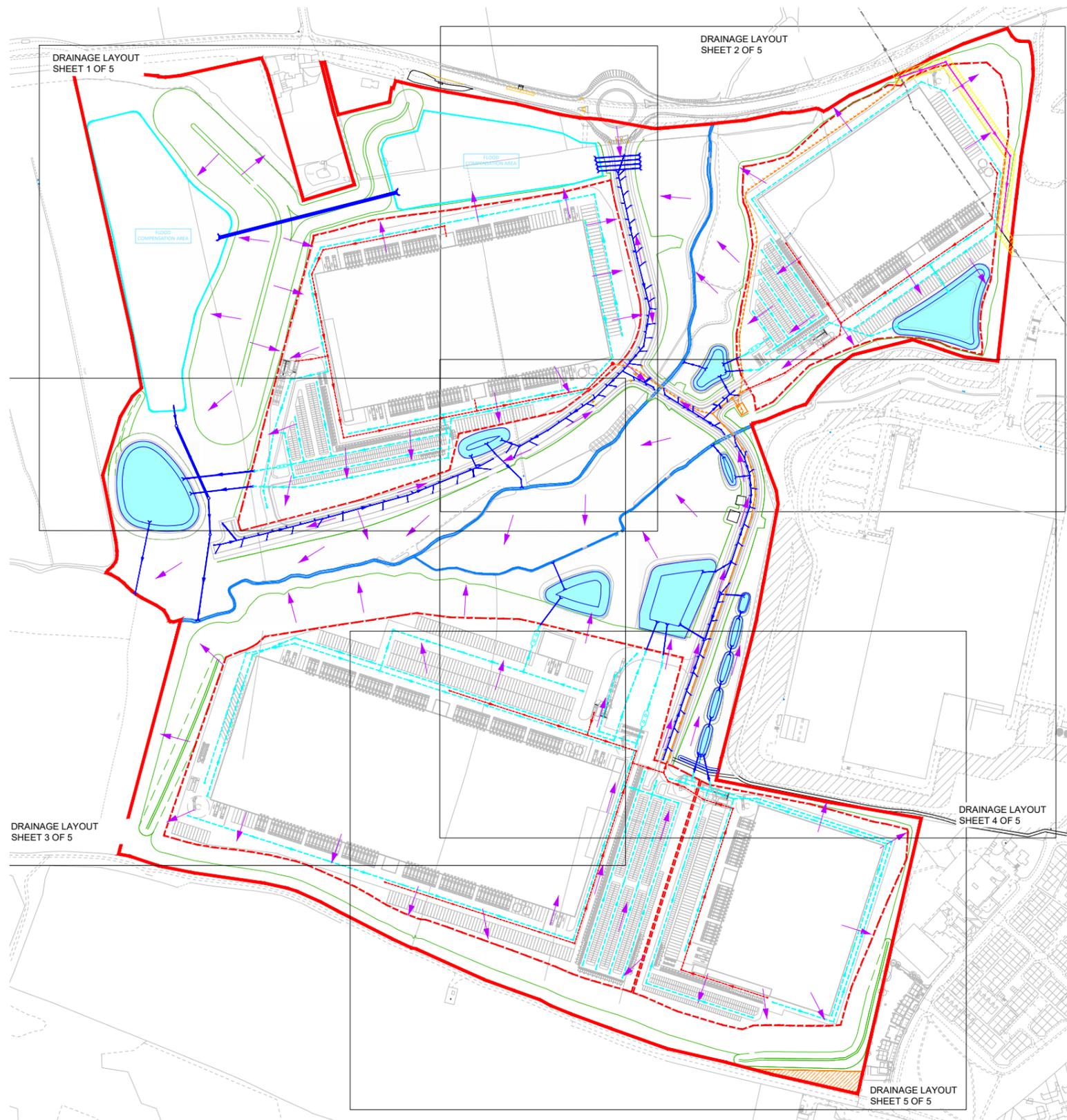
06 DEVELOPMENT PROPOSALS

06.4 BLUE INFRASTRUCTURE PROPOSAL

The blue infrastructure strategy has been designed to provide a cohesive scheme that benefits from a functional, sustainable and attractive blue network.

- Reduced energy consumption through the utilisation of a sustainable drainage system
- Attractive spaces throughout the site enhancing the experience of the user.
- Creation of additional habitats
- Mitigation of standing surface water to road and footways.

Overall the proposed blue infrastructure will be an important feature of the development, working together with the green infrastructure in order to create an environment that benefits human health and ecosystems alike.



Key	
	PLANNING APPLICATION BOUNDARY
	DEVELOPMENT PLOT BOUNDARY
	PROPOSED INFRASTRUCTURE SURFACE WATER
	PROPOSED INFRASTRUCTURE FOUL SEWER
	PROPOSED FOUL SEWER RISING MAIN
	INDICATIVE ON-PLOT SURFACE WATER
	INDICATIVE ON-PLOT FOUL SEWER
	INDICATIVE ON-PLOT OIL INTERCEPTOR
	PROPOSED PRIVATE SURFACE WATER ATTENUATION
	PROPOSED YW PUBLIC SEWER DIVERSION
	EXISTING YW PUBLIC COMBINED SEWER TO REMAIN
	SURFACE WATER EXCEEDANCE ROUTE
	EXISTING YW PUBLIC COMBINED SEWER TO BE REMOVED
	6m YW PUBLIC SEWER EASEMENT
	PROPOSED INFRASTRUCTURE FILTER DRAIN

06 DEVELOPMENT PROPOSALS

06.5 CONNECTIVITY

Being located directly adjacent to the A635 with links to the M1 and A1(M), the site is ideally suited for the distribution and logistics sector with strategic links to the north and midlands.

The strategic infrastructure already exists and having modelled the impact of the development on the local highway network, suitable measures will be incorporated to accommodate the increase in traffic from the development.



Existing Site Connectivity

There is currently no primary highway access into the site other than a gated farm entrance which enter the site on the Northern boundary from the A635. There is a new roundabout currently under construction providing direct access to the site from the A635.



Connectivity Strategy

The proposed site entry point will be via a new arm from the new A635 roundabout to the North of the site which will run Southwards through the site connecting the proposed development plots.



Sketch Proposal

The internal estate road network will form part of the detailed application and will be designed to adoptable standards, but will remain private highways.

06 DEVELOPMENT PROPOSALS



Connectivity on Site

06.6 CONNECTIVITY PROPOSAL

The proposals include a range of sustainability measures which will significantly improve non-motorised links to this site for staff and visitors. The proposals create improved linkages from the A635, and from the surrounding context.

These sustainability measures include:

- As part of the proposed vehicle access onto the A635, a new toucan crossing facility would be provided. This would significantly improve the existing walking and cycling connectivity of the surrounding context with the site by offering safe access to the site from the Billingley Green Lane Bus Stop;
- A new, illuminated, combined footway / cycleway from the North end of the Proposed Development, Southwards to connect to the proposed development plots and from the West from Carr Field Lane;
- Secure, sheltered cycle facilities on the site together with shower, locker and changing facilities for staff / visitors;
- Electric bike charging stations;
- Car parking to have electric charging with 5% active and 20% passive;
- Strong travel planning to include various measures including car sharing schemes, preferential car parking spaces, travel vouchers etc.

06 DEVELOPMENT PROPOSALS

06.7 ILLUSTRATIVE MASTERPLAN

Illustrative material is provided with the application which shows how the development could be brought forward within the parameters established through the outline element of the application. This includes an illustrative masterplan which demonstrates a potential configuration of buildings within the development plots, whilst reflecting the strong green buffer to the periphery of the development (as explained in the preceding section of this Design and Access Statement).

This section of the Design and Access Statement sets out the design rationale behind the illustrative material provided with the application. It also aims to establish a framework for future reserved matters applications.

Design and Approach

The Illustrative Masterplan shows how the site could be brought forward for a number of units of varying sizes and has evolved throughout the preparation of the application.

Within the site, the masterplan shows landscape treatment along the proposed centralised estate road, which provides a softer edge to the development plots. This helps the siting of buildings generally, as the built form is set back from the main highway routes.

Some key views and visual interest will be created along the spine roads within the development. Landscape fringes along the roads are used to soften edges to car parking and building facades, which can be further enhanced.

In terms of existing topography, the levels generally slope down from the Southern boundary towards Carr Dike. There are also slopes from the Northern boundary sloping down toward Carr Dike.

In order to tie in with the existing levels and respect the existing constraints including retained below ground water main easement, 3x separate development plateaus are proposed, with the largest plot located to the South side of the site.

The privately secure service yards to each of the units are to be directed into the centre of the development where possible and shielded from view. The service yards are set out to provide sufficient depth to accommodate the full turning circle of a HGV within the parking and circulation zones while allowing vehicles to carry on loading at the adjacent loading doors.

Logistics and Industrial Requirements

The illustrative masterplan achieves up to 204,000m² of B2/B8 floor space with associated ancillary B1 office floorspace across the development. It can accommodate a range of unit sizes from circa. 32,052m² up to 78,038m². The unit size variation offers optimum commercial flexibility.

Service yards shown on the masterplan are security controlled and indicate a minimum of 40m depth, which is appropriate to the circulation and manoeuvring space required for modern heavy goods vehicles. There is also facility within the masterplan design to support emergency fire appliance access around the buildings, which will be required under the Building Regulations.

The illustrative masterplan has evolved considerably from first principles and the layout plan represents the final version presented as part of this application. The masterplan has incorporated a series of detailed inputs and considerations which arose during the design process, as follows:

- Plots for buildings, private service yards, semi-private car parking areas and public access are clearly defined on the illustrative masterplan. Generate buildings and external spaces which are marketable for contemporary B2 and B8 occupiers.
- Each unit provides significant ancillary operational office blocks. These offer a strong datum point to signify the entrance to each Unit within the development
- All units provide a minimum of 40m deep secure service yard areas to accommodate effective HGV circulation and all have individual gatehouse control to the access. All units incorporate both dock leveller and level access doors addressing the service yard, for the loading and unloading of goods to the warehouses
- A cohesive scheme of on-plot landscaping which integrates with the perimeter landscaping and green and blue infrastructure
- Tree planting along both sides of the spine road in order to soften the impact of the HGV route and provide visual interest.
- A clear hierarchy for vehicular, cycle and pedestrian routes into and around the site.

- Connections are provided to the principal entrances to all the Units within the development, and opportunities for direct connections into the wider perimeter landscaping and green infrastructure are maximised

Existing Site Levels

The landform of the central part and Eastern/Western part of the site is relatively flat at approximately 25 metres AOD. There is a change in level to the North of the site leading up to road level in line with the A635 which stands at approximately 26 - 32 metres AOD. To the South of the site the land levels change to approximately 33 - 44 metres AOD leading up to the public footpath. Detailed analysis of the site levels has been done to design plateaus to screen future development and minimise visual impact to the surrounding context

Based on the published geology, historical use of the site, and subject to the findings of a detailed intrusive site investigation; no geotechnical or geoenvironmental barrier to undertaking the proposed earthworks exercise is anticipated.

Proposed Plateau Levels

The proposed cut and fill has been based on a rationale to lower the proposed finished floor levels across the site in order to lower the ultimate ridge height of buildings and reduce visual impact. Earthworks arisings from the site will be employed to form the landscape & ecology zone to the north and west proposed site.

Conclusions

The earthworks assessments have confirmed that a theoretical earthworks balance can be achieved on site whilst enabling site highway levels that are appropriate for means of vehicle and pedestrian access.

Appropriate means of disposing of both storm and foul water are available through the on-site infrastructure.

06 DEVELOPMENT PROPOSALS

06.8 GOLDTHORPE MASTERPLAN FRAMEWORK COMPLIANCE

Design Elements	Goldthorpe Masterplan Framework	Proposed Development	Compliance	Comments
Use	General employment Space	Use Class B2 (30%) & B8 (70%)	✓	Amount of B2 limited to reduce peak hour highways impacts.
Area	72.9 Ha	85.32 Ha	✓	Increased area for additional landscaping & flood compensation areas.
Floorspace	Around 204,000 sqm	Up to 204,000m ²	✓	
Siting	4 development platforms	4 development plots created on 3 plateau area	✓	In general accordance with the Masterplan Framework.
Heights	Buildings should not exceed 15m to the highest point where the footprint is 20,000sqm or less and should not exceed 18m to the highest point where footprints are over 20,000sqm.	Plot 1 - 568,500ft ² /52,815m ² Plot 2 - 370,000ft ² /34,374m ² Plot 3 - 840,000ft ² /78,038m ² Plot 4 - 345,000ft ² /32,052m ²	✓	Height reduced to a maximum of 18m for all buildings given pre-app discussions but the market signals suggests provision should be made for higher buildings.
Appearance	Grey and blue cladding considered.	See Illustrative Building Elevations	✓	
Access	Provide appropriate access to housing site reference HS51 from Billingley View	Land has been safeguarded.	✓	
Connectivity	Incorporate the existing PROW into design	Retention of PROW and introduction of additional cycle and pedestrian routes which connect into Goldthorpe Town and Bolton on Dearne.	✓	Enhancement of these routes will be considered in the context of becoming sustainable, active travel links to support the development of the Travel Plan.
Landscape	Retain the existing woodland and hedgerows on the site periphery & hedgerow in the north-west corner of the site.	These areas of vegetation will be protected where possible.	✓	These elements will help integrate the development into the surrounding area in landscape and ecological terms.
Green Infrastructure	Retention of Carr Dike and creation of a habitat corridor (at least 8m in width).	Carr Dike retained and deliver a corridor width of between 50 - 80m.	✓	This element will exceed the guidance and habitat enhancement to this corridor will be recommended by our ecologist.
Biodiversity Net Gain	Achieve at least 10% BNG	Achieve minimum 10% BNG through on and off-site provision.	✓	
Flood Risk	Avoid locating any built development in Flood zones 2 and 3	Plot 1 proposes an extended development platform in the flood zone 3 to the north.	✗	Areas of flood compensation to be provided as part of the development - refer to Section 6.13.
Sustainability	Consideration to be given to Sustainability standards.	Minimum of BREEAM 'Excellent'.	✓	Exceeds adopted planning policy requirements.

06 DEVELOPMENT PROPOSALS

06.9 NATIONAL DESIGN GUIDE COMPLIANCE

The National Design Guide sets out a series of key characteristics which address the question of how we recognise a well-designed place. The document outlines the government's priorities which should be considered when designing new places.



Characteristic	Key Point	Compliance	Comments
Context	<ul style="list-style-type: none"> C1 Understand and relate well to the site, its local and wider context C2 Value heritage, local history and culture 	✓	The proposed scheme responds positively to the surrounding context through the careful consideration of levels, building form, materiality, and ecology to minimise the impact on both the local and wider context.
Identity	<ul style="list-style-type: none"> I1 Respond to existing local character and identity I2 Well-designed, high quality and attractive places and buildings I3 Create character and identity 	✓	Consideration has been made to the surrounding buildings including Adli Goldthorpe RDC, Goldthorpe Industrial Estate and the nearby residential properties, to ensure the proposal fits coherently with the local character and context.
Built Form	<ul style="list-style-type: none"> B1 Compact form of development B2 Appropriate building types and forms B3 Destinations 	✓	The form of each of the proposed plots is based upon having an 18m ridge height to minimise visual impact on the surrounding context. The building type is suitable for the proposed location, in line with the Goldthorpe Masterplan Framework document, and being in close proximity to the existing Goldthorpe Industrial Estate.
Movement	<ul style="list-style-type: none"> M1 A connected network of routes for all modes of transport M2 Active Travel M3 Well-considered parking, servicing and utilities infrastructure for all users 	✓	The proposed scheme will utilise the newly proposed roundabout connection providing direct access from the A635 and an increased level of pedestrianisation, providing safer access to the site from the Billingsley Green Lane Bus Stops. The site will provide wide roads and footpaths to provide safe access around the site. The proposal also includes the inclusion of a new pedestrian route providing access to the site from the Bolton upon Dearne residential area.
Nature	<ul style="list-style-type: none"> N1 Provide a network of high quality, green open spaces with a variety of landscapes and activities, including play N2 Improve and enhance water management N3 Support rich and varied biodiversity 	✓	The proposal seeks to enhance the ecological value of the site by preserving the existing Carr Dike watercourse transecting the site. A technical proposal has been formulated to ensure the existing watercourse has been managed appropriately. The proposal includes the implementation of an SUSD (Sustainable Urban Drainage System). The use of balancing ponds can provide additional storage for the Carr Dike, reducing the risk of flooding. The scheme includes attenuation ponds which will provide additional habitats for wildlife. To limit the visual impact of the proposal, the inclusion of additional tree belts to the North, East and South of the site has been included in the proposal, not only reducing visual impact, but also enhancing the site ecologically.
Public Spaces	<ul style="list-style-type: none"> P1 Create well-located, high quality and attractive public spaces P2 Provide well-designed spaces that are safe P3 Make sure public spaces support social interaction 	✓	The newly proposed entrance to the site from the A635 will provide safer access for pedestrians with the inclusion of lighting columns and crossing points. Movement around the site is carried out through the use wide roads and paths to minimise congestion and ensure safe pedestrian access across the site.
Uses	<ul style="list-style-type: none"> U1 A mix of uses U2 A mix of home tenures, types and sizes U3 Socially inclusive 	✓	The proposed buildings on the site are in line with those described within the Goldthorpe Masterplan Framework document (Class B8 (and ancillary offices)). The proposed use sits coherently with the immediate surrounding context including Aldi Goldthorpe RDC and the Goldthorpe Industrial Estate.
Homes and Buildings	<ul style="list-style-type: none"> H1 Healthy, comfortable and safe internal and external environment H2 Well-related to external amenity and public spaces H3 Attention to detail: storage, waste, servicing and utilities 	✓	The plots will be designed to cater for the specific needs of the building users, employing WELL Building Standards to ensure the environment is safe both internally and externally during operation. The scheme includes external amenity space and a carefully considered external environment providing a positive external space for pedestrians and visitors. The scheme has considered storage, waste, servicing, utilities and accessibility to ensure the overall practicality of the scheme.
Resources	<ul style="list-style-type: none"> R1 Follow the energy hierarchy R2 Careful selection of materials and construction techniques R3 Maximise resilience 	✓	The scheme will aim to achieve a BREEAM Excellent Rating, considering sustainability across the project from energy to ecology. In terms of resources, the scheme will specify high-quality materials to ensure overall building resilience relative to the building typology.
Lifespan	<ul style="list-style-type: none"> L1 Well-managed and maintained L2 Adaptable to changing needs and evolving technologies L3 A sense of ownership 	✓	The typical lifespan for each unit would be approximately 50 years before maintenance may be required. Due to the nature of material selection, many of the materials to be specified will be recyclable following their lifespan. The employed construction technique used for the units will allow for adaptability as needs may change and technologies evolve.

06 DEVELOPMENT PROPOSALS

06.10 PROPOSED PLATEAU LEVELS

The proposed cut and fill has been based on a rationale to lower the proposed finished floor levels across the site in order to lower the ultimate ridge height of buildings and reduce visual impact.

The plateau levels and indicative finished floor levels in m above ordnance datum are as follows:

Plot 1 – 24.5m (FFL: 25.5m)

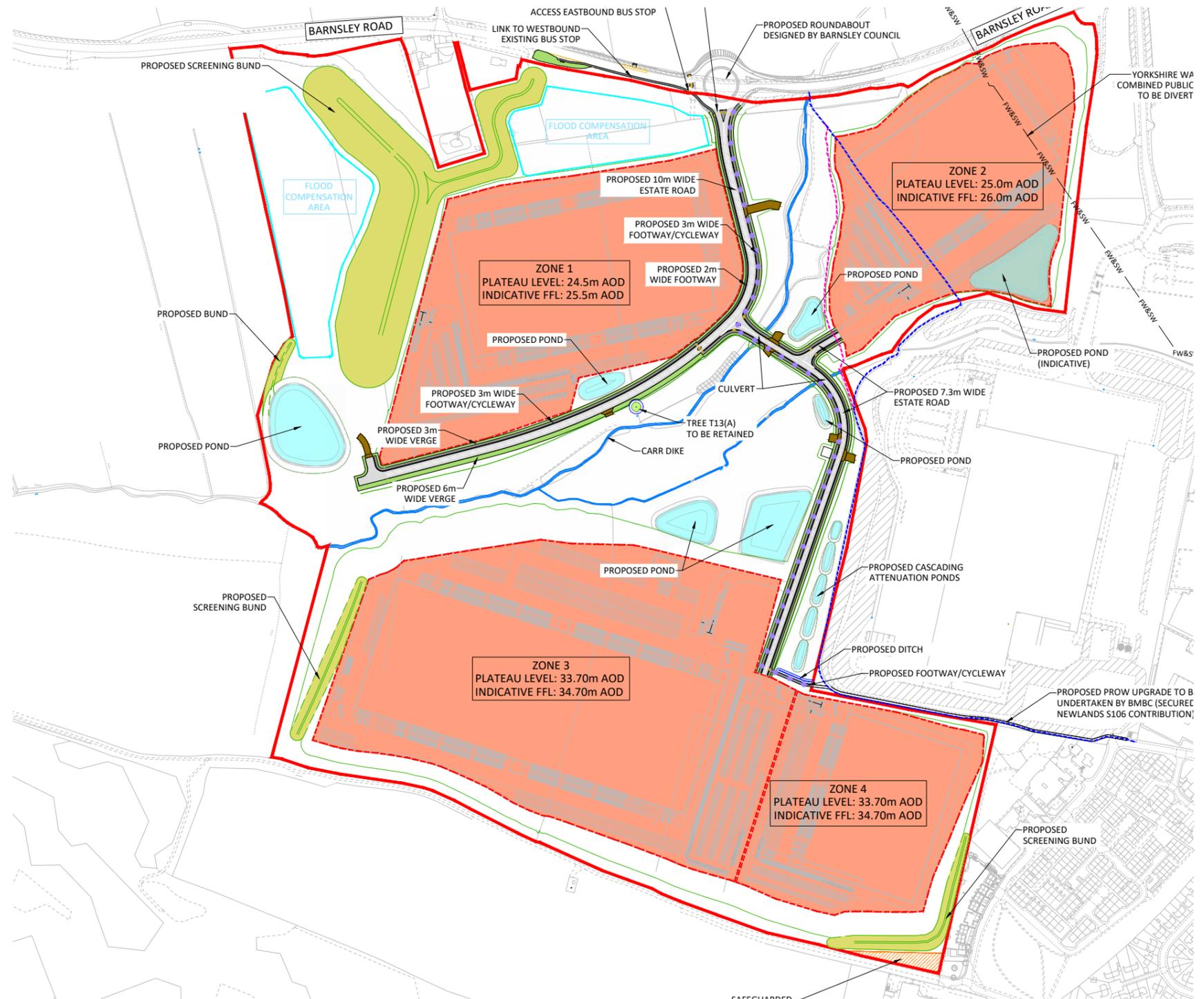
Plot 2 – 25.0m (FFL: 26.0m)

Plot 3 – 33.7m (FFL: 34.7m)

Plot 4 – 33.7m (FFL: 34.7m)



Illustrative Masterplan



Proposed Site Levels

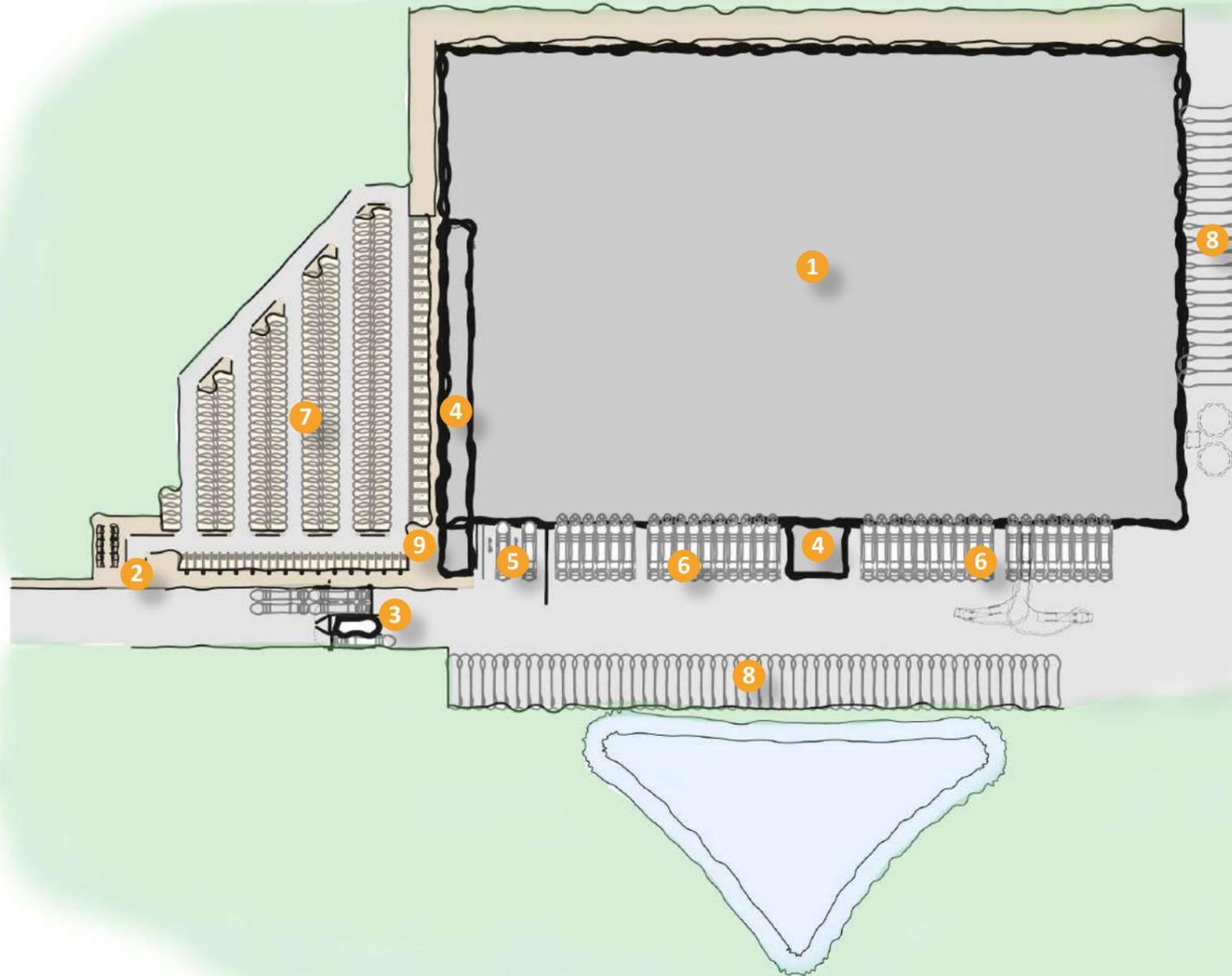
06 DEVELOPMENT PROPOSALS

06.11 PLOT DESIGN

The following are important factors when designing the layout of the development plots:

- Access and manoeuvrability
- Site Parking
- Loading Capacity
- Development Frontage
- Overall Masterplan

The sketch to the left is used as an example to explain the approach to the plot design below.



Indicative Plot Layout

Key

- | | | |
|------------------------|-----------------|-----------------|
| ① Unit | ④ Office | ⑦ Car park |
| ② Site Entrance (Cars) | ⑤ Level Access | ⑧ HGV Parking |
| ③ Site Entrance (HGVs) | ⑥ Loading Doors | ⑨ Main Entrance |

General Plot Design

The layouts aim to make effective use of the site whilst not impacting or restricting the development of the rest of the masterplan. The Unit layout has been designed to ensure efficient use of the site area, while optimising the soft landscaping that surrounds the development. Adequate parking is provided for cars through the use of a grade level car park which includes accessible parking spaces positioned within close proximity of entrance. Inclusive access throughout the site is achieved, where applicable, with 2m wide paths leading pedestrians from the car park to the ancillary office. Access into the site is sufficient for both disabled and able-bodied pedestrians.

An office and car park frontage has been provided to the Western, and Southern elevation of the building in order to provide a positive 'active' façade.

Heavy goods vehicles will arrive via the gatehouse controlled entry point (marked as '3' on the drawing to the left), to the perimeter plot boundaries. This will enable them to be checked in and given specific delivery instructions. Vehicles will exit the site from the same point. Service yards to units across the site are to be directed into the centre of the development and shielded from view. This allows sufficient circulation for HGV parking and loading for required number of loading doors. The service yards are set out with a minimum depth of 40m, sufficient to accommodate the necessary manoeuvring space in front of loading doors.

06 DEVELOPMENT PROPOSALS



Car Parks



Roads



Pathways



HGV Service Yards

06.12 PLOT HARD LANDSCAPING

Car Parks

The car park areas could be constructed in a mix of macadam and block work, which will form part of a coordinated hard landscaping strategy. Parking for cars is provided through the use of a grade level car park which includes 5% accessible parking spaces positioned within close proximity of the office entrance. Inclusive access throughout the plots is achieved, where applicable, with 2m wide paths leading pedestrians from the car park to the ancillary office.

Pathways

A 3m wide footpath / cycleway connection from the A635 will give access into the development from the North of the site, alongside the provision of a footpath to the East of the site providing access into the development from Goldthorpe/Bolton upon Dearne. This new route will link into the on-site infrastructure roads, all of which accommodate footpaths and cycleways as required. Lighting by street lamps during hours of darkness will help to provide a safe and secure environment for the pedestrian.

Inside individual plots, paths will lead from the car park to each office main entrance. Tactile paving and dropped kerbs will be provided at all road junctions, with further paving extended around offices and to the warehouse perimeter.

Roads

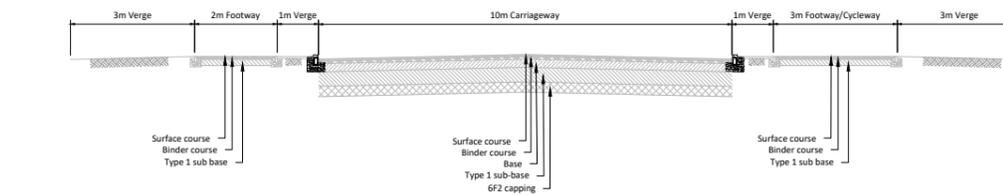
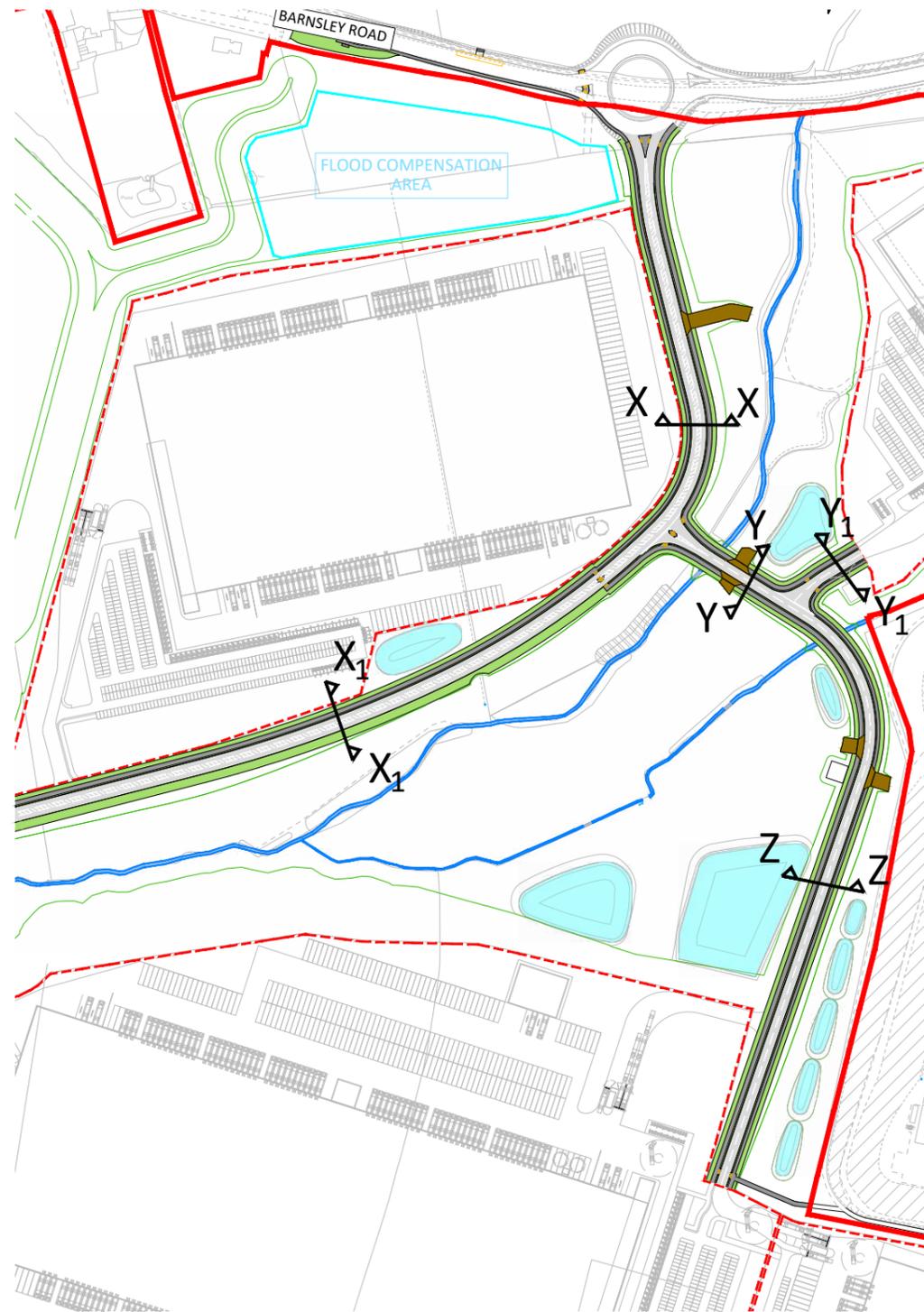
Access to the site will be via a new roundabout on the A635. The entrance into each unit will be designed to accommodate vehicle queuing, particularly at gatehouse / barrier control points which will help to manage and contain the flow of traffic both into the site and back out onto the roads.

HGV Service Yards

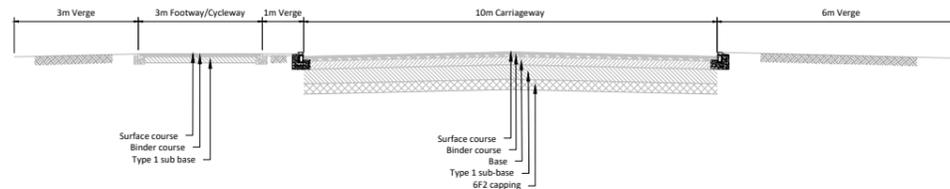
Concrete service yards are formed usually on one side of the main warehouse building to allow sufficient circulation for HGV parking and loading for required number of loading doors. The service yard is usually set out with a minimum depth of 40m.

06 DEVELOPMENT PROPOSALS

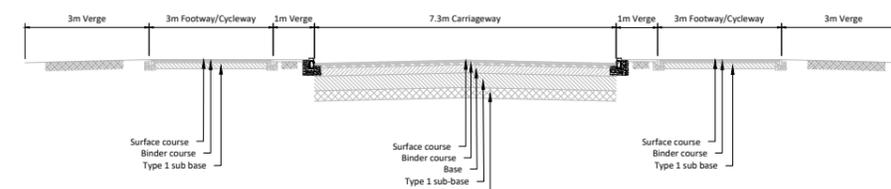
06.12 PLOT HARD LANDSCAPING



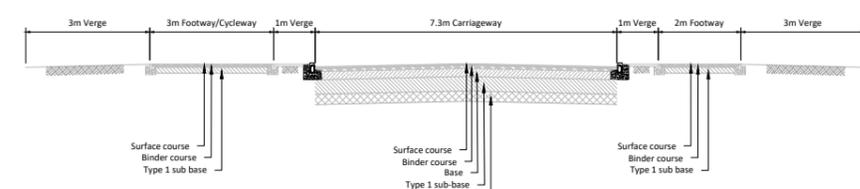
Section X-X



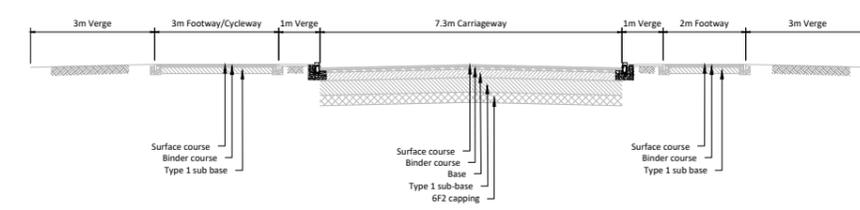
Section X1-X2



Section Y-Y



Section Y1-Y2



Section Z-Z

The proposed access roads across the site have been designed to provide appropriate connectivity to each of the plots. Upon entering the site, the proposed road width is 10m to provide safe access for vehicles entering the site. The remaining roads are proposed to be 7.3m wide. This will ensure safe access across the site for both vehicles, pedestrians and cyclists, alongside suitable access for emergency vehicles.

A technical proposal has been formulated which defines appropriate build ups based on the existing site topography and conditions.



Indicative Roads



Indicative Road/Path

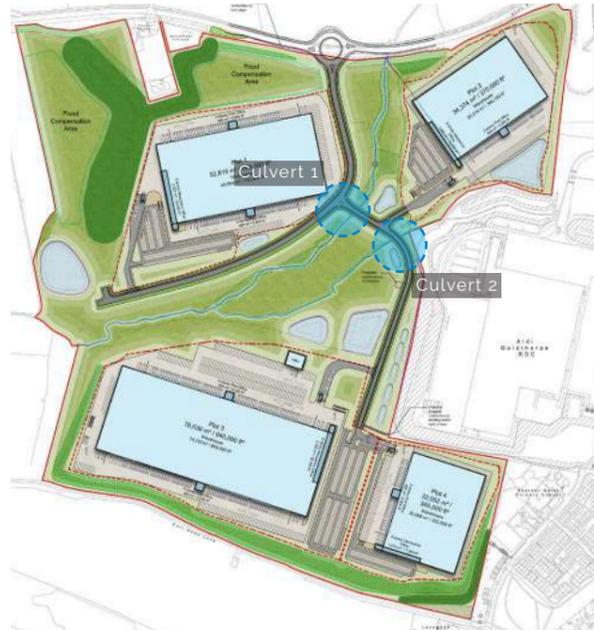
06 DEVELOPMENT PROPOSALS

06.13 PROPOSED CULVERTS

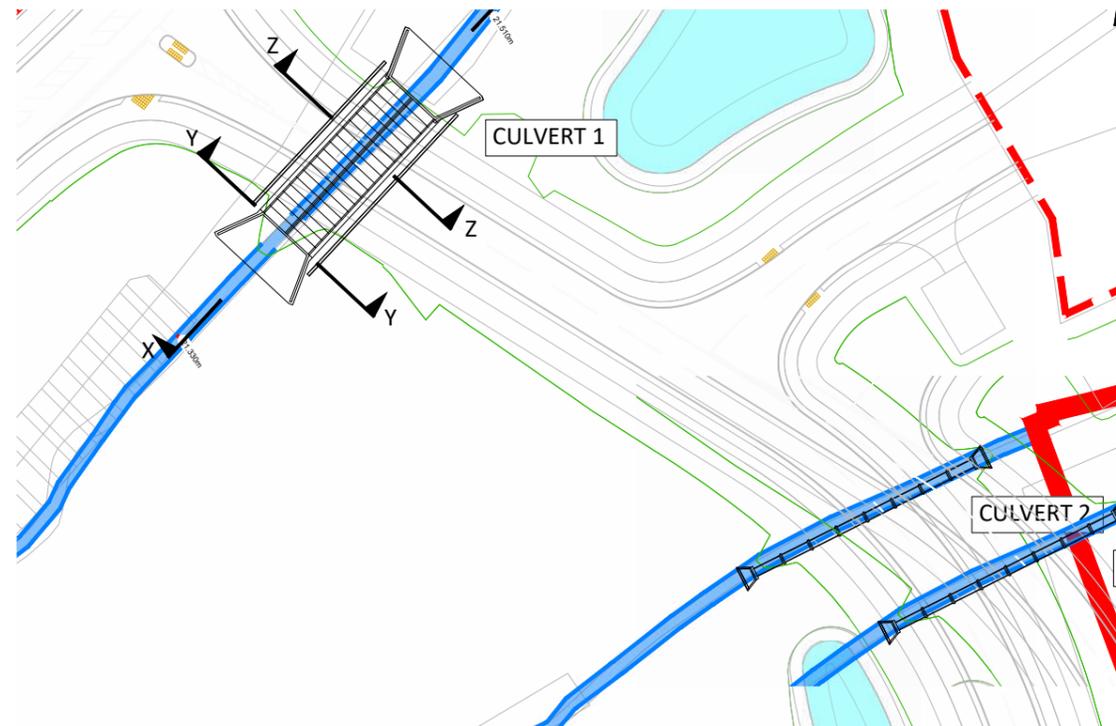
The proposed scheme aims to preserve the existing watercourse which transects the site. Due to the scale of the scheme and the route of the existing watercourse, the proposed access road crosses over the watercourse as indicated below.

As a result a technical proposal has been formulated which proposes the incorporation of two culverts at these crossing points.

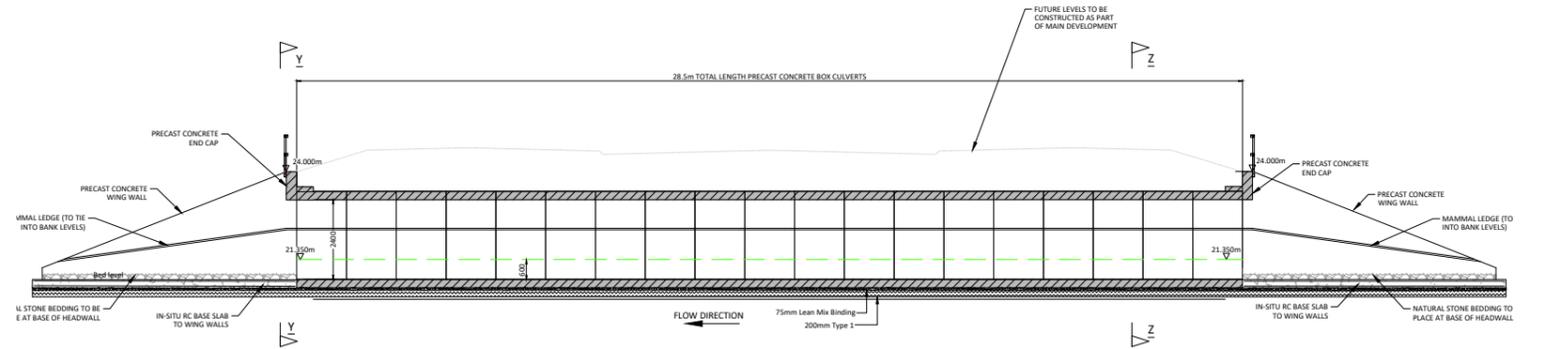
The culverts will not be detrimental to ecology of the site and will be necessary to allow the full extent of the site to be utilised. They form a pivotal part of the proposed connectivity strategy and the provision of a spinal road connecting the 4 plots within the site constraints.



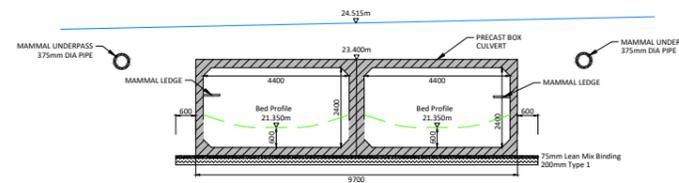
Illustrative Masterplan



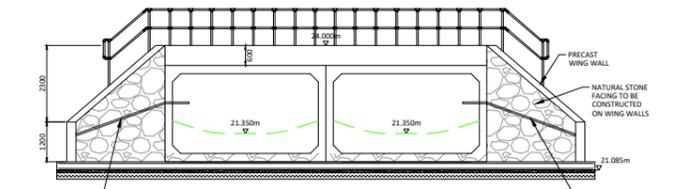
Proposed Culvert Locations



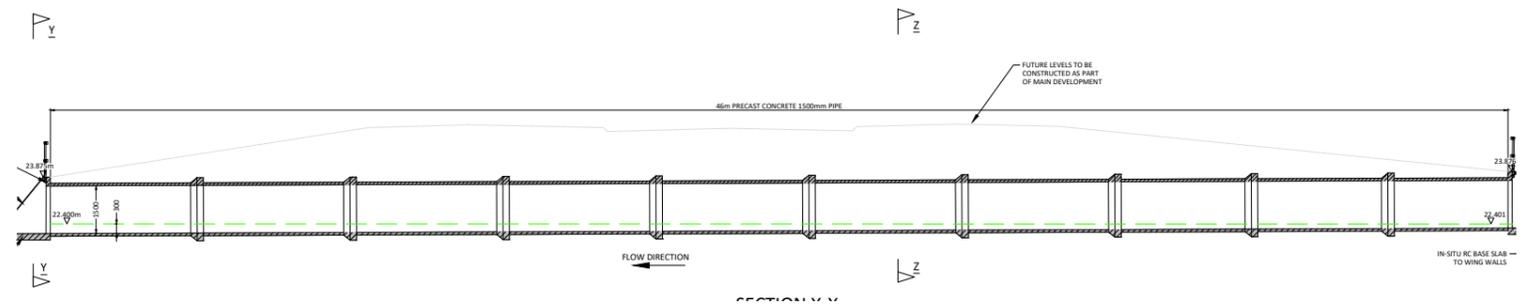
Culvert 1 - Section X-X



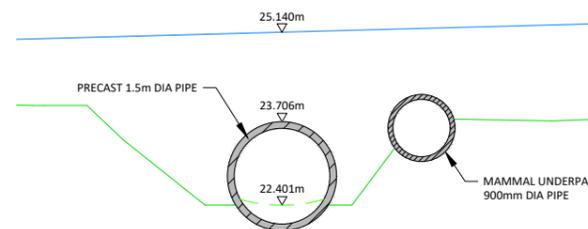
Culvert 1 - Section Z-Z



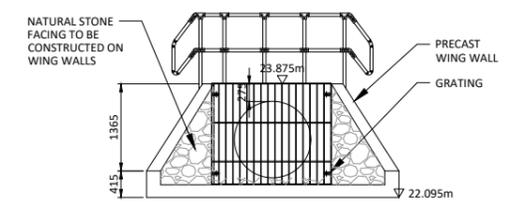
Culvert 1 - Section Y-Y



Culvert 2 - Section X-X



Culvert 2 - Section Z-Z



Culvert 2 - Section Y-Y

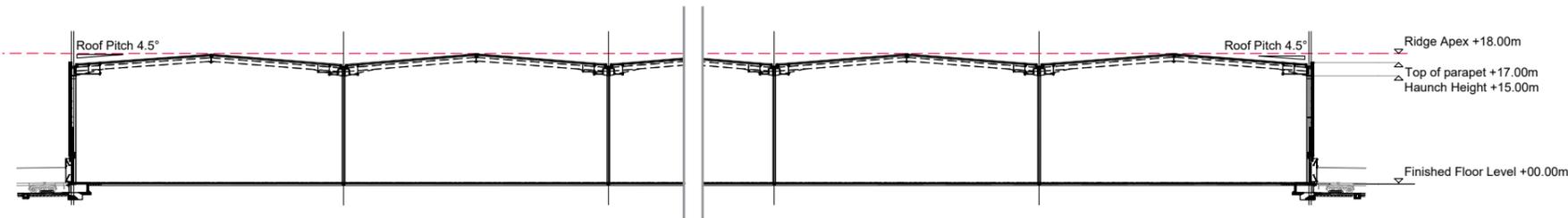
06 DEVELOPMENT PROPOSALS

06.14 SCALE AND MASSING

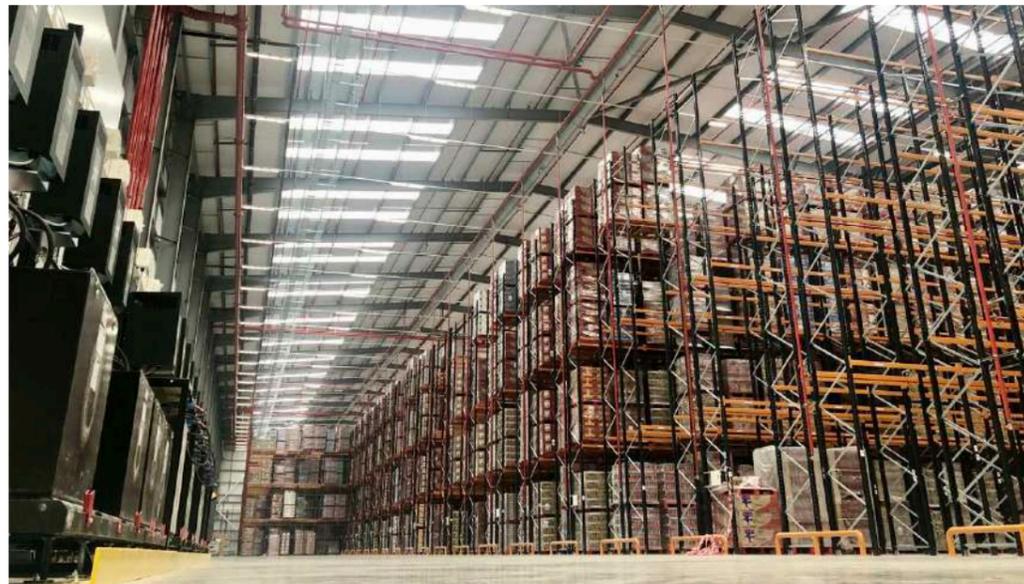
The proposed warehouse buildings shown on the illustrative masterplan are based around the operational practicalities and business needs of potential Occupiers. The size and height of the buildings, set within the proposed parameters, are designed to accommodate current market requirements, whilst building in flexibility for future changing needs. Furthermore, the scale is in line with the Goldthorpe Masterplan Framework which states "buildings should not exceed 15m to the highest point where the footprint is 20,000m² or less, and should not exceed 18m to the highest point where footprints are over 20,000m²."

To accommodate the types of storage and sortation equipment that have been introduced to units of this size in recent years, owing to improvements in automation & racking technology, warehouse building heights have generally increased as Occupiers look to achieve greater storage capacities.

To accommodate various types of sortation equipment that have been introduced to units of this size in recent years as well as being anticipated in the future, the proposed unit height shown is 18m from FFL to ridge providing a clear internal height of 15m. This is to minimise visual impact to the surrounding context and ensure compliance with the Goldthorpe Masterplan Framework.



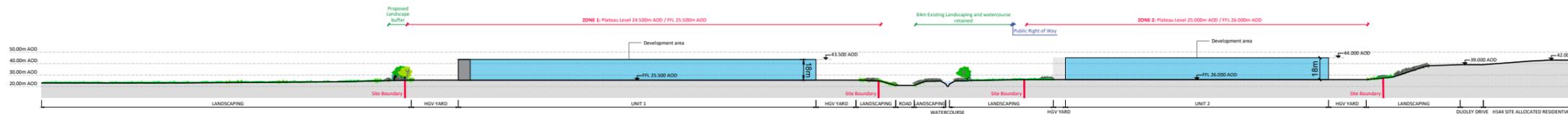
Unit 1 Indicative Warehouse Section



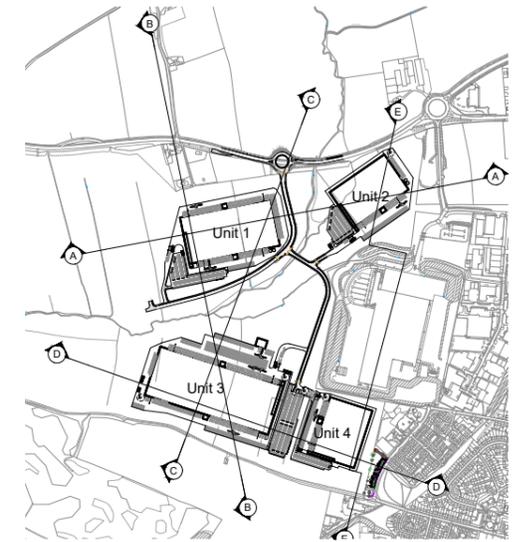
Example of Typical Racking

06 DEVELOPMENT PROPOSALS

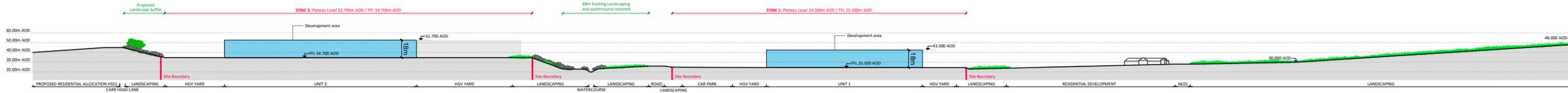
06.15 SITE SECTIONS



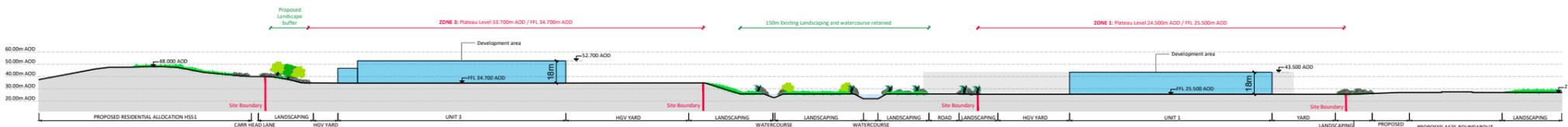
Site Section A-A (Not to Scale)



Location Plan (Not to Scale)



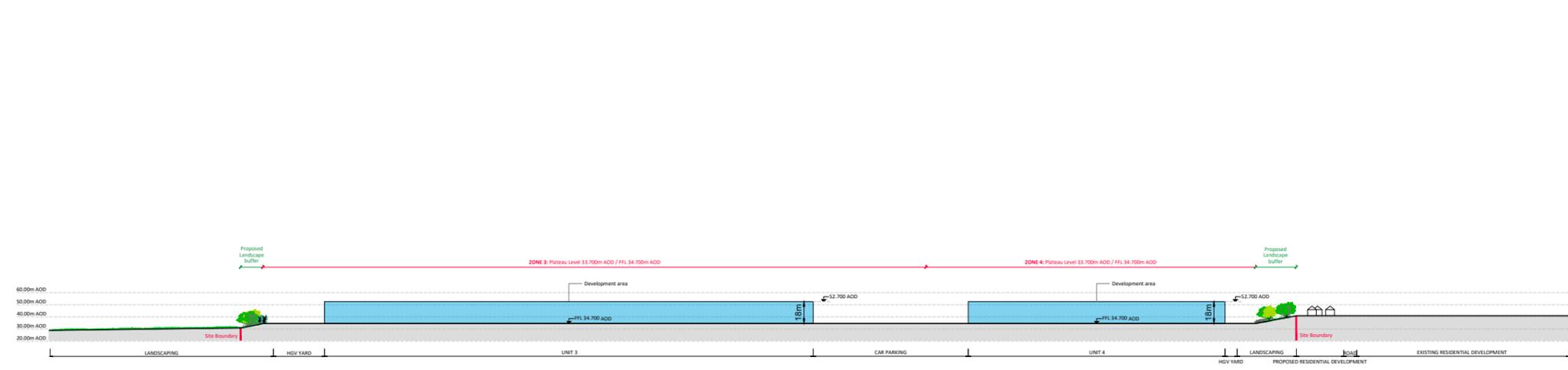
Site Section B-B (Not to Scale)



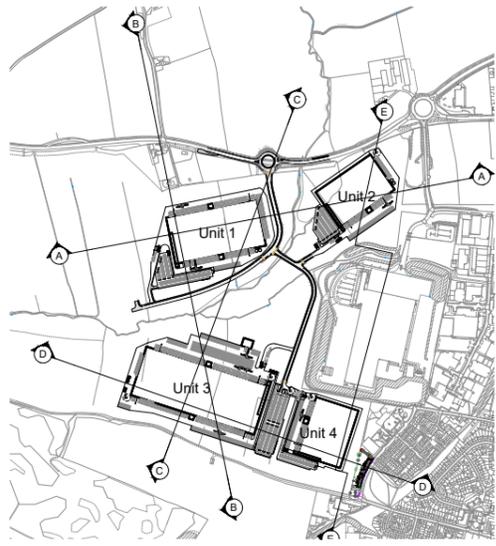
Site Section C-C (Not to Scale)

06 DEVELOPMENT PROPOSALS

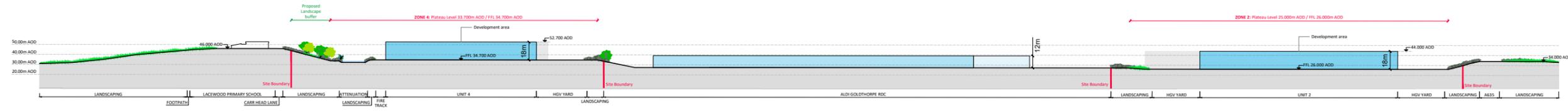
06.15 SITE SECTIONS



Site Section D-D (Not to Scale)



Location Plan (Not to Scale)



Site Section E-E (Not to Scale)

06 DEVELOPMENT PROPOSALS

06.16 APPEARANCE

This section illustrates how the façade design of future reserved matters applications could be developed in conjunction with the photomontage analysis work carried out.

The underlying principle of the design approach is to provide buildings that offer architectural character, quality and enhancement to the development whilst understanding that buildings of this type cannot be fully concealed.

The elevational treatment will be designed to minimise the visual impact of the buildings toward sensitive views, while allowing for interest and activity at the entrances to the development. A range of external materials and colour palettes are available to enhance building elevations and to soften the appearance and break up the visual proportions of larger building elevations. Combined with contemporary industrial detailing, materials such as profiled and flat metal cladding panels and glass curtain walling will be selected to offer high quality finishes with a low environmental impact utilising the Building Research

Establishment Environmental Impact Assessment rating system where possible, to include materials of excellent longevity and durability, in addition to being recyclable.

The appearance of large warehouse buildings can visually be broken down further into smaller sections in a number of ways with the use of different textured cladding, facing materials colours and textures which are complementary but provide visually contrasting areas to sub-divide the massing of the elements into smaller areas.

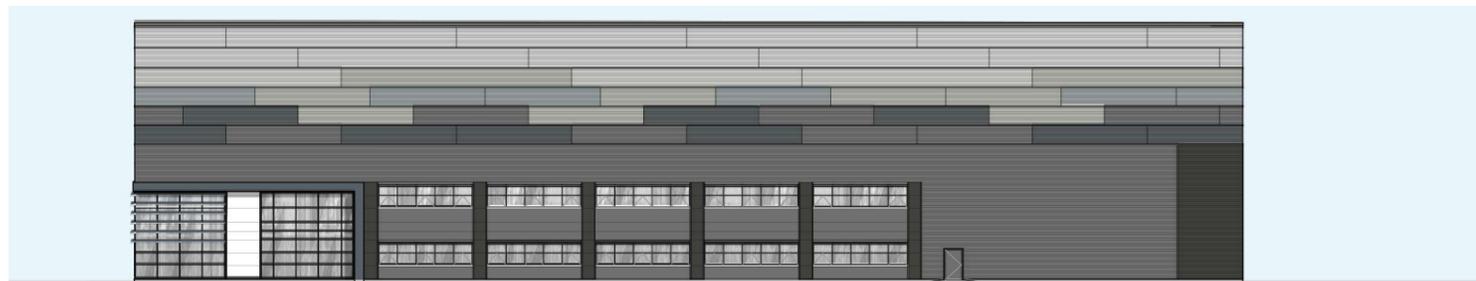
The elevations below take into account the following key criteria:

- To follow a parapet roof form to the building facades, which avoids high-level shadows being cast by over-hanging eaves. This principle was established following the photomontage analysis
- Cladding at low-level to utilise darker tones as a backdrop to the peripheral bunding and landscaping which surrounds the site This is particularly crucial in the orientations which address the key identified viewpoints.

- Simple pixelated grey tones with darker tones towards the base of the building, acting as a backdrop to the landscape bunding at the lower levels. The pixelated grey tones to the higher levels, articulate undulation and breakdown horizontal views.
- Offices should be used to give a strong focal point on the facades of the buildings.
- The design of the office elevations are to be high-quality, as these elements offer an opportunity to create feature areas of the buildings that will be visible from the estate road.
- The detailed treatment of cladding materials and fenestration are to be carefully considered such that the final design reflects a strong identity and hierarchy.



Indicative Elevation 01



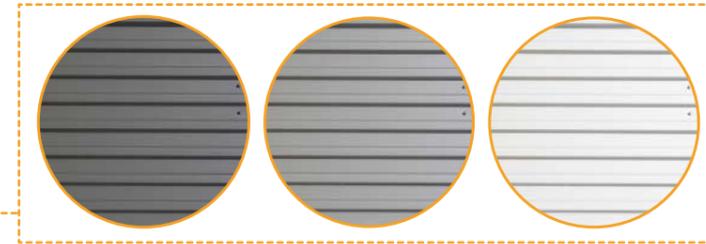
Indicative Elevation 02

Illustrative Building Elevations

06 DEVELOPMENT PROPOSALS

06.17 MATERIALS

The below images provide an indication of the typical materials palette.



Pixelated Cladding

The use of darker grey tones help anchor the facades back into the landscape whilst delivering visual interest and break up the overall mass of the building. The upper levels of the facade could have lighter shades to create the illusion of the building blending into the sky



Horizontally Laid Profiled Cladding

The use of anthracite solidifies the building's position with the surrounding context whilst delivering visual interest and break up the overall mass of the building.



Feature Band and Glazing

The use of darker grey tones help anchor the facades back into the landscape whilst delivering visual interest and break up the overall mass of the building.



Office Anthracite Curtain Walling, Windows & Doors

Whilst the dark aluminium curtain walling, windows and doors will provide a contemporary appearance, these elements help to enable good amounts of natural lighting within the office.



06 DEVELOPMENT PROPOSALS

06.18 CASE STUDIES

The below three case studies illustrate schemes delivered which have adopted a similar material palette to that of the proposed scheme.



Plot 111, Peterborough Gateway - External View



Coloplast, Peterborough Gateway - External View



Coloplast, Peterborough Gateway - External View



Plot 111, Peterborough Gateway - External View



Plot 111, Peterborough Gateway - External View



Coloplast, Peterborough Gateway - External View

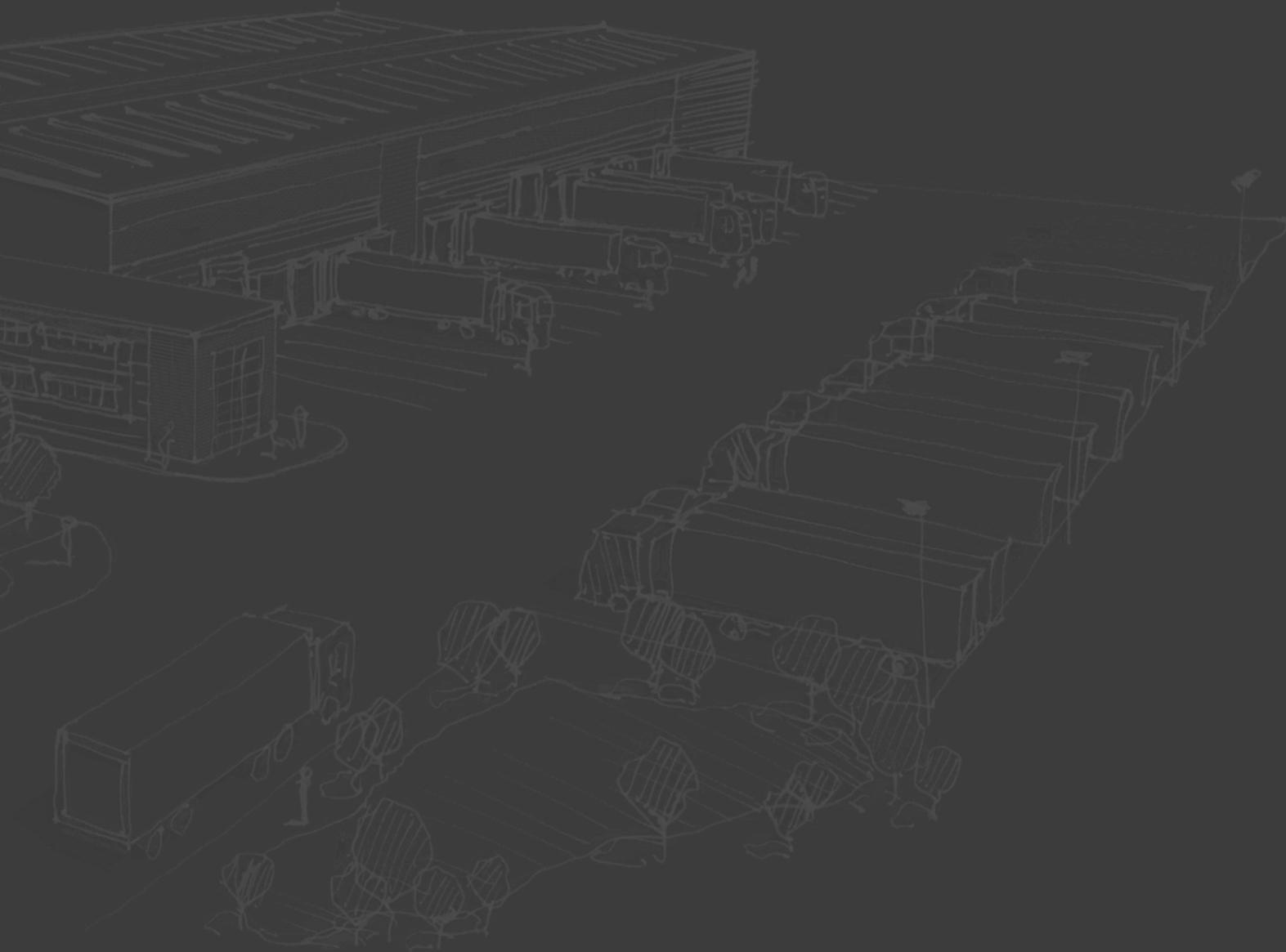
06 DEVELOPMENT PROPOSALS

06.19 ILLUSTRATIVE VIEWS

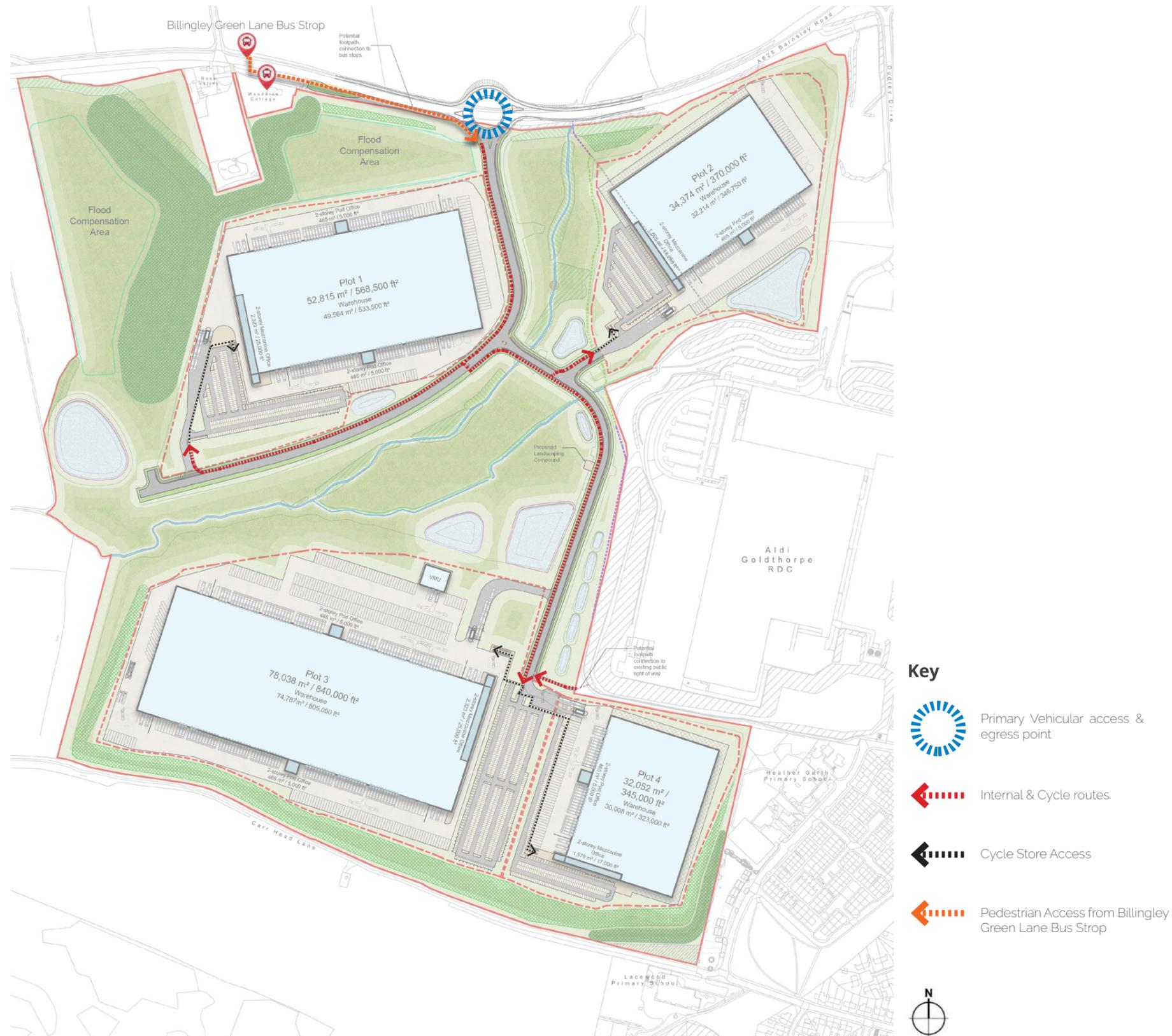


07 ACCESS

- 07.1 Pedestrian & Cycle Access
- 07.2 Vehicular Access Strategy
- 07.3 Public Transport Access Strategy
- 07.4 Pedestrian Access - Internal



07 ACCESS



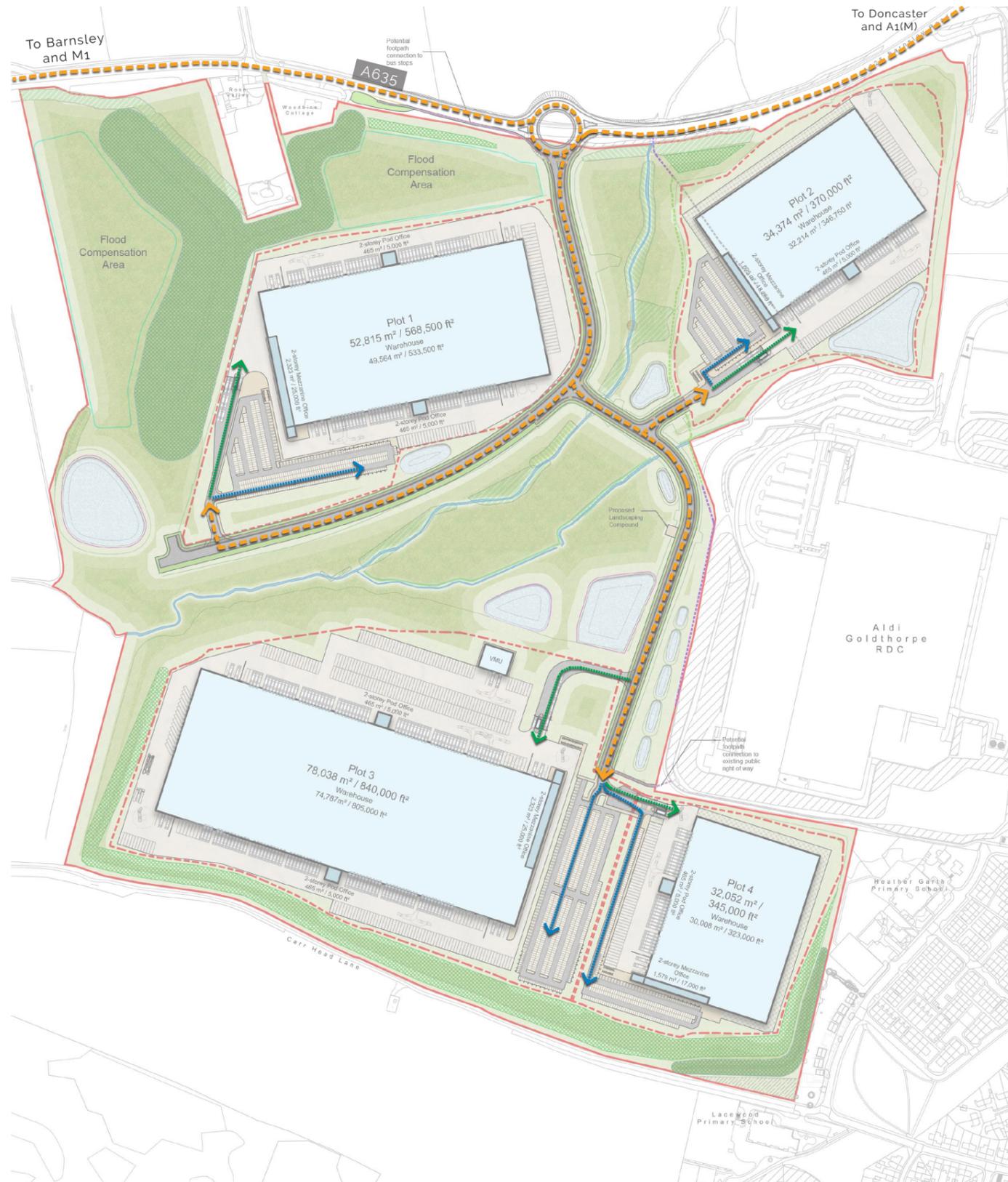
Pedestrian & Cycle Network Plan

07.1 PEDESTRIAN & CYCLE ACCESS

A series of measures would be implemented within the Proposed Development to encourage Walking and Cycling, including:

- quality cycle parking being provided within each unit, being covered, secure, lit and in well-located areas;
- these cycle parking spaces being provided as near as possible to the main entrance of the buildings, and would be covered by natural surveillance and CCTV;
- all occupiers being required to provide shower, changing and locker facilities to support more active journeys - such as those wishing to undertake longer distance cycling or to run or jog to and from the Proposed Development; and
- commitment being required from the occupiers to ensure that these facilities are suitably equipped - such as including hair dryers - and maintained regularly to ensure they remain attractive.

07 ACCESS



Vehicular Access Plan

07.2 VEHICULAR ACCESS STRATEGY

The proposed development would be served from the A635 via the new three arm roundabout (Planning Reference: 2021/1511).

Within the site, it is proposed to have 2 primary vehicular access roads which will serve each of the four units. The primary road upon entering the site is proposed to be 10m wide to provide efficient access and egress to the site. The secondary road would be 7.3m wide to provide efficient access to the units on site.

Both HGV access and visitor access have been separated with the aim to alleviate congestion on site and to provide safer access.



Indicative Road & Pavement Image

07 ACCESS



07.4 PEDESTRIAN ACCESS - INTERNAL

Internal building access will be subject to the detailed design of each individual building within future reserved matters applications, however will follow the general principles outlined below.

Reception

The reception area to the main offices could be suitably sized to accommodate wheelchair users, including appropriate space and waiting zones. All floor finishes will be selected to be suitable for wheelchair access.

Horizontal Circulation

It is anticipated that internal corridors will be a minimum of 1500mm wide at the pinch point. All doors will have a minimum clear opening of 800mm and an opening force below the recommended maximum. Door furniture will contrast with the background colour of the door leaf, and be of either lever type, at 1000mm above floor level, or pull handles, commencing at 1000mm above floor level. Doors in corridors will be fitted with vision panels, commencing at 500mm above floor level.

Vertical Circulation

Stairs would provide vertical access around the offices, and a passenger lift will provide access to all floor levels. All staircases and lifts will be designed in accordance with Approved Document M and/or relevant guidance at the time of the reserved matters application including contrasting nosings, and treads/risers suitable for ambulant disabled members of staff or visitors. Any member of staff, or visitor, with a visual handicap would be actively managed within the building.

Industrial Space

The warehouse space is to be level throughout with clearly defined pedestrian routes, with fire exits clearly defined between the main offices and operational areas. External stairs leading to the yards will be provided where necessary with minimum 900mm wide x 1400mm long refuge bays. Landings will be level with the finished floor levels at these exits.

Toilets

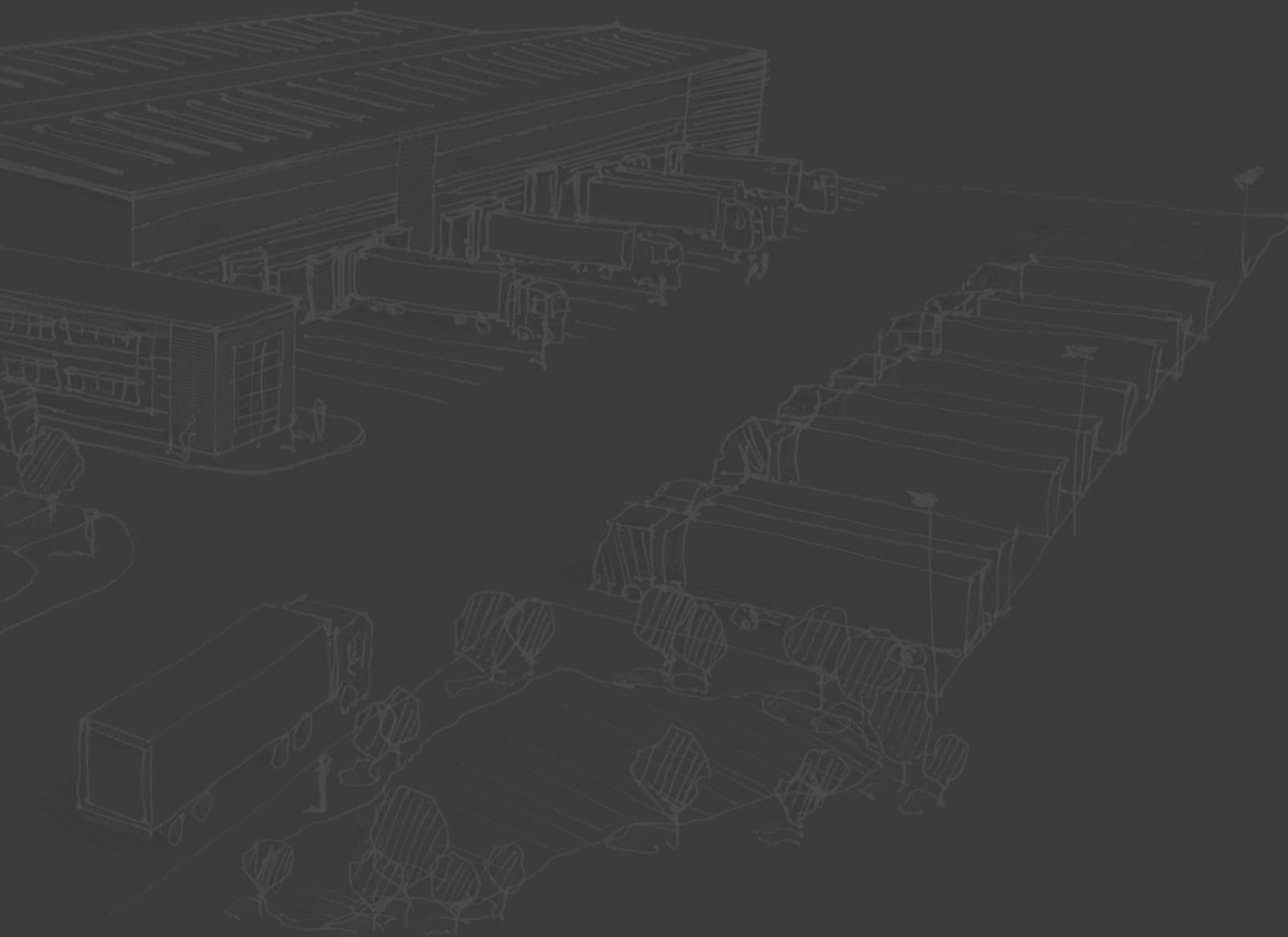
Toilets and shower facilities would be provided that are suitable for ambulant disabled staff, subject to detailed design development to suit future tenant occupancy requirements. In the offices, toilets would be provided for male, female and disabled users – these are to be found on each floor. A WC will be provided for ambulant disabled users.

Finishes

All floor finishes are to be of a non-slip type, with carpets being of a shallow dense pile, allowing easy passage for wheelchair users. The walls, wall coverings and paint finishes are to be suitably contrasting with the joinery of the doors and low surrounds. Where wall tiles are to be used, they are to have a satin finish to reduce glare.

08 LANDSCAPING

- 08.1 Proposed Landscaping Design
- 08.2 Proposed Bund Landscaping Design



08 LANDSCAPING



Proposed Landscaping Site Plan

08.1 PROPOSED LANDSCAPING DESIGN

Detailed permission is sought for the strategic and structural perimeter landscape and green and blue infrastructure for the site. Further landscaping will be included within the development plots, to be determined through future reserved matters applications.

The landscaping strategy aims to enhance the amenity value of the site and provide an attractive and welcoming environment sympathetic with the existing landscape character of the area. In order to achieve this the following targets and objectives will be met;

- Recognise and respect existing landscape character;
- Conserve and enhance landscape areas and features where possible as an integral and structuring part of the landscape framework;
- Create a high quality new landscape framework, including woodland and structure planting, hedgerows, other mixed habitats, open spaces and sustainable drainage features giving rise to biodiversity enhancements;
- Provide significant new planting as part of a thorough and long-term approach to the growth and management of the overall landscape framework;
- Minimise any potential adverse landscape or visual effects through the application of best practice design principles and careful attention to design through all stages of the development process; and
- Adopt specific landscape measures to mitigate any potential adverse landscape, visual or other environmental effects (e.g. screen mounding proposals).

The proposals include the following:

- A strong landscape framework forms an integral part of the Proposed Development, comprising the conservation of existing hedgerows and trees where possible, reinforced by significant new tree, hedgerow and shrub planting and other habitats. The formation of new earthworks and mounding proposals to the main site's perimeter will include much of this new native planting and will be important in mitigating and screening views of the Proposed Development from beyond the boundary.
- Largely native trees and shrubs would be used to reflect those in the existing locality. A mix of planting sizes and densities would be adopted to satisfy the differing objectives, principally those of providing amenity and a level of screening and filtering in the short and longer terms and of establishing well balanced woodland and planting habitats.
- At a strategic level, these proposals will reflect the broad stated aims and approaches outlined in the Natural England Green Infrastructure (GI) and the National, County and District based landscape character and GI guidance.

08 LANDSCAPING

08.2 SITE PERIMETER INFRASTRUCTURE

In order to establish the perimeter design criteria a Landscape and Visual Impact Assessment (LVIA) has been prepared which forms part of the overall Environmental Statement for the Proposed Scheme.

The main elements of the site's structural landscaping has been developed around the perimeter of the site and focuses on woodland planting and specimen avenue trees to roadsides. Detailed planning permission is sought for these elements and they will be developed as part of the advanced works within the site. This provides extensive screening, ecological habitats and passive/ active recreational space for walkers and cyclists.



Indicative Views of Car Park



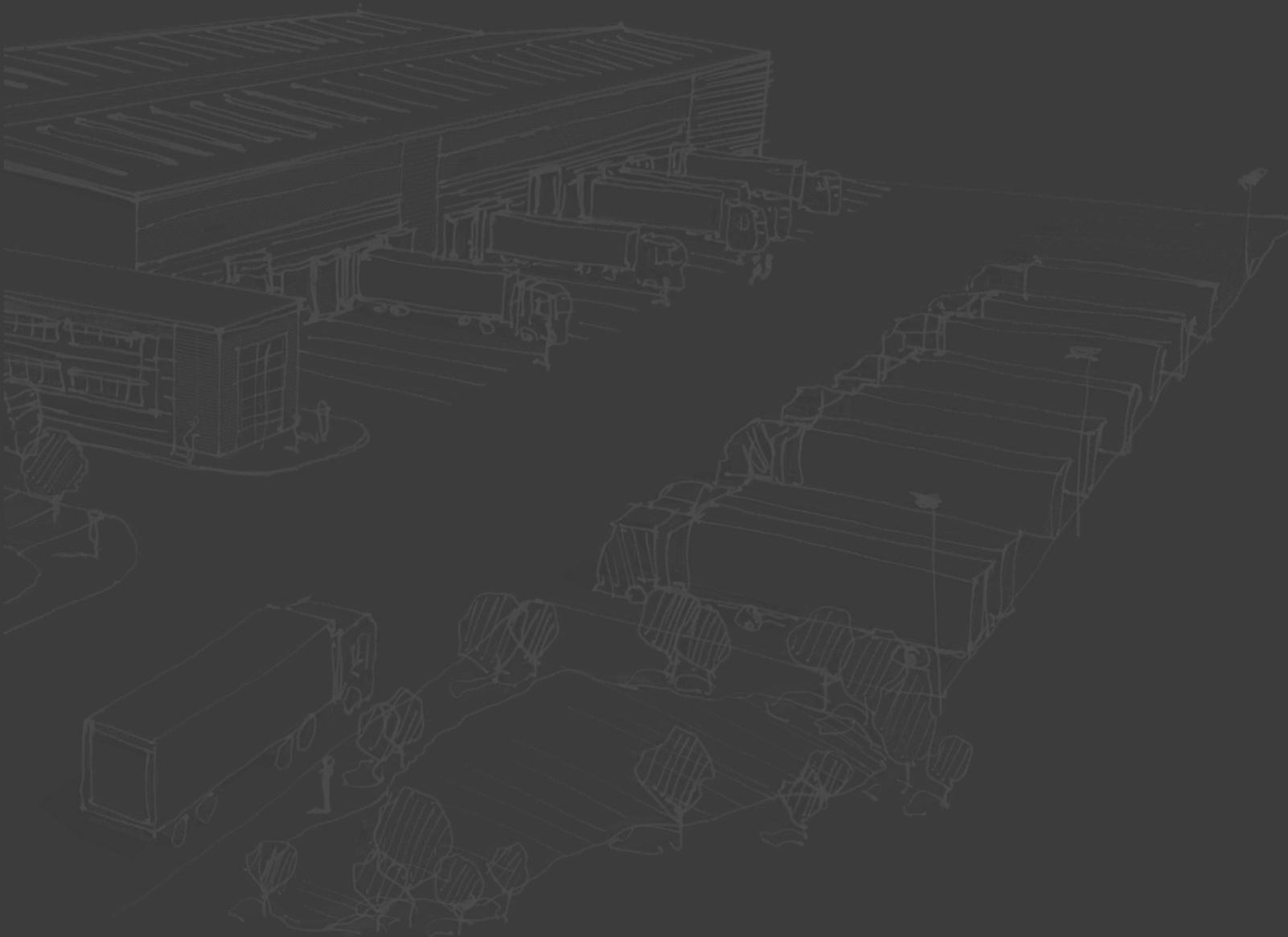
Forestry image



Avenue trees Image

09 SUSTAINABILITY

- 09.1 Sustainability, Wellbeing & Biophilic Concepts
- 09.2 Future-Proofing for Net Zero Carbon



09 SUSTAINABILITY



Exploring enhanced bus and public transport options



Priority parking for car sharing schemes as part of site-wide Travel Plan



Electric Bike Charging



Bird and bat boxes in established trees



Exploring locally sourced planting for landscape areas



Safe cycle and pedestrian routes around site



Biodiverse SuDS drainage ponds



Rainwater harvesting for use in WCs to reduce water consumption



Secure cycle storage and shower facilities for offices



Water Saving Taps & Dual Flush WCs

09.1 SUSTAINABILITY, WELLBEING & BIOPHILIC CONCEPTS

The Sustainability Strategy for the development not only conforms to all national and local planning policies, it also aims to achieve the highest viable levels of sustainable design and construction, whilst minimising environmental impacts. The proposed buildings which will be applied for under subsequent reserved matters applications will be designed to high environmental and quality standards, the key features of which are illustrated in the adjacent image and summarised as follows:

- To provide a high-quality development that is adaptable and resilient to future climate change, with all buildings built to achieve at least a BREEAM 'Excellent' rating under the New Construction 2018 scheme;
- To support the move towards a circular economy by using recycled materials where possible and eliminating waste to landfill;
- To commit to an operational (regulated at energy only) carbon net zero;
- An additional 0.15kN/m² roof loading for future Photovoltaic extension;
- To create a safe and friendly environment that will be flexible to the needs of its occupants, encourages active travel and creates a sense of wellbeing;
- To have a positive impact on the local community by connecting the development with local residents and natural settings through sustainable modes of transport and green spaces, whilst being considerate of local residents during construction;
- To future-proof the development for the transition to zero carbon, including significant provision for EV charging and renewable energy technologies comprising solar PV, solar thermal and air source heat pumps and an 'A' Rated Energy Performance Certificate (EPC);
- To create a development that adds significant social value to the area.

Facilitating Electric Vehicles

5% Active and 20% passive electric charging points will be provided within the car parks of each building to provide employees with the infrastructure to charge their EV's. The use of EVs (both cars and cycles) will also be encouraged via the sustainable travel plan to be developed as part of future reserved matters applications.

Cycle Store and Showers

To encourage staff to cycle to work, cycle stores and showers will be proposed.

Percussion (push) Water Saving Taps

These taps require pressure to be turned on and automatically turn off when not in use. The taps can be set to operate for a few seconds only. This reduces water use by elimination of dripping and taps being left on, sometimes for long periods. In addition, the taps reduce the flow of water so that a reduced amount of water is used for each washing event.

09 SUSTAINABILITY



Electric Vehicle charging points - 5% active and 20% passive



Carpets with 80% recyclable yarns



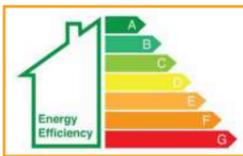
Recycled material partitions and mineral fibre ceiling tiles



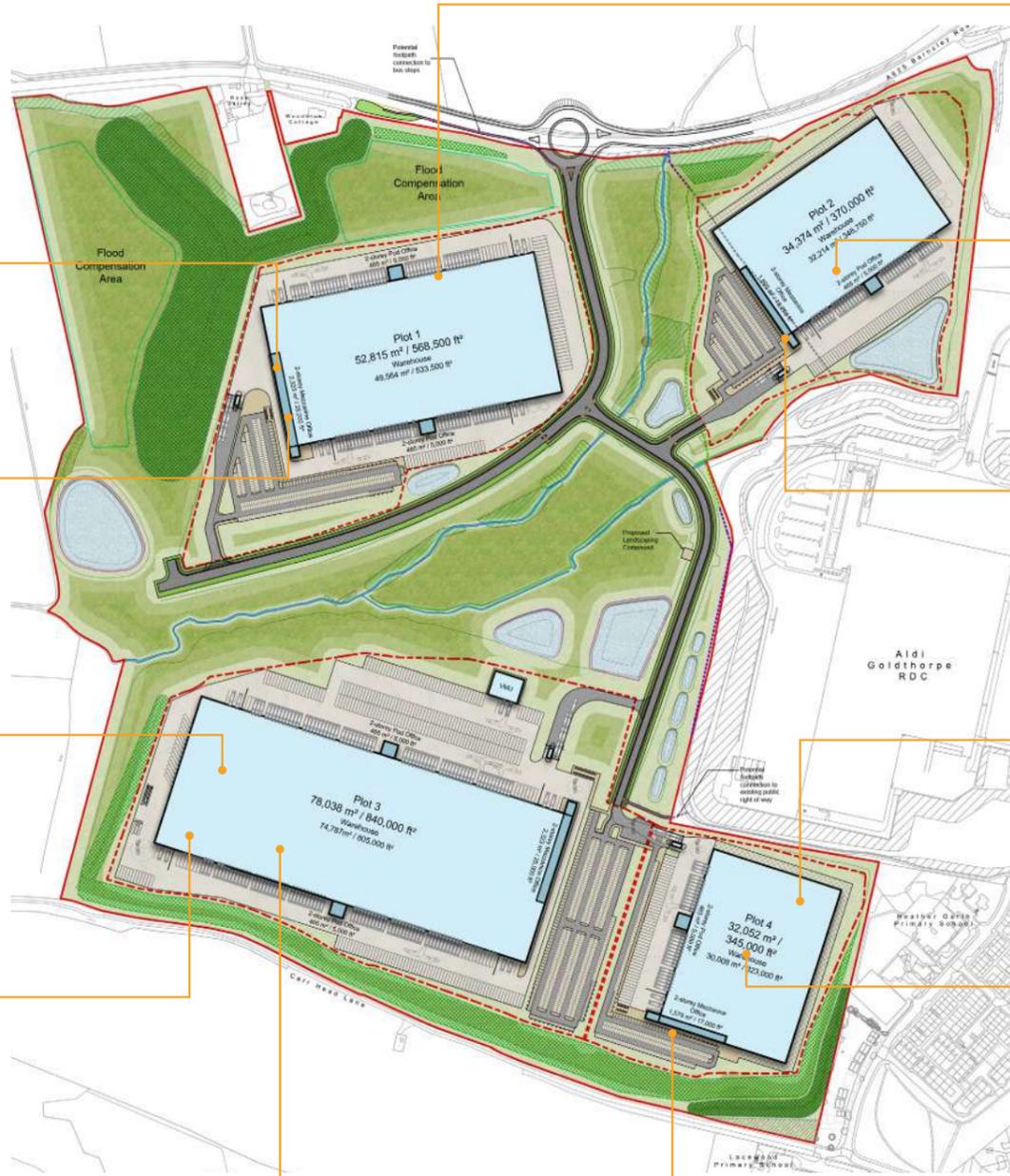
Recyclable frame and envelope solution



Minimum BREEAM 'Excellent' but working with occupiers to achieve higher ratings



EPC A-Rated Buildings



Enhanced facade system to meet air tightness as low as 1.5m³/hr/m² at 50Pa



15% Rooflights that maximise natural daylight and minimise lighting demand



LED lighting with PIR & daylight sensors to internal and external areas



Solar Thermal to supplement hot water provision



15% Solar PV provision, with Solar-ready roofs for achieving zero-carbon reductions



Air Source Heat Pumps to provide low carbon heating & cooling

09.2 FUTURE-PROOFING FOR NET ZERO CARBON

Natural Daylight/Rooflights

High levels of natural daylight will be provided, wherever possible, through effective window design and 15% roof lights to the warehouse space to reduce the need for artificial lighting

EPC

The applicant will also implement the requirements of the Energy Performance of Buildings Directive and assess the operational energy performance of the building, targeting an 'A' rated Energy Performance Certificate (EPC).

Solar Technologies

Building roofs will be solar ready for the installation of PV panels.

Air Tightness

In accordance with the requirements of a low energy building, the air tightness characteristics will be addressed. Through robust design and detailing the building will achieve high air tight standards to mitigate air leakage, far in excess of current Building Regulation minimum requirements.

BREEAM

In order to benchmark the environmental performance of the building, the applicant will carry out an assessment against the Building Research Establishment (BRE) Environmental Assessment Method (BREEAM). This is a voluntary scheme that aims to quantify and reduce the environmental burdens of buildings by rewarding those designs that take positive steps to minimise their environmental impacts. Projects are assessed using a system of credits which results in a formal certification giving a rating on a sliding scale. The proposed buildings will achieve a minimum BREEAM rating of Excellent.

Minimum BREEAM Excellent rating to promote high energy performance, water use reduction and on-site renewable energy;

Promoting sustainable transport options including public transport, walking and cycling for future end users;

New and replacement biodiverse landscaping, tree planting and sustainable drainage strategy.

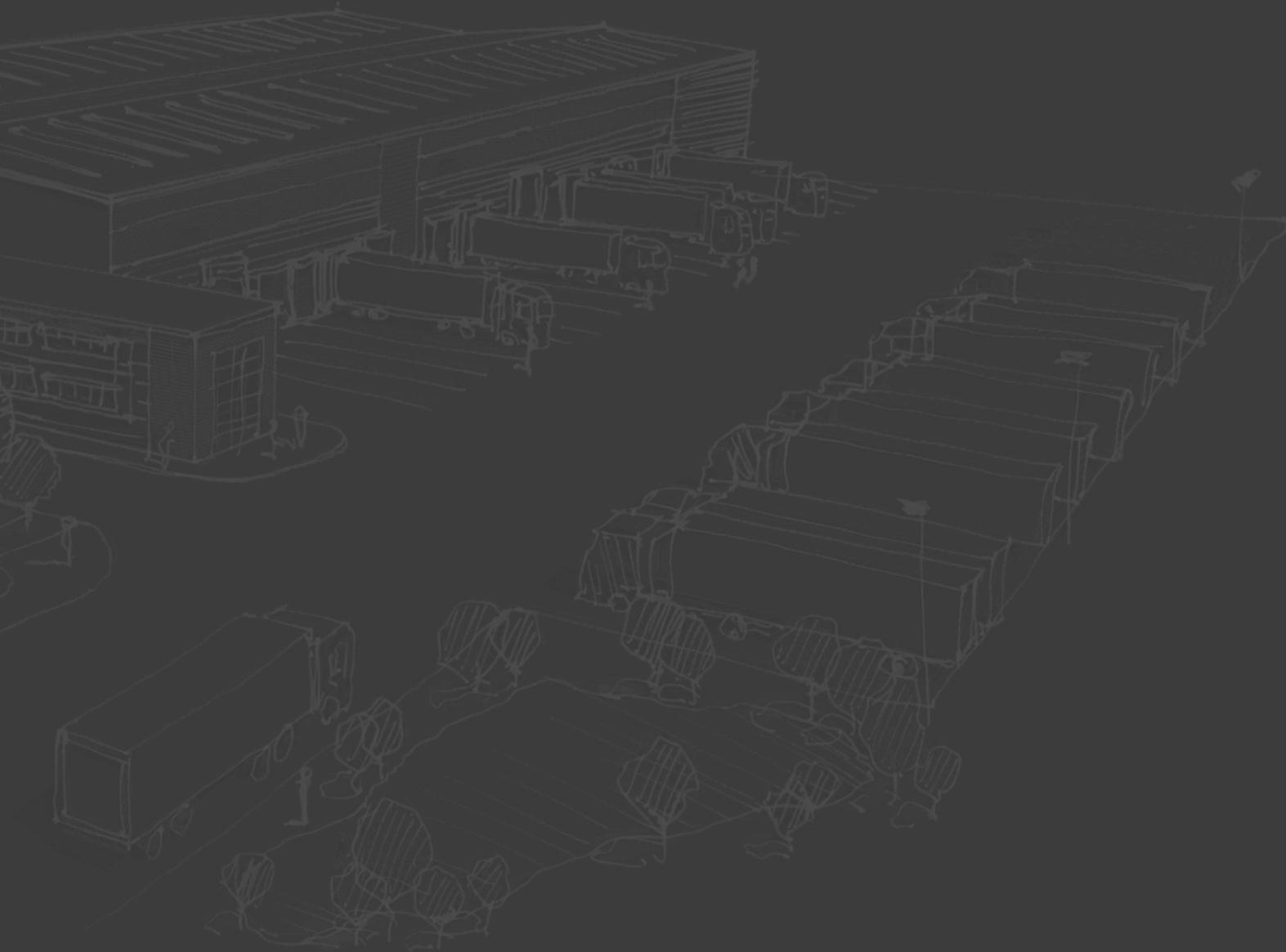
All electric buildings with potential use of on-site renewable energy such as air source heat pumps and PV panels;

Minimum EPC rating of Band 'A' with a target for A+;

Electric vehicle charging points.

10 CRIME PREVENTION & LIGHTING STRATEGY

- 10.1 Crime Prevention Strategy
- 10.2 Access and Movement
- 10.3 Structure
- 10.4 Physical Protection
- 10.5 Lighting Strategy



10 CRIME PREVENTION & LIGHTING STRATEGY

10.1 CRIME PREVENTION STRATEGY

Consideration has been given to the illustrative layout of the development to ensure personal safety. This relates not only to ensuring that the illustrative layout of the development does not create an environment conducive to crime, but also to how occupiers and visitors to the site can move freely without risk of injury.

10.2 ACCESS AND MOVEMENT

Spaces and pedestrian routes are currently well defined with easy to recognise entrances; this provides convenient movement without compromising security. Proposed car parking is provided in the most prominent locations possible.

10.3 SURVEILLANCE

Natural surveillance will be a key factor in the overall design of the site and the positioning of the offices overlooking proposed car parking will offer a high degree of visual control. The building's designs and layouts will be considered to minimise visual obstacles and eliminate places of concealment and any potential dark areas will be well lit.

Formal surveillance will be in the form of an extensive CCTV system provided in line with the occupier's requirements.

10.4 PHYSICAL PROTECTION

Boundary protection will be provided to all service yard areas, but will be considered to maximise natural surveillance.

10.5 LIGHTING STRATEGY

The proposed development will introduce new artificial light sources primarily related to external lighting for car and lorry parking, at loading bays and around the peripheries of the buildings. Lighting will be designed to provide a safe environment for workers, vehicles, cyclists and pedestrians while also avoiding light pollution, especially with regard to sensitive habitats. Lighting for all adoptable roads, footpaths and cycle ways shall be agreed with the Local Authority.

The following general principles and design considerations shall govern the detailed lighting design:

- the requirements for nature conservation shall be integral to the lighting design;

- lighting shall be designed to be sensitive to and provide an appropriate interface between the Development and the surrounding context;
- lighting shall provide a safe environment during dark hours;
- lighting shall be controlled by switching/dimming regimes where appropriate; and
- lighting shall be of a high quality and designed to minimise all forms of light pollution.

LED luminaires shall be mounted on the outside of the proposed buildings and on columns to provide lux levels in accordance with the submitted lighting design by DFL.

Car parking and road areas shall be lit from columns no greater than 8 metres in height to provide an average of 15 lux, with a minimum of 5 lux at kerb lines.

Illuminance shall increase locally to 50 lux at staff entrances and in loading bay areas.

Nature Conservation

Where adjacent ecological sensitivities are identified, care shall be taken to prevent light spill onto habitat through careful design and positioning of lighting units. Supplementary mitigation shall be employed at detailed design stage if needed. This can include the following, used singly and in combination:

- Local reduction of lighting level
- Reduced mounting height
- Luminaire shields to block light spill
- Warm white LED sources (peak wavelength >550 nm, zero UV emission)
- Construction phase: lighting extinguished after dark during the sensitive season.

All lighting within or near to sensitive habitat shall be designed in consultation with an ecologist and in accordance with ILP/Bat Conservation Trust 'Bats and Artificial Lighting in the UK'.

Transport and Access

The following principles and design considerations shall be followed for street lighting:

- all new or upgrading of street lighting shall be assessed for its benefits to the community from safer roads, reduced night time crime and the improvement or regeneration of the area involved;

- all new or upgrading of street lighting shall be designed as appropriate for the road classification and in accordance with BS 5489-1:2020. All designs shall be approved by the Street Lighting Manager or Lighting Engineer;
- a lighting class shall be chosen to provide adequate levels of illumination for the task required. The lighting class shall be selected in accordance with PD CEN/TR 13201 Part 1 with the lighting levels in accordance with BS EN 13201 Part 2. The checking of lighting levels shall be in accordance with BS EN 13201 Part 4; and
- the option of part night operation shall be considered.

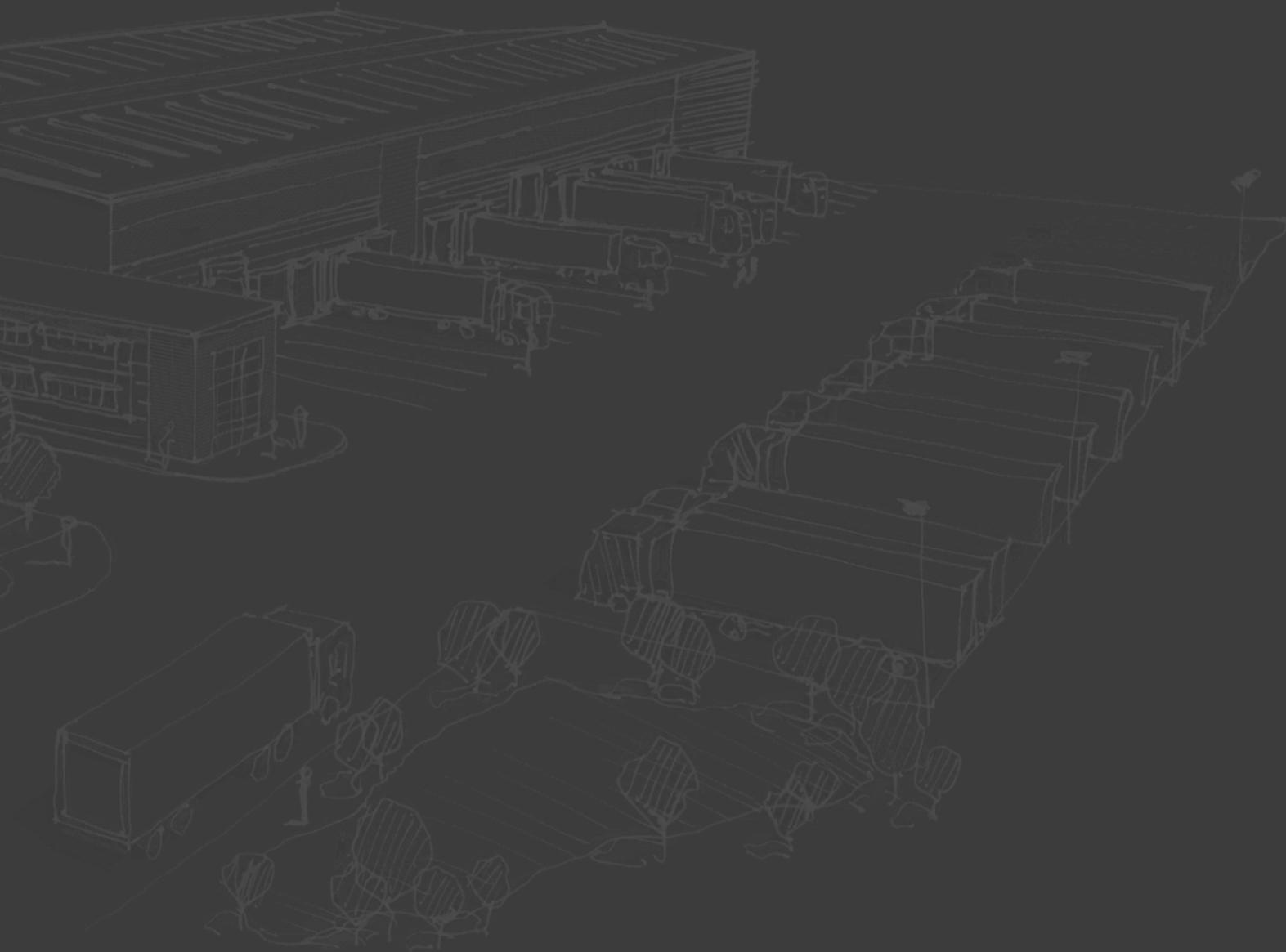


Indicative Plot Frontages



Indicative Security Fencing

11 SUMMARY



11 SUMMARY



Illustrative Aerial View of Development

11.1 SUMMARY

This Design and Access Statement forms part of the hybrid planning application supporting the delivery of a high-quality development which will attract leading national and international logistics businesses as well as locally based businesses seeking to expand. The site delivers a scheme aimed to present as a best in class employment location and an economic asset Barnsley District Council.

The site is commercially attractive for a distribution/logistics hub being well connected to the A1(M) and M1 Motorway, and strategic road network in a region which has a significant under supply of logistics accommodation. The development represents a substantial investment to the area and has been developed in conjunction with national and regional development aspirations

Natural Connectivity is achieved through a 'landscape led' design philosophy, integrating the scheme back into the local area whilst enhancing and expanding existing green and blue infrastructures. Benefiting from these improved natural resources, pedestrian and cycle circulation is prioritised and encouraged throughout the development.

High quality design will be executed throughout the development through carefully sited and appropriately sized built forms. Quality architecture will establish a strong sense of place, which is complementary to the site location.

Illustrative building designs have been provided for the outline plots, to allow a glimpse into future reserved matters applications. Whilst designed individually, the overarching architectural language will read as a cohesive development, fit for purpose and providing a safe and accessible development for all to use.

The proposals seek to approach sustainability through implementation of a range of passive and active sustainable design measures.

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